



**Donald Insall Associates**  
Chartered Architects and Historic Building Consultants

**Empress Conservation Area Regeneration Plan**

For Trafford Council



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**Annex B** - Conservation Area Appraisal and Management Plan

**Annex C** - Design Code

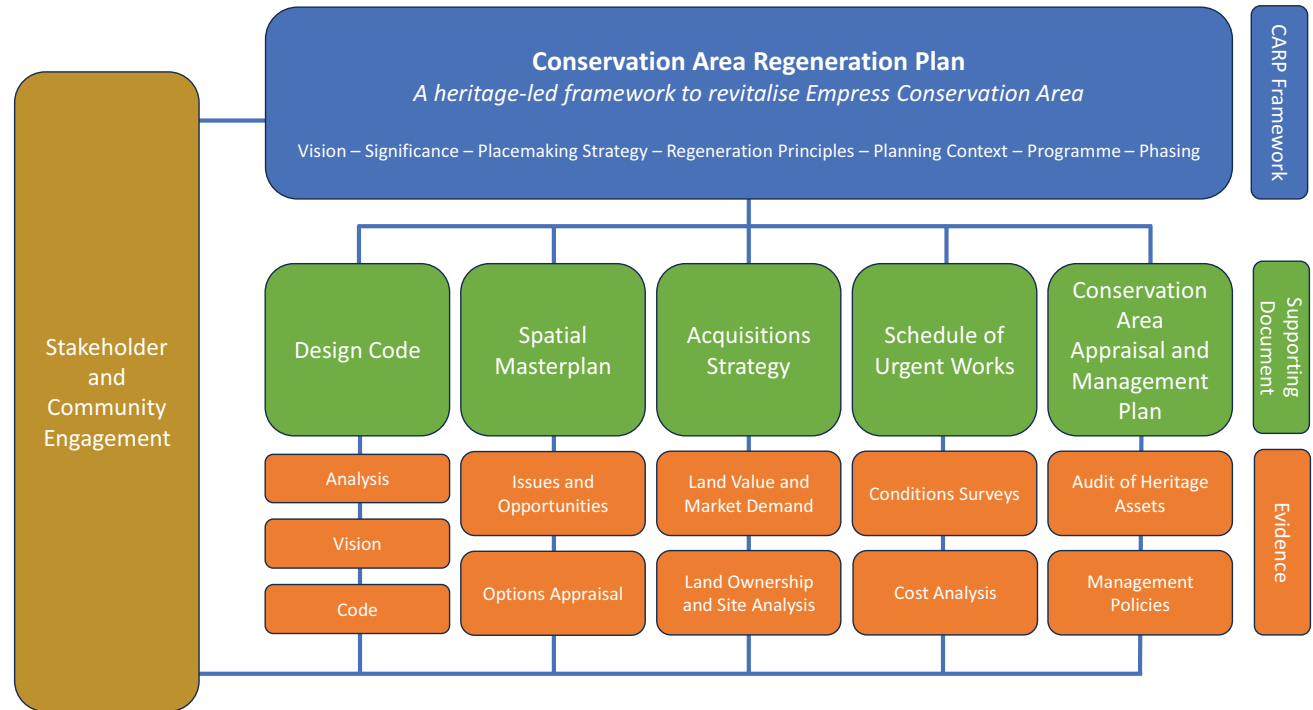
**Annex D** - Options Appraisal

# 1. Introduction

Trafford Borough Council, in partnership with Historic England and Homes England, has commissioned a multidisciplinary team including Donald Insall Associates (Insall), LDA Design (LDA) and Lambert Smith Hampton (LSH), to produce an aspirational, and practical Conservation Area Regeneration Plan (CARP) for the Empress Conservation Area.

The area evolved as an industrial and residential hub alongside the canal, railway and dock networks in the 19<sup>th</sup> and early 20<sup>th</sup> centuries. It retains a mixed industrial and residential character visible in its historic buildings and land uses with smaller-scale terraced housing on Chester Road coinciding with large, grand, industrial blocks such as the Essence Factory, Former Empress Brewery, Veno, Trafford Press and Empress Mill. These collectively place the area in its significant historic context and tell the story of its industrial origins and that of Trafford more widely.

The area has, however, suffered from years of decline and has been on Historic England's Heritage At Risk Register since 2012; reflecting numerous factors including unsympathetic and stalled developments, loss of traditional features, and critically the deteriorating condition of landmarks at its heart. In the context of local, regional, and



**1.1 Conservation Area Regeneration Plan Work Streams and Framework**

national government focuses on levelling up and good placemaking, the CARP grant scheme, launched and co-funded by Historic England, seeks to support local authorities in enabling transformational socio-economic growth specifically through leveraging the significant impact of heritage regeneration.

The CARP for the Empress Conservation Area is underpinned by several workstreams **[Plate 1.1]** which are presented as technical appendices to this vision statement including:

- Condition Surveys
- Conservation Area Appraisal and Management Plan
- Design Code










Further informed by a process of stakeholder engagement and public consultation, this statement outlines the high-level vision for the CARP, providing an understanding of what makes the area special and what is putting that at risk, addressing key themes such as placemaking, constraints and opportunities and providing a visionary regeneration plan for the area.

The principal aims of the CARP are to:

- address the causes of decline in and around Empress
- generate investment
- sensitively redevelop the area
- encourage the community to understand and enjoy this undervalued historic part of Old Trafford
- secure the area's longer-term vitality and vibrancy.



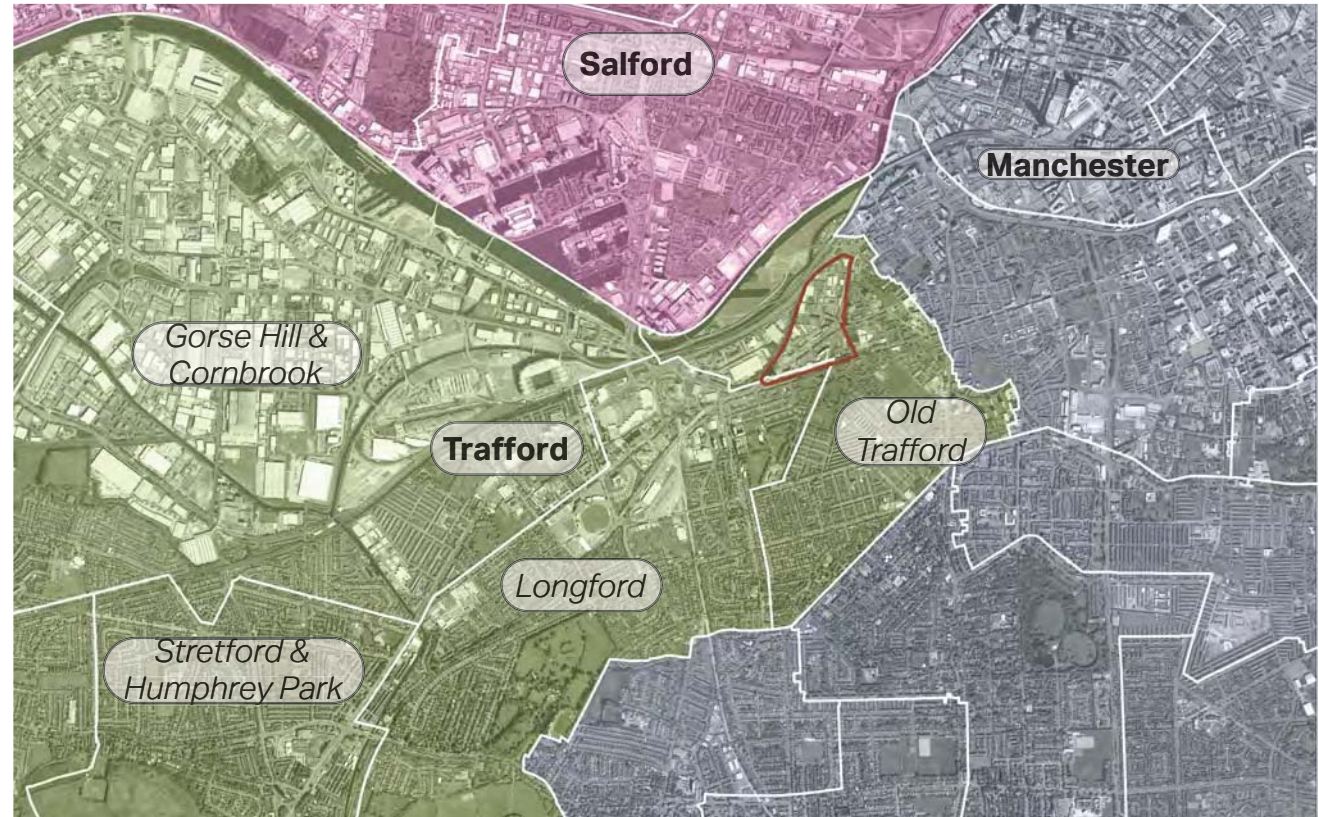


-  Regeneration plan boundary
-  Indicative layouts for development areas
-  Existing buildings to be retrofitted
-  Private, communal, and public green spaces
-  Key hard open spaces
-  Existing site plots and buildings
-  Immediate development opportunities
-  Sites for retrofit / enhancement
-  Future development opportunity for change



## A strategic location at the gateway between Trafford and Manchester

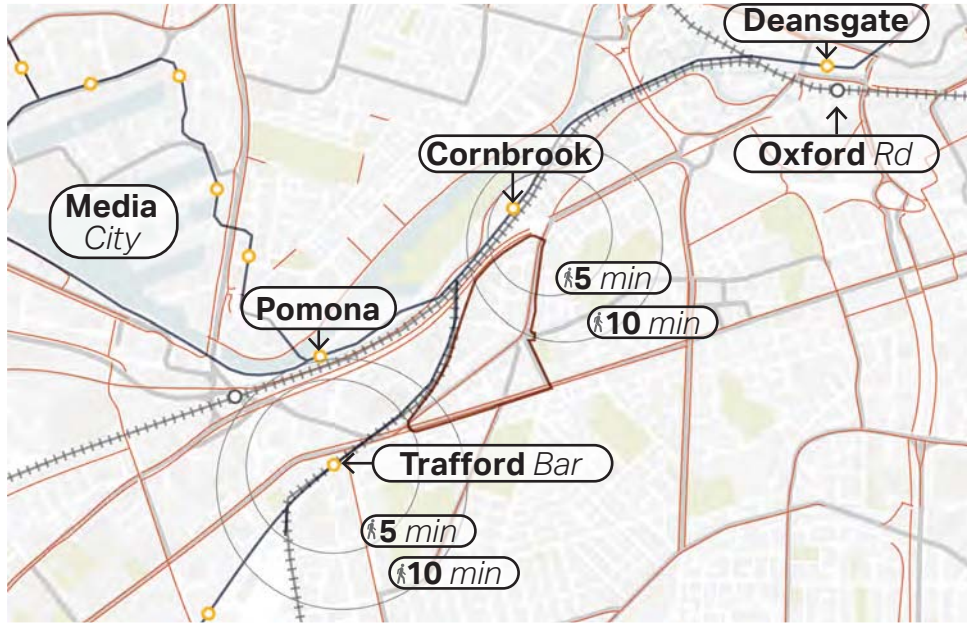
- The study area, focussed around the **Empress Conservation Area**, is located in the north of Old Trafford.
- Chester Road runs through the heart of the site and provides **direct connection into Manchester city centre** and into Old Trafford.
- Located to the north of the predominantly residential area of Old Trafford and Hulme, as well as the emerging regeneration areas of Cornbrook and Manchester Waters.
- The site is located within close proximity of both the **Cornbrook and Trafford Bar Metrolink** stations, offering connectivity into Manchester City Centre (10mins), Salford and the rest of Trafford.
- Existing connections to surrounding cultural and recreational landmarks, including **MUFC and Media City**.
- Limited access to local centres and amenities.



Empress sits at the gateway between three boroughs, forming a strategic opportunity for regeneration at the entrance to Trafford.

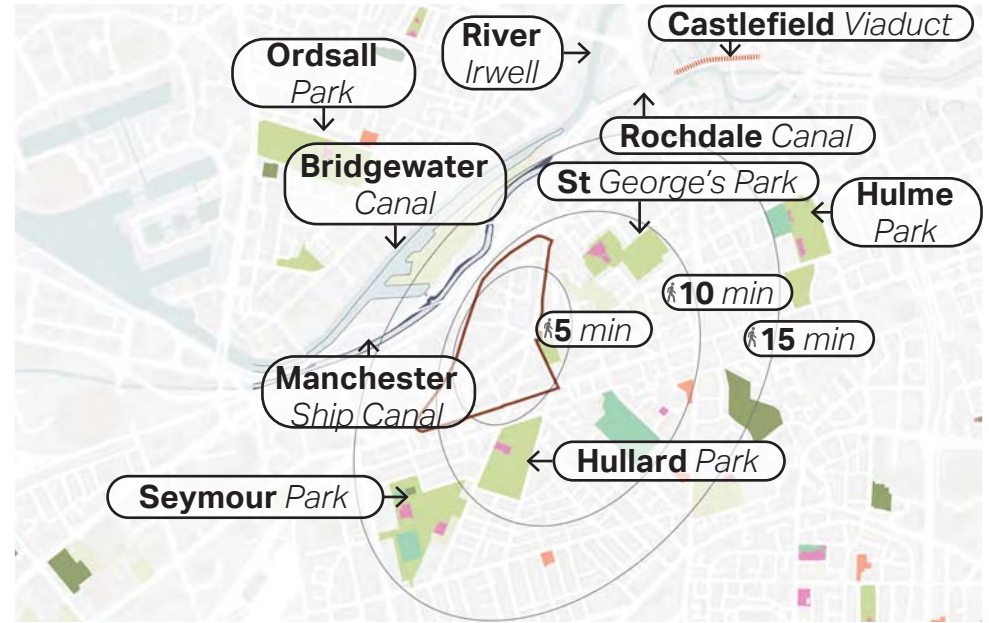






The site is located adjacent to two Metrolink stations (Cornbrook & Trafford Bar), however pedestrian connection to both stations is limited due to severance created by large highway infrastructure. The site is well served by cycle lanes connecting the site to the city centre and through Trafford.

- Site boundary
- Railway line
- Railway stations
- Tram line
- Tram stations
- Greater Manchester bus route
- Cycle routes
- 5/ 10 min walking distance from Cornbrook & Trafford Bar tram stops



The site is located within 5-10 minutes walk of a number of high quality public parks, including Seymour and Hullard Park. Pedestrian and cycle access to these parks, however, is poor.

- Site boundary
- Indicative green space
- Cemetery
- Playing field
- Religious ground
- Public park/ garden
- Allotment
- Sports facility
- Castlefield viaduct
- Green/ blue wildlife corridor
- Play space
- 5/ 10/ 15 min walking distance from centre of site

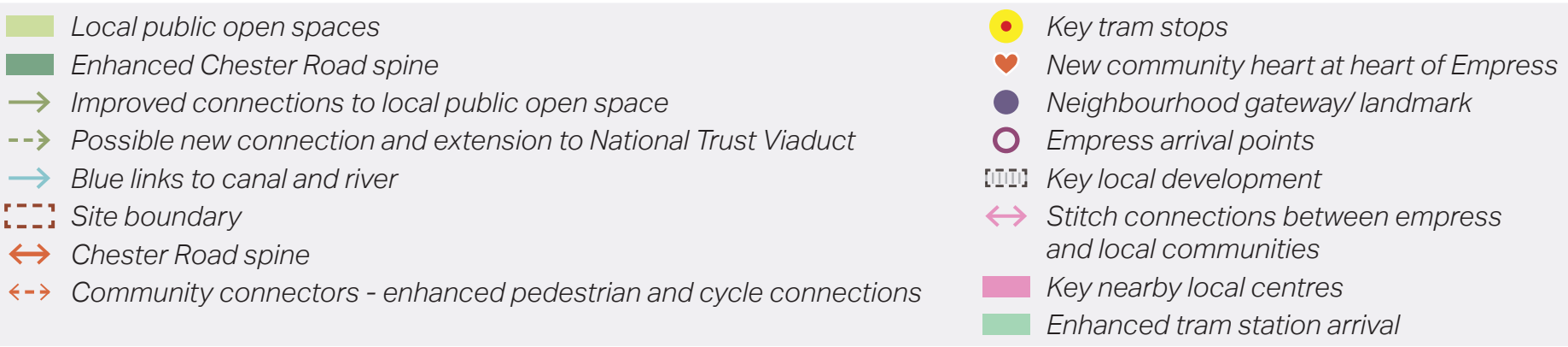
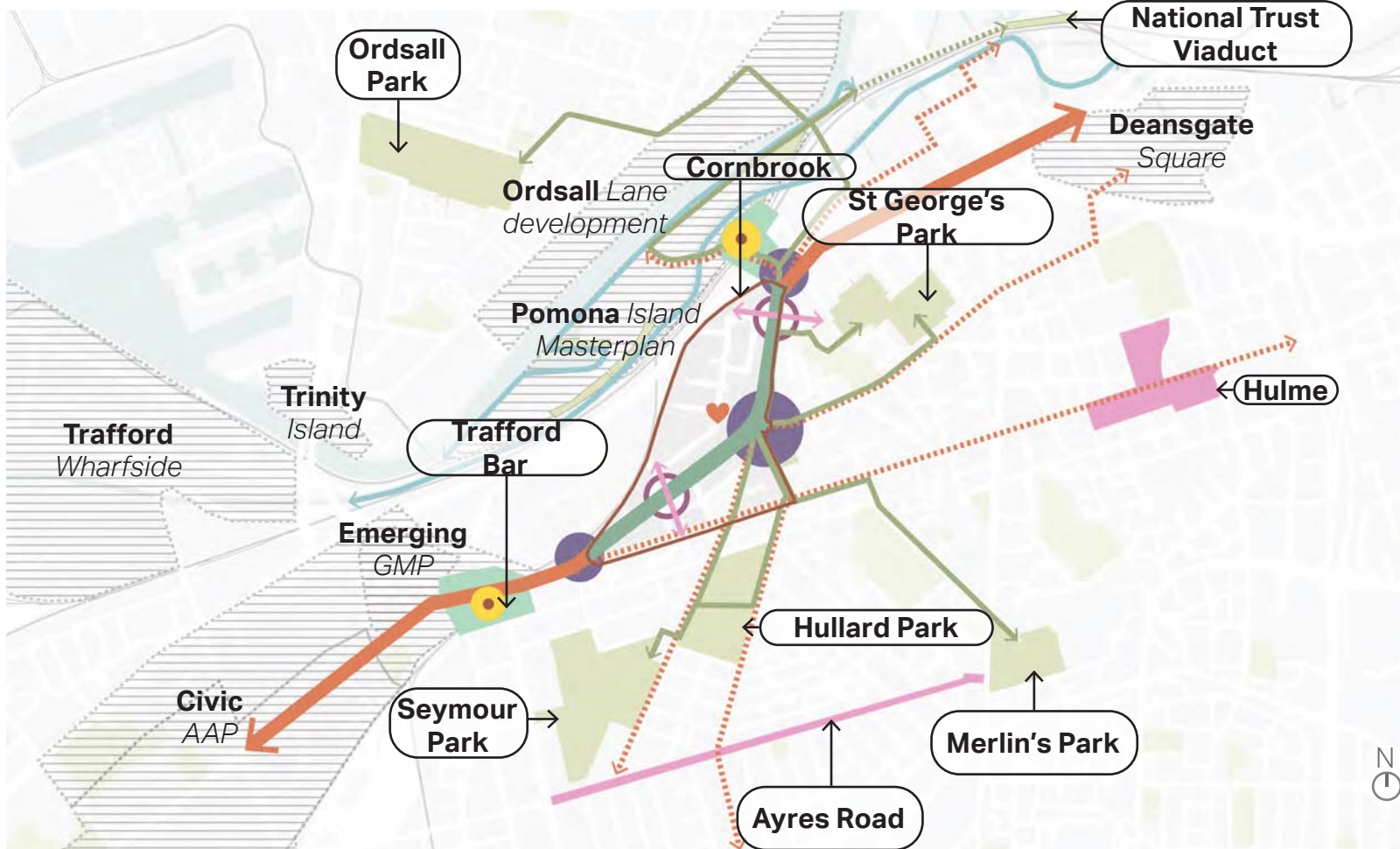
## 2. Neighbourhood Placemaking Plan

The adjacent plan outlines a set of strategic proposals which will knit the site into the wider area, delivering a welcoming and active new community which is connected to the wider neighbourhood and its green infrastructure.

The following strategic proposals are being considered:

- Delivering an **outward looking** community which engages the **existing and emerging communities**
- Give the place a **heart**, offering a **place to live, work, create** and engage at the **gateway** to Old Trafford
- Turn **Chester Road** into a **stitch** between **Empress** and the **surrounding community**, not a barrier
- Create **green connections** to surrounding parks and play spaces, coupled with **community links** to Ayres Road, Trafford Bar, Castlefield and Hulme.
- Unlock connections to the **canal network** and Manchester Waters, including link to the new National Trust Castlefield Viaduct and Ordsall
- Create **strategic connections** to surrounding **activity hubs**, including the Civic Quarter, Chorlton, Media City and the City Centre
- Enhanced connections to surrounding **public transport links** and emerging development areas at Civic Quarter and Cornbrook.





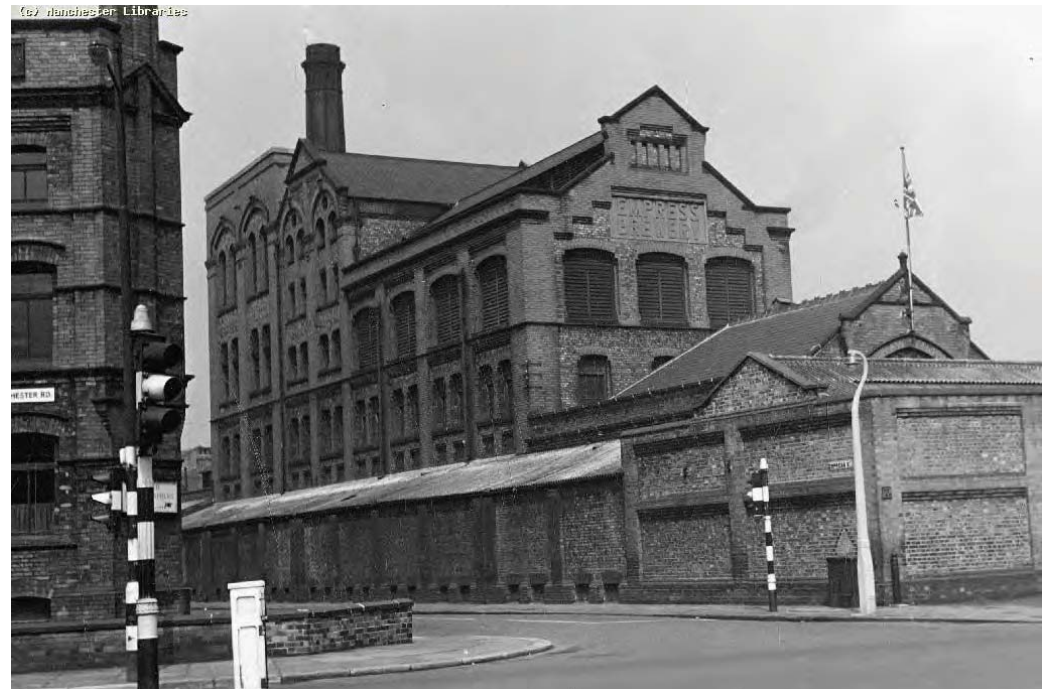
### 3. Character and Significance

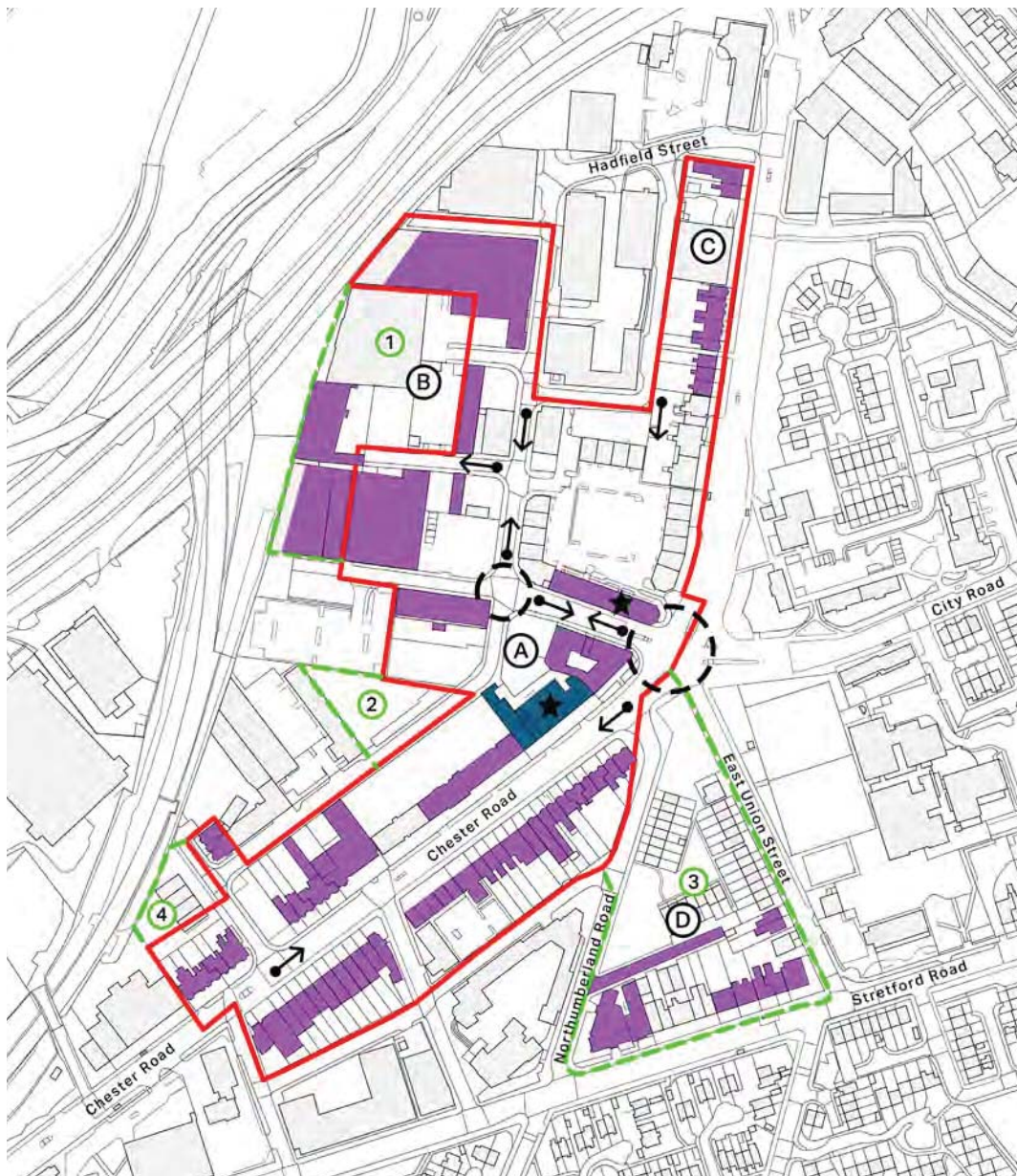
#### Empress is special, but at risk ...

The Empress Conservation Area retains a mix of residential and industrial uses that have significant historic depth. The area grew in the industrial booms of the 18<sup>th</sup> and 19<sup>th</sup> centuries, central to the broader evolution of Trafford. This importance is represented in the proximity of red brick terraced houses, focused on Chester Road, to larger-scale factories and other industrial buildings, many of which continue to operate today.

This includes landmarks such as the former Empress Brewery, now an active business centre, as well as the Essence Factory, Veno Building, Former Trafford Press Offices and Empress Mill, all clustered around Chester Road and Empress Street. These buildings and uses both contrast and complement with one another to tell the story of the area's growth, and continued importance to Trafford.

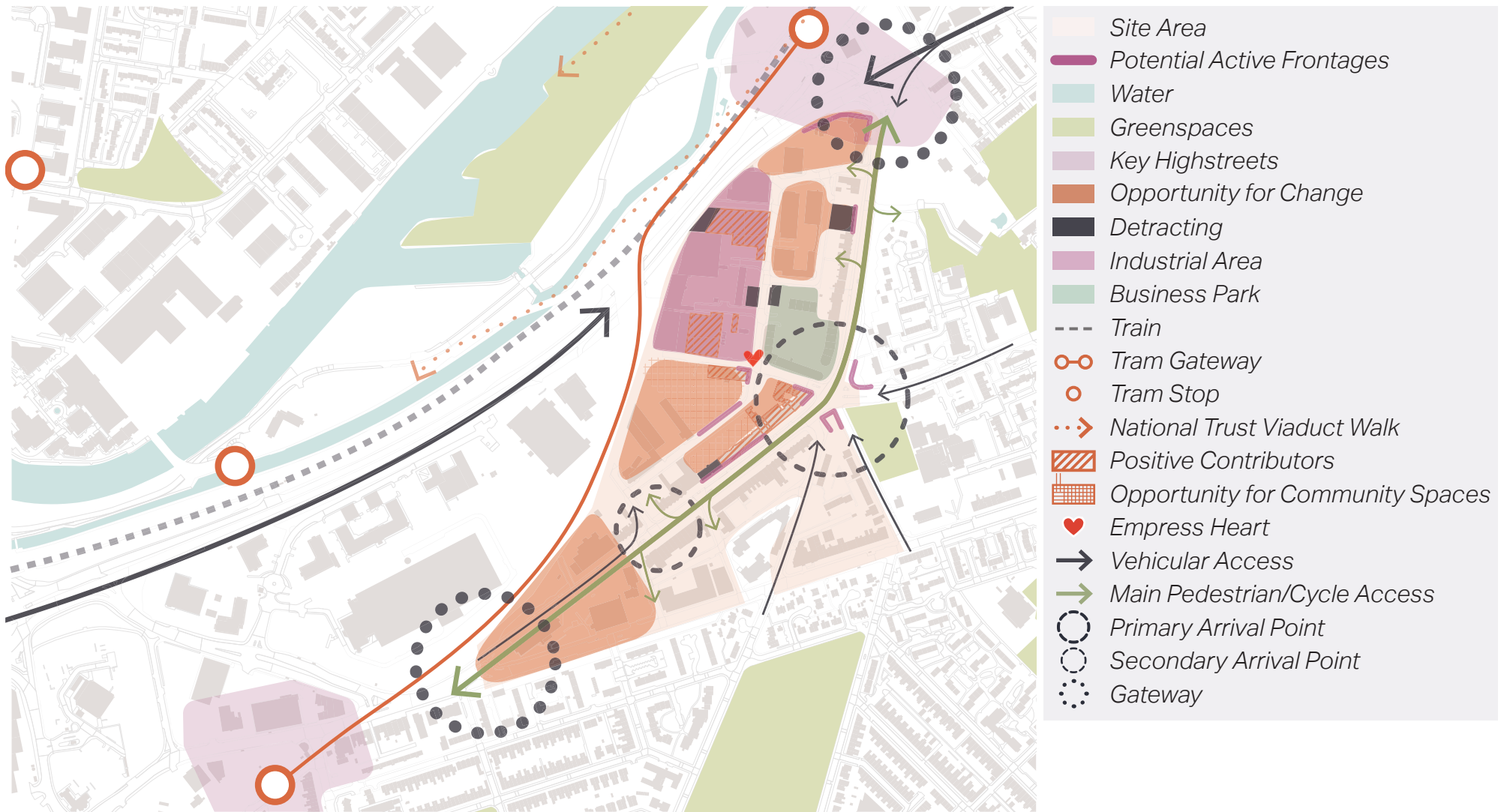
Whilst the area has a quintessential industrial and urban character, comprising residential and industrial uses, the area is at risk. Key landmarks such as the Essence Factory and Empress Mill are vacant, and their condition deteriorating. Similarly, schemes to bring forward conversion or new development, including the adjoining v and Trafford Press Buildings have stalled. This heritage, however, can play a critical role in regenerating the area and enhancing what makes it special to those that use it.
















- Current Boundary
- - - Proposed Inclusion
- ① Industrial development west of Hadfield Street
- ② Land to the south of Empress Mill
- ③ Land bound by Northumberland Road and East Union Street
- ③ West side of Darwen Street
- Listed Buildings
- Positive Contributors
- ➔ View within the conservation area
- Nodal Point & View Cluster
- ★ Landmark

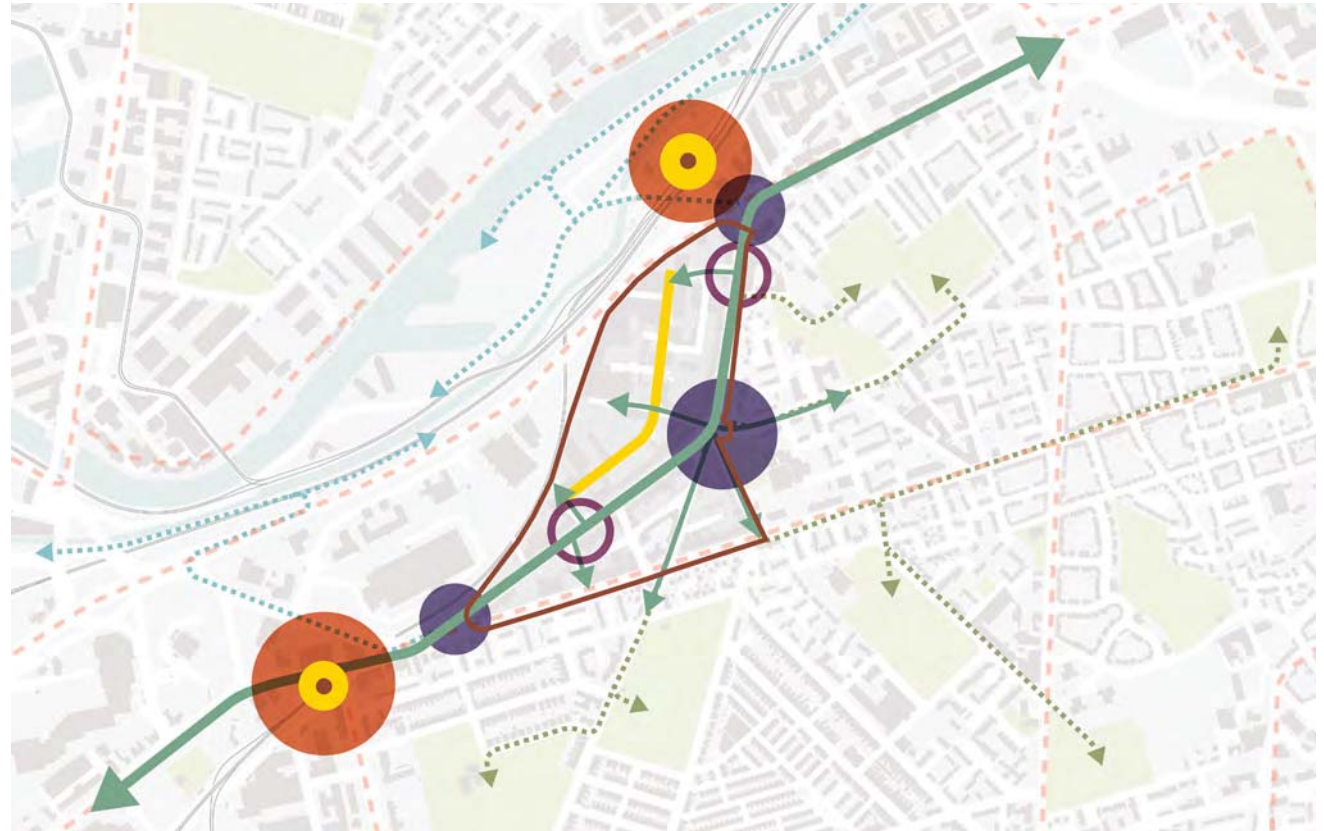
## 4. Opportunities



- Site Area
- Potential Active Frontages
- Water
- Greenspaces
- Key Highstreets
- Opportunity for Change
- Detracting
- Industrial Area
- Business Park
- Train
- Tram Gateway
- Tram Stop
- National Trust Viaduct Walk
- Positive Contributors
- Opportunity for Community Spaces
- Empress Heart
- Vehicular Access
- Main Pedestrian/Cycle Access
- Primary Arrival Point
- Secondary Arrival Point
- Gateway

## Connectivity

-  Site boundary
-  Chester Road central spine
-  Connection in and out of the site
-  Key tram stops
-  Enhanced tram station gateways
-  Key local cycle networks connecting with site
-  Welcoming gateways connecting Empress to the wider city
-  Enhanced sense of arrival at key entrances into site for residents and visitors
-  Internal core of Empress
-  Enhanced visual and physical connection to local green spaces
-  Enhanced visual and physical connection to local waterways and blue infrastructure



There are **two key gateways** into the site forming northern and southern **arrival points** along Chester Road, the main street running through the centre of the site between the two local tram stops. These gateways have the potential to draw **activity towards the centre** of site, being well connected by **public transport** and **pedestrian access**, as well as the nearby **cycle network**.

Empress Street marks the **key arrival point** for the site, offering potential for enhanced **public space and increased activity**. Darwen Street and Hadfield Street will provide secondary arrival points and enable access to a new active central spine, along with a central heart at Empress Street, which will draw the new neighbourhood together.

## Green Infrastructure & Public Realm



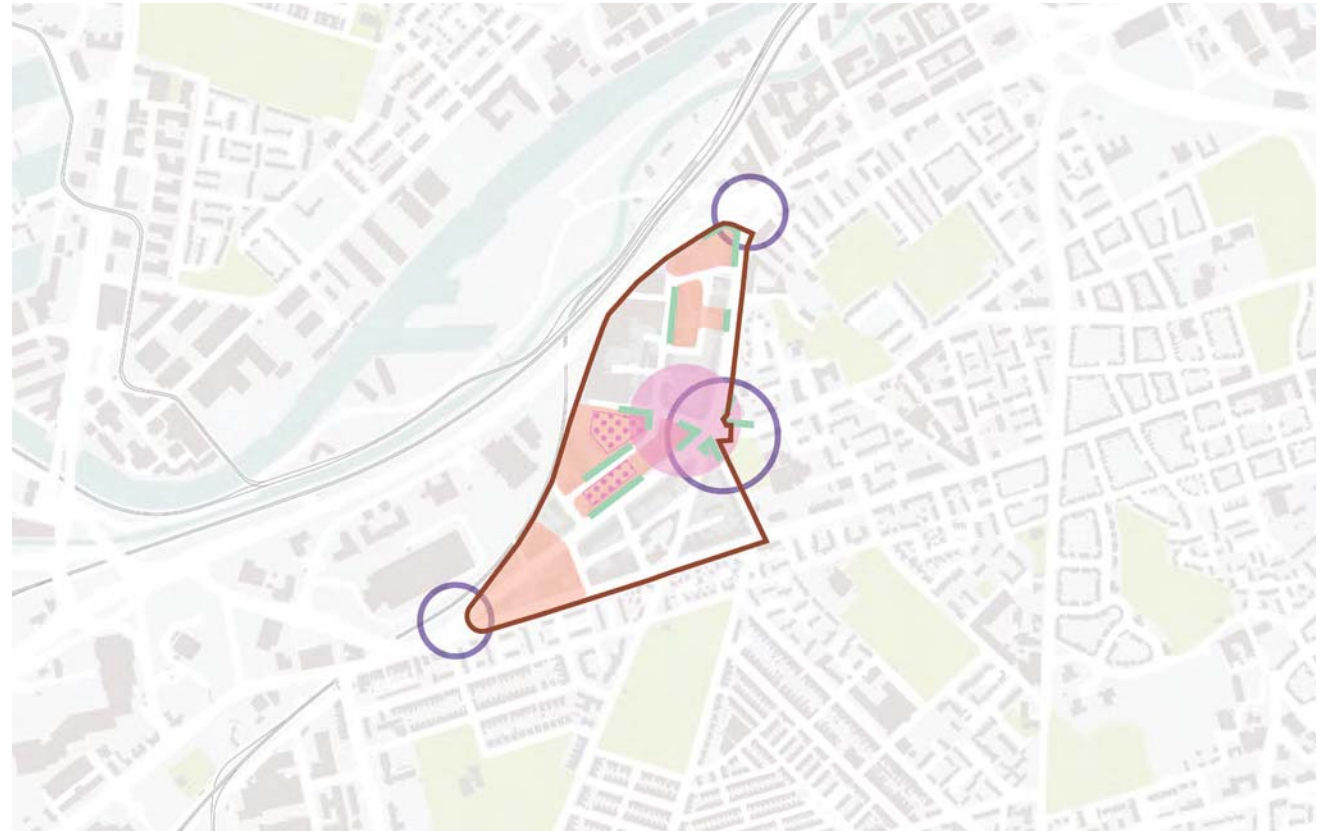
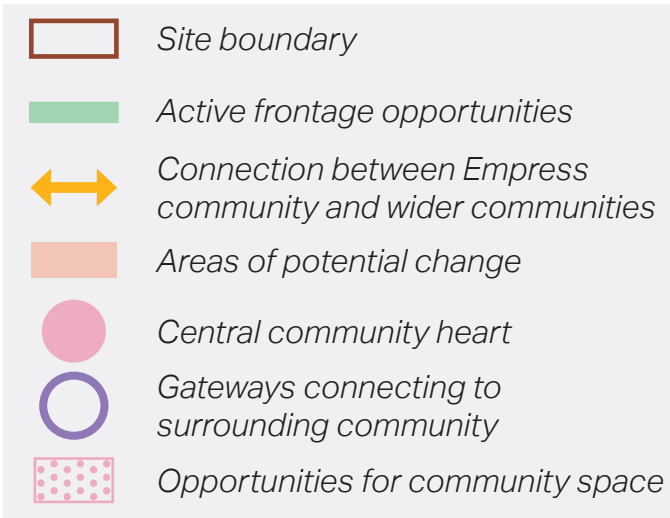
The area is in close **proximity to pedestrian and cycle networks**, including Bridgewater Way along the canal, and many green spaces. This allows for the opportunity to integrate the site within the existing network of open spaces. This may be achieved through the **softening of the streetscape**, which would provide better **physical and visual connection** to these spaces.

The following key moves are proposed:

- Enhanced greenery along Chester Road
- A set of green streets connecting to Seymour Park, Hullard Park and St George's Park
- Strategic green connections to Manchester Waters, Bridgewater Canal, Ordsall Park and Castlefield Viaduct



## Community



The site is situated close a number of **residential neighbourhoods** in Trafford, offering the opportunity to develop proposals that **integrate Empress** within the **existing community** as well as providing **new amenity** that is **accessible** to these existing residents as well as new communities.

The potential active frontages aim to **draw activity** into the **centre** of the site, where interactions within the community may take place, as well as knit the site into the fabric of the existing communities and highlight Empress as a key destination.

## 5. Neighbourhood Placemaking Strategy

### Future Vision for Empress: A place to live, work, create and engage

Building on the site's history as a place where traditional worker homes and industry sit side by side, the future of Empress will be a dynamic live-work environment where creative businesses, communities and industry come together to create a connected and liveable community.

As the area continues to be a place of innovation and making, the place-making strategy aims to build on its rich history as a live-work environment. Our vision is for the neighbourhood to become a place for communities to thrive in new, high-quality sustainable homes. New shops, cafes, and bars will enliven the place and embrace the independent and creative spirit that has existed at Empress for decades. Rather than turning its back on the surrounding community, it will be a welcoming place that opens its arms to the people who live in and visit the area. Historic assets will no longer be unknown relics, instead becoming landmarks that define an exciting, characterful, and engaged community for the future.

The plan is to create green connections to surrounding parks and play spaces, along with community links to Ayres Road, Trafford Bar, Castlefield and Hulme. The aim is to unlock connections to the canal network and Manchester Waters, including a link to the new National Trust Castlefield Viaduct and Ordsall. There will be enhanced connections to surrounding public transport links and emerging development areas at Civic Quarter and Cornbrook. All these steps will help in making the area more accessible and well-connected, providing a more convenient and pleasant experience for residents and visitors alike.



## Strategic Objectives

The following key strategic objectives will guide the change and growth of Empress:



### **Celebrate History and Character**

Celebrating the history, distinctive character and identity of Empress, drawing on the “Live-Work” history and creating a new lease of life for key buildings and landmarks in the area. Celebrate the emerging and existing community by reflecting the diverse, multi-generational and multi-cultural communities surrounding Empress.



### **Unlock Positive Growth**

Create opportunities for new development and use which preserve and enhance the existing historic and industrial fabric of the place. Support the sustainable and successful growth of the area through a dynamic mixture of uses - including new homes and local centre uses, whilst retaining the industrial core of the place.



### **A New Trafford Landmark**

Open a gateway within Trafford and into the wider city, which is inviting and unique, that celebrates the different cultures, people and the history of Trafford, through the independent identity of community, work, life and creativity.



### **A New Heart to the Community**

A celebratory sense of arrival, drawing people into the heart of Empress, unlocking active uses and bringing together people from different walks of life.



### **Connected into the Network**

Enhancing connections into and towards the site, creating a place that is easy to understand and creating better links to Trafford, Manchester and Salford.



### **Active and Engaged Community**

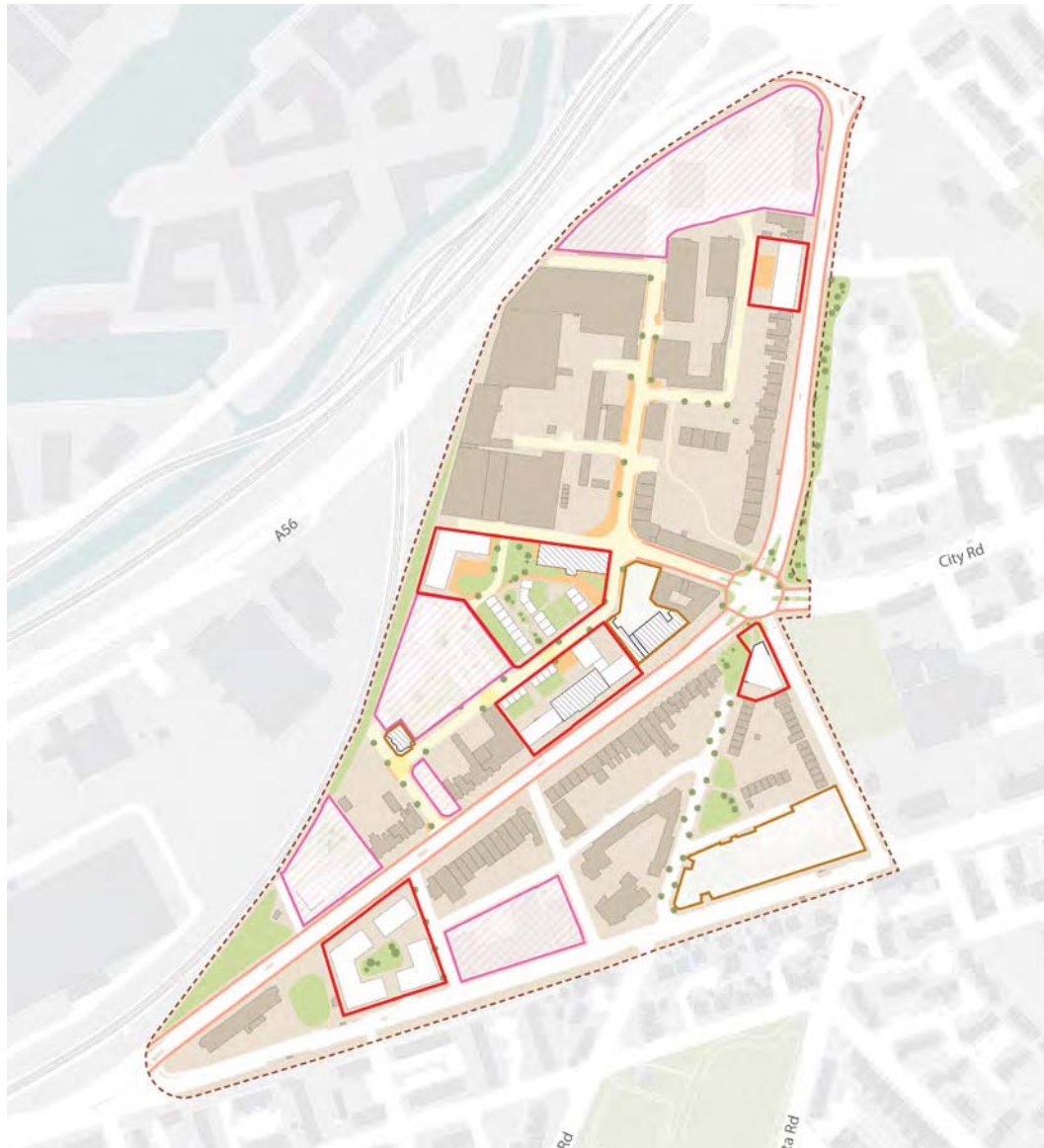
Creatively activate spaces and streets through a mix of uses and high quality public realm, encouraging community activity, engagement and pride. Supporting and integrating existing enterprise, creativity and independence.












### **Outward-Facing and Active**

Knitting together the surrounding community, creating a welcoming edge which connects people into and through Empress and creates a new community destination for the area.

## Illustrative Masterplan



-  Regeneration plan boundary
-  Indicative layouts for development areas
-  Existing buildings to be retrofitted
-  Private, communal, and public green spaces
-  Key hard open spaces
-  Existing site plots and buildings
-  Immediate development opportunities
-  Sites for retrofit / enhancement
-  Future development opportunity for change

The plan on the right illustrates the vision for Empress and identifies sites for immediate development opportunities, retrofit and enhancement, potential future development opportunities and key opportunities.

The design code focuses on the conservation area.

## 6. Design Vision

Our vision is to create an outward-looking community that not only engages existing and emerging communities but also fosters a sense of belonging and inclusivity by offering a place to live, work, create and engage at the gateway to Old Trafford.

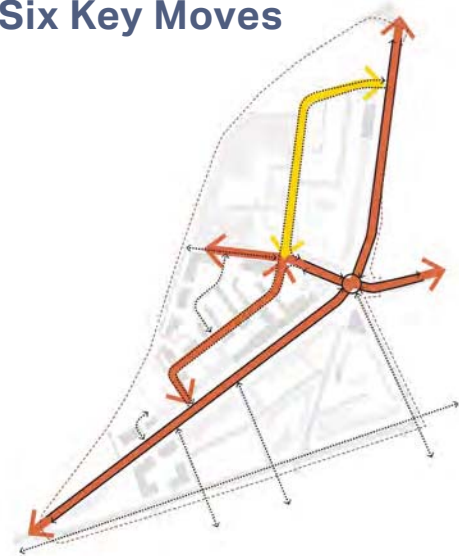
The adjacent strategic moves underpin the structure of the masterplan and will foster a vibrant and inclusive new community. These moves will not only connect the community to the broader neighbourhood but will also enhance its livability and activity.

Empress Street marks the site's primary arrival point, offering the potential for enhanced public space and increased activity. Darwen Street and Hadfield Street will provide secondary arrival points and enable access to a new active central spine and a central heart at Empress Street, which will draw the new neighbourhood together. The northern section of the site will retain heavy goods vehicle access for the industrial and commercial units, while the southern part will form a residential focus.

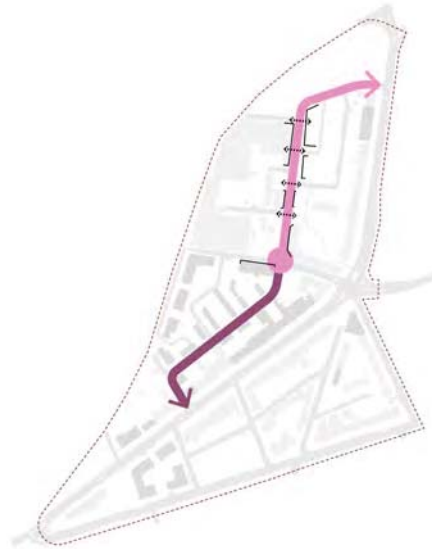
A string of public open spaces creates permeability to pass through the site whilst responding to the various uses, including pocket parks for meeting, spaces with informal play, community parks, routes for dog walkers, social spaces adjacent to meanwhile uses, and spill-out areas for breweries or other commercial activities. The public open spaces align with the urban ecosystem strategy to form a network of green and blue infrastructure by which to access the site, to connect people with nature and improve local biodiversity.



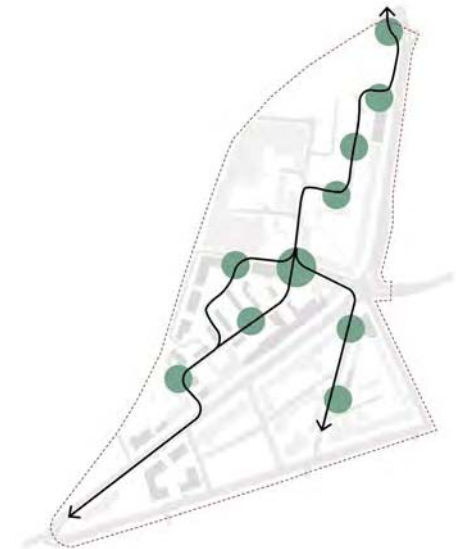
## Six Key Moves



1. Enabling connectivity



2. Active and creative spine



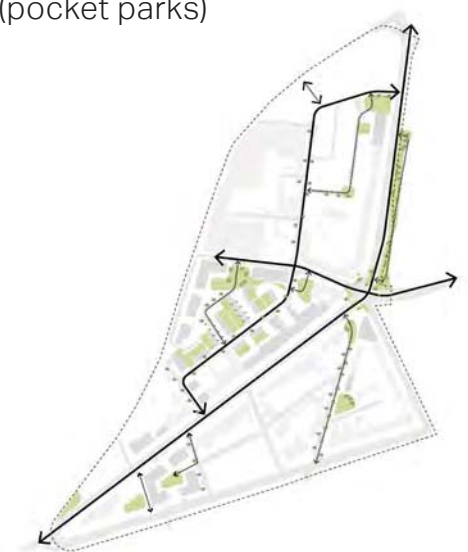
3. String of active public yards (pocket parks)



4. Unlock development opportunities



5. Rejuvenate, activate and celebrate historic assets



6. Define a new urban ecosystem

## Vision for Placemaking



The movement strategy diagram defines the key connections through the site.

The diagram on the right illustrates the vision for Empress to create a place to live, work, create and engage.

## **Annex B - Conservation Area Appraisal and Management Plan**





Donald Insall Associates  
Chartered Architects and Historic Building Consultants

Empress Conservation Area Appraisal and Management Plan

Trafford Borough Council



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Volume 1

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## Volume 1: Appraisal

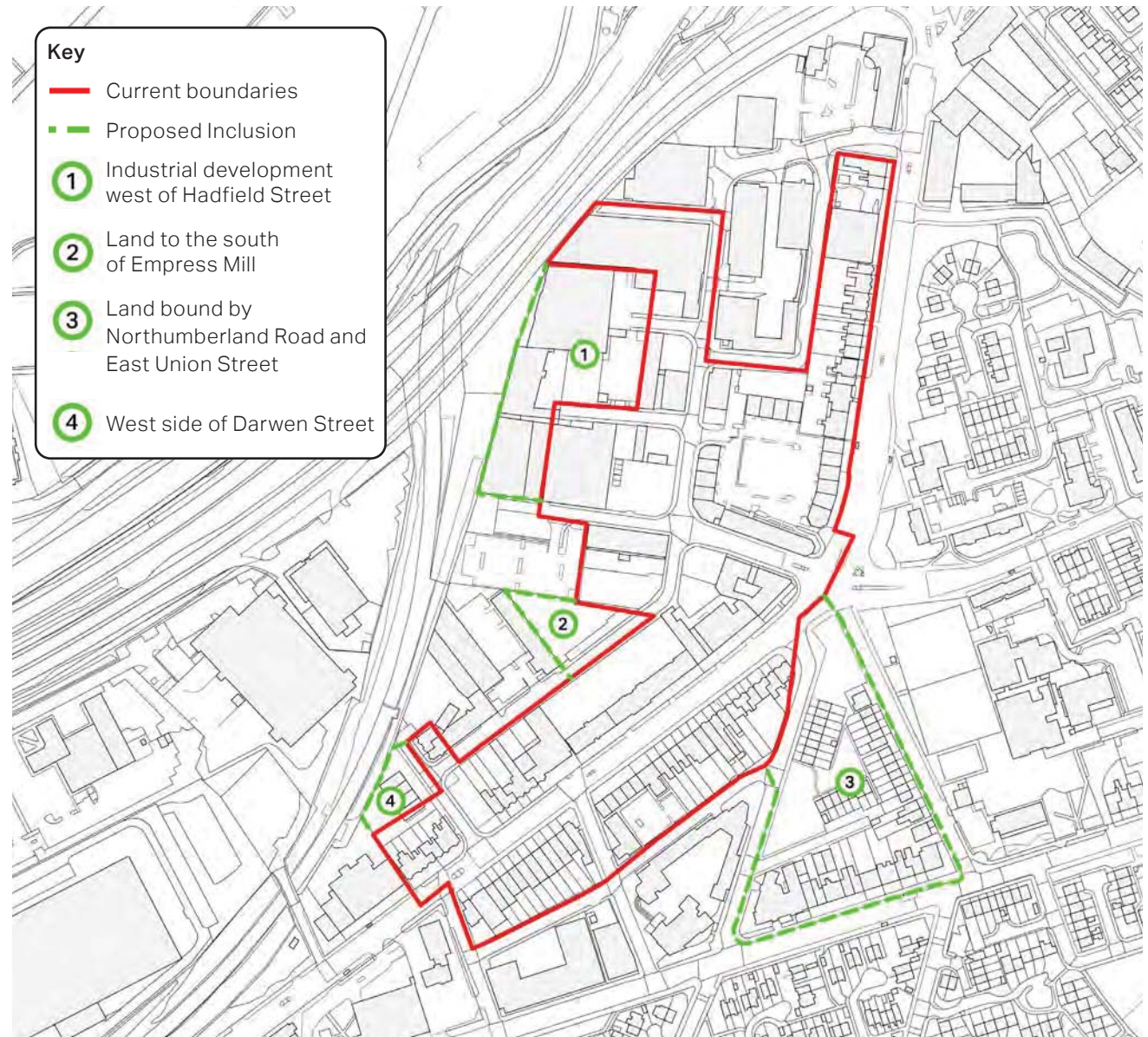
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## Executive Summary

The Empress Conservation Area was first designated in 1995 and was added to Historic England's Heritage at Risk Register in 2012. It was reviewed in 2016, as part of the preparation of its first Appraisal, which was adopted as SPD 5.17 in October 2016.

This document was prepared in 2024-25 as an update of the Conservation Area Appraisal and Management Plan undertaken by Purcell in 2016. An Appraisal of the conservation area is provided in Volume 1 and the Management Plan in Volume 2.

For ease of reference, the Townscape and Character Area Maps of Volumes I and II are also reproduced separately in Appendix A, with the list of heritage assets. The criteria for the identification of positive contributors is provided in Appendix B. An audit of the heritage assets contained within the current conservation area and within the proposed extensions to its boundaries is provided in Appendix C. Control measures brought about by designation follow in Appendix D, alongside the provisions of a proposed

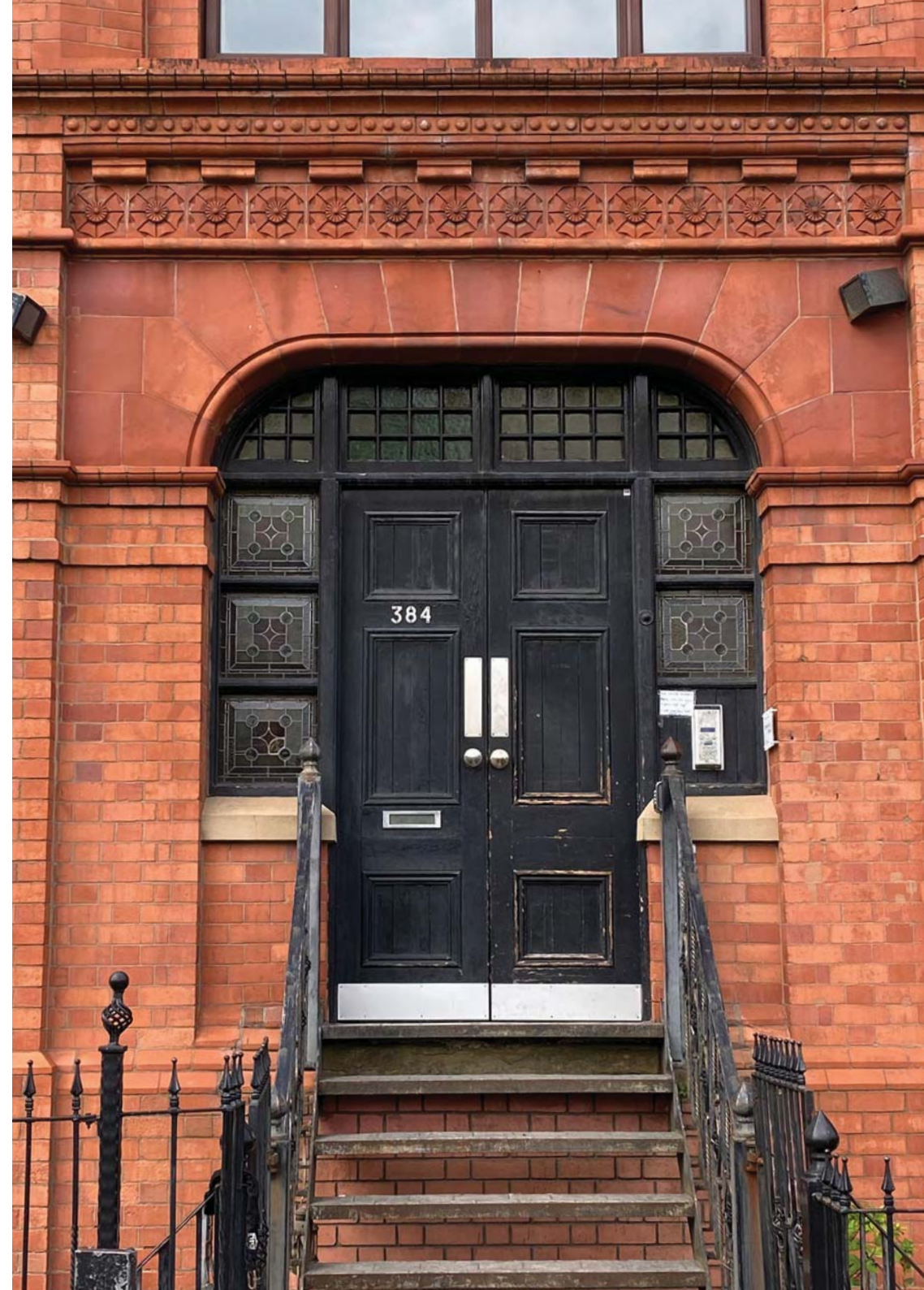


1.1 Current and Proposed Conservation Area Boundaries

new Article 4 Direction. Finally, Appendix E provides further links and a glossary of heritage and architectural terms.

To ensure that its contents are as accessible as possible, the digital version of this document includes elements such as easily navigable links between sections.

The document has been commissioned by Trafford Council following joint funding from Trafford Council, Historic England and Homes England for the production of a Conservation Area Regeneration Plan (CARP) for the area, delivered by Donald Insall Associates, LDA Design and Lambert Smith Hampton. The CARP aims to build on what is special about the area to positively leverage the historic environment in the area's regeneration and placemaking, enhancing the positive impact that heritage can have on people's wellbeing and sense of place. The broader regeneration plan is underpinned by reports and evidence documents including the Conservation Area Appraisal and Management Plan (CAAMP).



# 1 Introduction





# Volume 1 Appraisal

## 1 Introduction

### 1.1 Statement of Special Interest

1.1.1 The Empress Conservation Area draws its principal interest as a mixed residential, industrial and commercial area that developed along the spine of Chester Road from the mid-19<sup>th</sup> century onwards. It forms a key part of the broader growth of Stretford, Trafford and Manchester in the industrial booms of the 18<sup>th</sup> and 19<sup>th</sup> centuries.

1.1.2 This interest is reflected in its character and appearance, which include its visual variety, the underlying visual cohesion and shared materiality of red brick, slate and terracotta that unites it, and its surviving historic development plan.

1.1.3 The area comprising the Empress Conservation Area (and its proposed additions) has survived as an island of historic character amongst a sea of change. This almost chance survival (prior to its designation as a conservation area) of a character that might otherwise

been swept away has allowed the Empress area to survive as an essentially Victorian and Edwardian industrial suburb that preserves and reflects all periods of Trafford's development from the early 1800s onwards. It does this modestly and without fanfare and the buildings of each of these periods also tell the story of their own particular stage of the area's social and economic evolution.

1.1.4 Thus, Empress combines evidence of both the first early-19<sup>th</sup>-century upper-middle-class enclave and the subsequent Victorian industrial powerhouse that now housed its own factory foremen and office workers and a new invention of the Victorian period – commuters. It is this character introduced in the rapid and all-embracing changes of the Victorian period that have created the Empress that exists today.

1.1.5 Here, rows of terraced housing front onto the principal road and showpiece factory buildings, showrooms and offices lead to a grid-like industrial heartland

behind, all backing onto those railway tracks – the Manchester Ship Canal behind – that made this transformation possible. Empress proudly preserves the industrial might that created Trafford as a modern borough, and it continues this industrial activity into the present day.

1.1.6 Empress is also a small area of large contrasts: the small scale of its traditional terraced houses set against its substantial landmark industrial buildings; the grandness of these landmark buildings highlighted by the much more functional architecture of the manufactories behind; and the wide expanse of Chester Road leading into the shorter, narrower, side and back streets behind.

1.1.7 Beneath this variety, however, lies an essential unity of architectural character, and it is this unity that is the special character of Empress. Although built in different periods and for different occupants, the houses are united by their coherency of form, the regularity in their height, scale, styles and

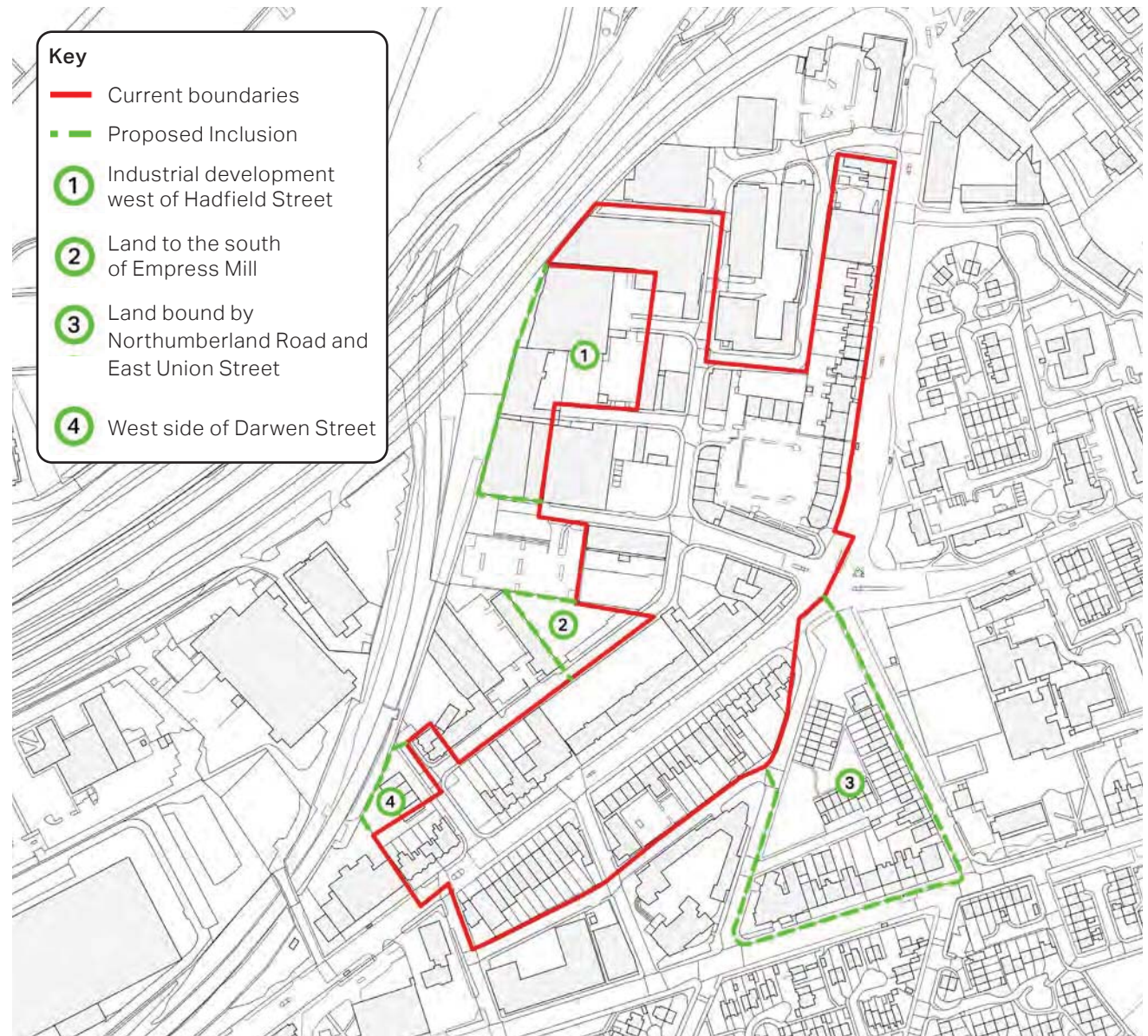
materials creating an evocative built landscape of red brick, stone detailing and slate-covered pitched roofs. The industrial buildings are similarly united by their red brick, their detailing of brick, stone and terracotta, and the machine-like regularity of their windows, the showpieces combining this with additional ornamentation, interesting roof forms and lines, and matching boundary walls. Modern development continues to respect this established form, and Empress's variety, giving new life to that distinctive dual combination of domestic and industrial spaces established during the Victorian period.

Buildings, the Essence Factory and the former Empress Brewery continue to define the Empress area as the source of its identity and the epitome of its character. They are at its visual heart, the focal point of the area, and mark and proudly announce the visitor's arrival in Empress as something special.

- 1.1.8 At the centre and heart of Empress is its landmark industrial and commercial buildings. Their occupants created their corporate identity around these buildings and placed them at the centre of their advertising materials with pride, causing the architecture of Empress to become known to audiences across Britain and beyond. Their occupants may have changed but the Trafford Press and Veno

## 1.2 Introduction

- 1.2.1 This Appraisal and Management Plan form part of a suite of documents relating to the Empress Conservation Area within the Metropolitan Borough of Trafford and also including a Design Code [Plate 1.1].
- 1.2.2 First designated in 1995, the conservation area was added to Historic England's Heritage at Risk Register in 2012. It was reviewed in 2016, as part of the preparation of its first Appraisal. This first Appraisal was adopted as SPD 5.17 in October 2016.
- 1.2.3 These documents are an update of reports prepared by Purcell in 2016. It has been commissioned following joint funding from Trafford Metropolitan Borough Council, Historic England and Homes England to produce a Conservation Area Regeneration Plan (CARP) for the area, which has been delivered by Donald Insall Associates, LDA Design and Lambert Smith Hampton.



1.1 Current and Proposed Conservation Area Boundaries

## 1.3 Planning Policy Context

1.3.1 A conservation area is an *'area of special architectural or historic interest, the character and appearance of which it is desirable to preserve or enhance'* (section 69 of the Planning (Listed Buildings and Conservation Areas) Act 1990). Designation takes place primarily by local planning authorities under Section 69 of the Planning (Listed Buildings and Conservation Areas) Act 1990 and recognises the distinctive townscape of an area, including its buildings, streets, and public realm.

1.3.2 Paragraph 204 of the National Planning Policy Framework (NPPF) states that *'when considering the designation of conservation areas, local planning authorities should ensure that an area justifies such status because of its special architectural or historic interest and that the concept of conservation is not devalued through the designation of areas that lack special interest'*.

1.3.3 Local planning authorities also have a duty from time to time to review the extent of designation and to designate further areas if appropriate (Section 69 of the Act). Section 71 of the Act imposes a duty on the local planning authority to formulate and publish proposals for the preservation and enhancement of conservation areas. Proposals should be publicised and should incorporate public comment.

1.3.4 Section 72 of the Act requires local planning authorities to pay special attention to the desirability of preserving or enhancing the character or appearance of a conservation area when exercising their planning functions. This includes policies, development control decisions, enforcement, use of their highway powers, and the application of controls relating to advertisements, trees and buildings requiring maintenance.

1.3.5 Once designated, conservation areas are considered as *'designated heritage assets'*. Paragraph 205 of the NPPF states that *'when*

*considering the impact of a proposed development on the significance of a designated heritage asset, great weight should be given to the asset's conservation (and the more important the asset, the greater the weight should be). This is irrespective of whether any potential harm amounts to substantial harm, total loss or less than substantial harm to its significance. Under Paragraph 206 of the NPPF 'any harm to, or loss of, the significance of a designated heritage asset (from its alteration or destruction, or from development within its setting), should require clear and convincing justification'.*

1.3.6 Consequently, any proposals for change or development must assess the effect that they might have on the character and appearance of the area. This is reinforced by local policy including The Core Strategy (2012) and Revised Unitary Development Plan (2006), which at the time of writing (2024-25) are being replaced with the Trafford Local Plan, and the recently adopted

Greater Manchester Places For Everyone Joint Development Plan Document (2023).

- 1.3.7 Planning policy also affords a degree of protection to heritage assets of other kinds. The NPPF defines a heritage asset as 'a building, monument, site, place, area or landscape identified as having a degree of significance meriting consideration in planning decisions, because of its heritage interest. It includes designated heritage assets and assets identified by the local planning authority (including local listing)'.
- 1.3.8 Designated heritage assets are defined in the NPPF as '*a World Heritage Site, Scheduled Monument, Listed Building, Protected Wreck Site, Registered Park and Garden, Registered Battlefield or Conservation Area designated under the relevant legislation*' (see Annex 2).
- 1.3.9 By contrast, the guidance to the NPPF defines non-designated heritage assets as '*buildings,*

*monuments, sites, places, areas or landscapes identified by plan-making bodies as having a degree of heritage significance meriting consideration in planning decisions but which do not meet the criteria for designated heritage*'. Non-designated heritage assets may be identified by a range of means, including, but not limited to, local listing, the local and neighbourhood plan-making processes, and conservation area appraisals and reviews. For the purposes of this Appraisal all positive contributors – buildings that make a positive contribution to the conservation area – are considered to be non-designated heritage assets.

## 1.4 Location

- 1.4.1 The Empress Conservation Area is located in the north of the Old Trafford area of Trafford Borough (itself part of Greater Manchester). The boundary between the Borough of Trafford and the City of Manchester is at the immediate north of the conservation area, running to the immediate north of Cornbrook Road, crossing Chester Road and continuing along Princess Street and the rear of Virgil Street. Stretford is situated some 2.5 miles to the southwest and the centre of the city of Manchester approximately 1.5 miles to the northeast.
- 1.4.2 The western edge of the conservation area is bound by the A56 (Bridgewater Way), to the immediate west of which are situated the railway and Metrolink lines and the Bridgewater Canal. Major junctions of the A56 are positioned to both southwest and the immediate northeast of the conservation area, the A56 a key route into Manchester from the

M56 and M60 and the boroughs to the southwest, these arterial roads connecting the centre of the City of Manchester with the towns to the west. Cornbrook Metrolink Station is positioned a short distance to the northwest, on the opposite side of the A56.

- 1.4.3 Empress is the most northeasterly of Trafford's conservation areas and the closest to central Manchester. Its wider setting is urban, combining industry, commercial buildings and residential estates. Development pressure has increased across the past decade and the southern edge of the City of Manchester to the immediate north of the conservation area is now dominated by the busy road junction and a number of mid-rise apartment blocks, with further high-rise blocks visible across the horizon to the city centre in the north.
- 1.4.4 There are no other conservation areas within the vicinity of Empress.

## 1.5 General Description and Uses

- 1.5.1 Empress Conservation Area runs along the spine of Chester Road, primarily along its western side, where it projects into the industrial area behind. A section of housing on the southeast side of Chester Road is also included (comprised of two terraces), the remainder of this side being comprised of modern development. The conservation area's landmark buildings – all of an industrial/commercial character – are mainly clustered around its main junction, situated at the intersection of Chester Road with City Road and East Union Street (to the east) and Empress Street (to the west). Some gap sites to Chester Road are the believed result of Second World War bomb damage and associated clearance, where the ends of terraces have been lost.
- 1.5.2 Chester Road is a heavily trafficked A-road that connects with the A56 further to the north. Side roads run off to the west/northwest and east/southeast at irregular intervals, those on the west running into the

industrial area and those to the east connecting with Stretford Road, also a secondary A-road.

- 1.5.3 The historic buildings of the conservation area are primarily early/mid and late Victorian, with some industrial buildings of mid- and late- 20<sup>th</sup> century dates. They combine terraced housing of varied styles with industrial buildings of mixed character and scale. Red brick, sometimes in conjunction with terracotta, is the predominant historic building material, although there is increasing application of render to the rear of dwelling buildings, which has impacted on character. Modern redevelopment and infill development is of a mixture of materials, scales and characters and is both residential and industrial. There are no public areas and little public soft landscaping, with the public realm generally poor.

1.5.4 The conservation area's dual industrial and residential use and associated mixture of built form is a primary component of its special interest. Although a number of the terraced dwellings have now been converted to office-based or light industrial commercial uses they still retain all or most of their domestic character, particularly to the front.

## 1.6 Aims of the Appraisal

1.6.1 This Appraisal is informed by research and supported by public engagement undertaken in spring 2024 and site surveys completed in 2024-25. It aims to:

- Stimulate interest in the care and management of the local historic environment.
- Enable Trafford Council, stakeholders, and local people to work collaboratively to manage change to the historic environment in an informed manner in both the immediate and longer terms.
- Support improvements to those elements that detract from its character and appearance, for the conservation area to be removed from the Heritage at Risk Register, as part of its longer-term revitalisation.

## 2 Significance





## 2 Significance

### 2.1 Special Interest

2.1.1 The Empress Conservation Area has its principal heritage interest as a mixed residential, industrial, and commercial area that developed along the spine of Chester Road from the mid-19<sup>th</sup> century onwards. It formed a key part of the broader growth of Stretford, Trafford and Manchester in the industrial booms of the 18<sup>th</sup> and 19<sup>th</sup> century.

2.1.2 This historic context is evident in the immediate locality in infrastructure such as the Bridgewater Canal (1765), the Manchester South Junction and Altrincham Railway (1840s) and the Manchester Ship Canal (1890s). It is also expressed in the presence of housing accommodating the middle classes who were escaping the city smog (although the housing that subsequently accommodated the area's growing population of workers has been lost).

2.1.3 This interest is reflected in the conservation area's

character and appearance, which include:

- **Visual Variety:** The area has a historically important mixed form. This includes the close proximity of small-scale two-storey terraces retaining their domestic character despite commercial use, and larger-scale commercial and industrial buildings with a mixture of functional and decorative designs based on their historic uses.
- **Visual Cohesion:** Despite the variations in scale and design, there is an overarching commonality in the built form, defined by the common use of red brick, with detailing in brick, terracotta and stone, which creates a distinct reddish hue across the area.
- **Development Pattern:** The area retains a development pattern centred on the linear spine of Chester Road branching off to the northwest into areas of industrial use via two important nodal junctions. The building line to Chester Road is strong, creating cohesive and generally enclosed sweeps of domestic and commercial forms, reinforced by low brick and stone boundary walls and their railings. Where the grain is finer, to the terrace groups, there is an overall repetition of materials and features,

although with subtle variation between and even within individual terraces. Even to the northwest, where the townscape is of a looser grain – reflecting its ad hoc uses and the loss of earlier, dense, terracing – there remain important forms, including low-lying long red brick industrial blocks. There is also some variation in the development pattern to the industrial areas, the mass and scale at its greatest to the landmark buildings to Chester Road and its junction with Empress Street and falling to the north and west. This creates distinct character areas within the industrial core.

2.1.4 The area comprising the Empress Conservation Area (and its proposed additions) has survived as an island of historic character amongst a sea of change. This almost chance survival (prior to its designation as a conservation area) of a character that might otherwise been swept away has allowed the Empress area to remain an essentially Victorian and Edwardian industrial suburb that preserves and reflects all periods of Trafford's development from the early 1800s onwards. It does this modestly and without fanfare. There is no

sudden arrival at the conservation area and its special character reveals itself only gradually, in the realisation of its contrast from the thoroughly modern built landscapes surrounding. It is only on reaching the heart of the conservation area that the landmark buildings – the focal point of all its views – loudly announce the area’s distinctive identity and special character.

2.1.5 In Empress all periods and phases of Trafford’s modern development are visible. Here, within a relatively small area, the Georgian, the early, mid and late Victorian, the Edwardian and the interwar, the post-war, and the modern sit side-by-side.

2.1.6 Buildings of each of these periods also tell the story of their own particular stage of the area’s social and economic evolution from an early-19<sup>th</sup>-century upper-middle-class enclave to a Victorian industrial powerhouse housing not only its own factory foremen and office workers but also that new

breed of professionals, which by the 1890s was commuting into the growing city.

2.1.7 It is this character introduced in the rapid and all-embracing changes of the Victorian period that have created the Empress that exists today. Here, rows of terraced housing front onto the principal road and showpiece factory buildings, showrooms and offices lead to a grid-like industrial heartland behind, all backing onto those railway tracks – the Manchester Ship Canal behind – that made this transformation possible. Empress proudly preserves the industrial might that created Trafford as a modern borough and continues this industrial activity into the present day.

2.1.8 Empress is a small area of large contrasts: the small scale of its traditional terraced houses set against its substantial landmark industrial buildings; the grandness of these landmark buildings highlighted by the much more functional architecture of the manufactories behind; and the wide

expanse of Chester Road leading into the short side streets. On first glance its houses might appear fairly uniform but a closer inspection rewards the viewer with a range of decorative details and materials, whilst Empress’s historic industrial sites span the full range from small manufactories to large factories and mills and the even larger industrial and commercial showpieces.

2.1.9 A corresponding variety of architectural character underlies this contrast, its showpiece buildings – an expression of company pride and ambition – each adopting the styles and materials of its own time, creating a side-by-side montage of British high-end industrial architecture across a period spanning a century and a half. To their rear, set back from the main road, lie the more functional manufacturing buildings, some with archetypal historic industrial features such as loading doors, sawtooth roofs and red brick chimneys.

2.1.10 Beneath this variety, however, lies an essential unity of architectural character, and it is this unity that is the special character of Empress. Although built in different periods and for different occupants, the houses are united by their coherency of form, the regularity in their height, scale, styles and materials creating an evocative built landscape of red brick, stone and brick detailing and slate-covered pitched roofs. Historic walls and gatepiers, occasional surviving sash windows, and green gardens to the front all create a distinctive residential character that preserves the memory of past occupants and those historic communities that made Empress their home previously.

2.1.11 The industrial buildings are similarly united by their red brick, their detailing of brick, stone and terracotta, and the machine-like regularity of their windows, the showpieces combining this with additional ornamentation, interesting roof forms and lines, and matching boundary walls. Modern development continues to respect this established form, and Empress's variety, giving new life to that distinctive dual combination of domestic and industrial spaces established during the Victorian and Edwardian periods.

2.1.12 At the centre and heart of Empress is its landmark industrial and commercial buildings. Their occupants created their corporate identity around these buildings and

placed them at the centre of their advertising materials with pride, causing the architecture of Empress to become known to audiences across Britain and beyond. Their occupants may have changed but the Trafford Press and Veno Buildings, the Essence Factory and the former Empress Brewery continue to define the Empress area as the source of its identity and the epitome of its character. They are at its visual heart, the focal point of the area, and mark and proudly announce the visitor's arrival in Empress as something special.

## 2.2 Views into and Out of the Conservation Area

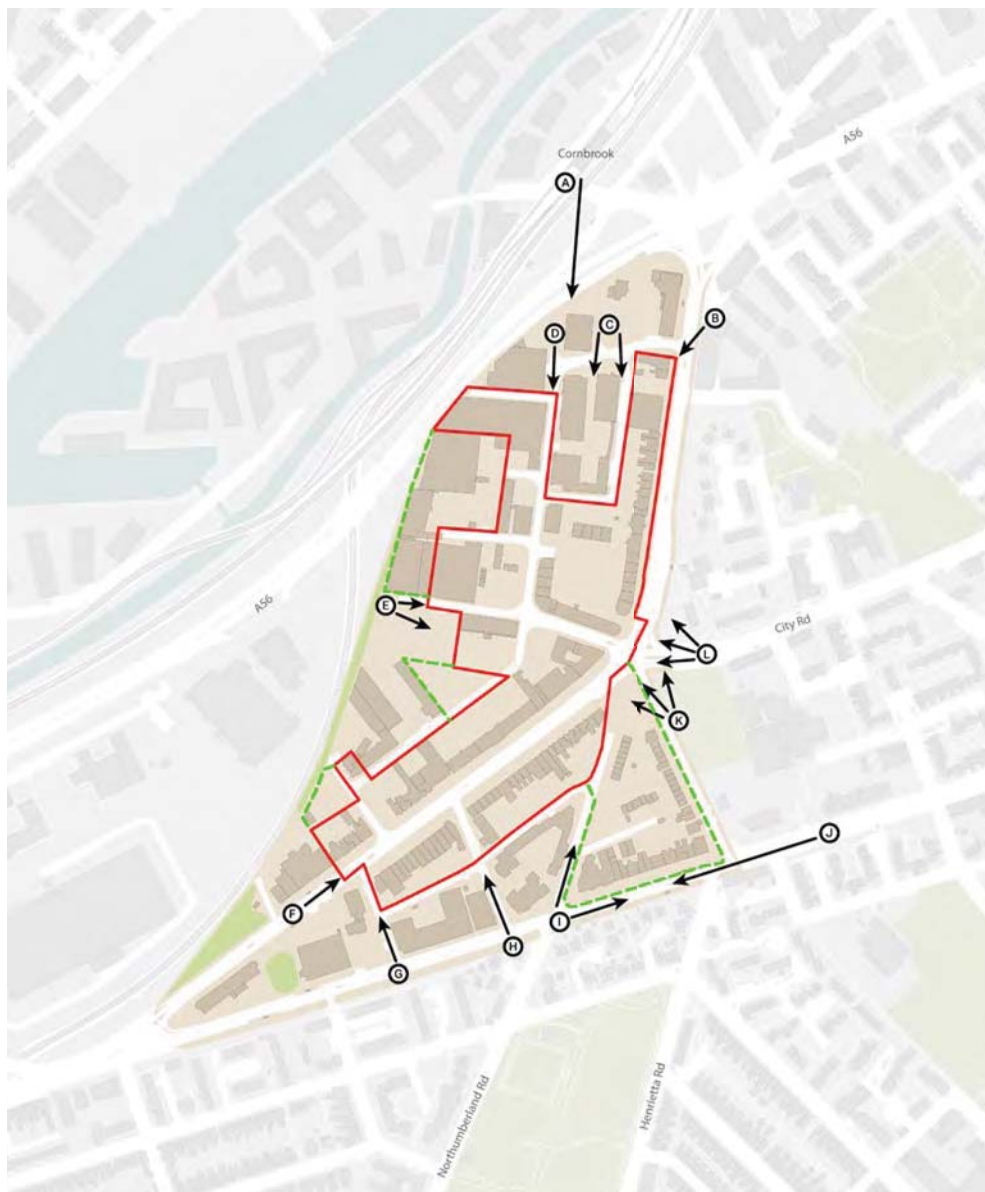
2.2.1 The area's setting has been altered substantially. Its immediate and longer-range surrounding are now predominantly comprised of modern development, which has isolated it from important historically connected areas such as Pomona Docks. There is, furthermore, no visual relationship between the conservation area and the Bridgewater Canal, Pomona Docks and railway that gave rise to the area's origins and, into the 20<sup>th</sup> century, its continued industrial activity (road transport subsequently rising to the fore).

### Views into the Conservation Area

2.2.2 Approaches into the conservation area are comparatively short, in part due to the built-up character of the area, whilst the entry points into the conservation area are typically understated [Plate 2.1]. There is only one long-range view into the conservation area, which is provided from the elevated platform at Cornbrook Metrolink Station (View A). This reveals the industrial buildings of Character Area B, plus several of the landmark buildings, with a distinctive roofscape and brown brick materiality, with several features puncturing the skyline [Plate 2.2].

2.2.3 Otherwise, those views from outside the boundaries that contribute to the special interest of the conservation area are short range (which mirrors and enhances those inside the conservation area, where views of the landmark buildings in particular are entirely revealed and/or dynamic). These short-range views from outside the conservation area are concentrated around six viewing places:

- from the northeast entry point into the conservation area (Hadfield Street and Chester Road, **View B**) [Plate 2.3]
- from the immediate northeast (Hadfield Street, **Views C and D**) [Plates 2.4-2.5]
- from Empress Street, looking east (**View E**) [Plate 2.6]
- from Chester Road, at the immediate south of the conservation area (**View F**) [Plate 2.7]
- from Stretford Road, looking northwest along Nuttall Street and Chorlton Street (**Views G and H**) [Plates 2.8-2.9]
- from the mouths of East Union Street and City Road, at the immediate east of the conservation area (**Views K and L**) [Plates 2.10-2.11]



2.1 Key Views into the Conservation Area and Proposed Extensions

**Key**

- (A) Elevated view from Cornbrook Metrolink station
- (B) Views of the front and side of Cornbrook Terrace from the northeast
- (C) Glimpsed views towards the landmark buildings from the north
- (D) Views from the north, Hadfield Street buildings in the foreground and landmark buildings beyond
- (E) Empress Mill and other landmark buildings beyond, in short views along Empress Street
- (F) Views of the small-scale terraces to Chester Road, the Essence Factory rising behind, all capped by the high-rise blocks of Manchester in the distance
- (G) Narrow view west along Nuttall Street showing the small scale terraces
- (H) Narrow view west along Chorlton Street showing the small-scale terraces
- (I) Former Technical Institute and houses adjacent (proposed extension), from the immediate south/southwest
- (J) Former Technical Institute and houses adjacent (proposed extension), from the south
- (K) Views from East Union Street of the ensemble of landmark buildings to Chester Road, framing the sawtooth roofs of Empress Street
- (L) Views from the City Road of the ensemble of landmark buildings to Chester Road, framing the sawtooth roofs of Empress Street



**2.2** View A, southeast into the conservation area (revealing landmark and other industrial buildings, with a distinctive roofscape)



**2.3** View B, southwest into the conservation area



**2.4** View C, south into the conservation area



**2.5** View D, south into the conservation area



**2.6** View E, east and southeast into the conservation area along Empress Street



**2.7** View F, north into the conservation area



**2.8** View G, looking northeast along the proposed extension



2.10 View K, west, southwest and northwest into the conservation area



2.11 View L, west, southwest and northwest into the conservation area



2.9 View H, north into the conservation area along Chorlton Street



2.12 View I, looking west through the proposed extension and east along its edge

2.2.4 Views along the Stretford Road frontage of the proposed extension to the southeast (Views I and J) also add to the area's historic and architectural interest through their distinct character. These provide views of comparatively ornate dwellings, juxtaposed and contrasting heavily with not only the modern development behind but also their own utilitarian rears, and those of the conservation area beyond **[Plates 2.12-2.14]**.

2.2.5 The conservation area's setting to the southeast is heavily altered and the narrow views of the Victorian terraces visible along the side streets contrast starkly with the modern development surrounding. Although not key views, these glimpses from the southern end of Chorlton Street and Nuttall Street highlight the conservation area's distinction from its immediate setting and, therefore, its special interest.

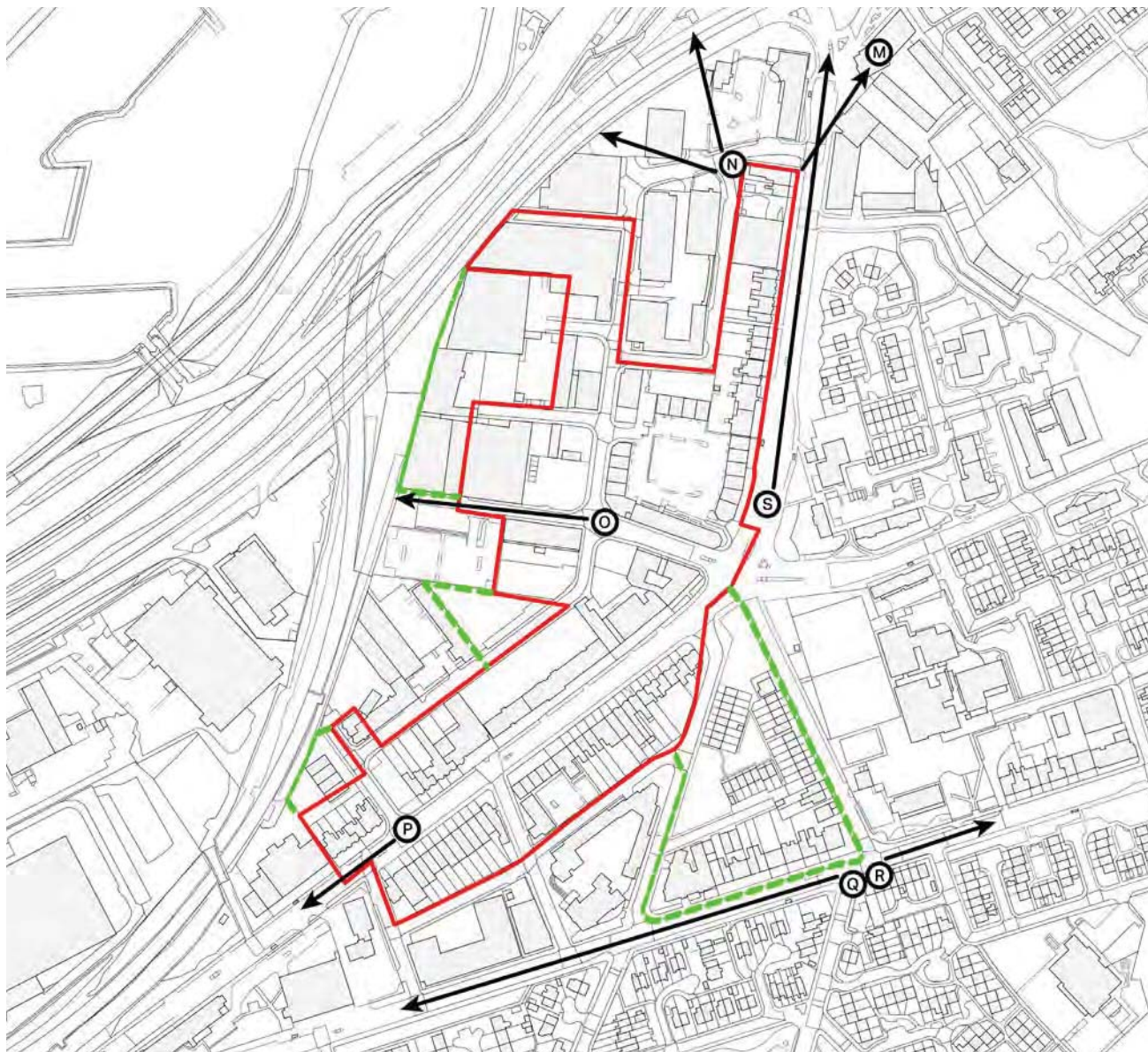


2.13 View I, looking east along the proposed extension



2.14 View J, west and northwest along the proposed extension





2.15 Key Views out of the Conservation Area and Proposed Extensions

## Views out of the Conservation Area

2.2.6 The key views out of the conservation area and its proposed extensions are generally weaker and less coherent, due to its degraded setting. They do not, therefore, add substantially to its special interest [Plate 2.15]. They include:

### Key

- (M)** Views of 255 Chester Road to the northeast and its red brick setting beyond the Manchester Arch, the Manchester Arch articulating the conservation area's position on the border of Trafford
- (N)** Green border to the northwest providing relief and signifying the presence of the railway, Metrolink and Bridgewater Canal
- (O)** Green border to the west providing relief and signifying the presence of the Bridgewater Canal
- (P)** Views of the altered setting of the conservation area to the south
- (Q)** High-rise modern development contrasting with the conservation area and framing the view to the southwest
- (R)** Avenue of trees framing views to the northeast, offset by tower blocks to the west
- (S)** Long views towards medium and high-rise development to the north, establishing a strong contrast with the conservation area

- views articulating the contrast between the built form of the conservation area and that of the surrounding (modern), development, particularly in the latter's mid- and high-rise blocks (**Views M, S and R**) [**Plates 2.16-2.18**]
- greenery to the immediate northwest and west, signifying the presence of the Bridgewater Canal and railway line adjacent, and the enduring enclosure and self-containment of the industrial area from its surroundings to the west (**Views N and O**) [**Plates 2.19-2.20**]

- other soft landscaping to the immediate setting, enhancing the visual amenity of the conservation area (View Q) [**Plate 2.21**]

2.2.7 Although too diminutive to be the dominant townscape feature, the red brick Victorian 252 Chester Road also has a presence in View M, framed by modern mid-rise development to either side. Of greater prominence is the intervening red/brown brick 'Manchester Arch', which articulates the area's strategically important location abutting the boundary of Trafford and Manchester (crucial to its origins and emergence as an industrial suburb) [**Plate 2.22**].



2.16 View M, views out to the northeast



2.17 View S, long view out to the north



2.18 View R, view to the northeast from the proposed extension



2.19 View N, view out to the northwest enclosed by trees



2.20 View O, tree screening to views to the west



2.21 View Q, soft landscaping framing the west of the view



2.22 Manchester Arch and 252 Chester Road featuring in View M

## 2.3 Setting

2.3.1 In the degree and pervasiveness of the change to the surrounding area the conservation area has lost much of its historic relationship with its setting. This erosion of the wider setting increases the importance of the small cluster of Victorian and Edwardian townscape surviving to the immediate east/northeast of the conservation area in the area between Northumberland Road, Stretford Road and East Union Street (a proposed extension to the conservation area). By contrast, the busy A56 creates something of a visual, conceptual and physical barrier to the immediate west and northwest, giving an almost island-like character to the conservation area.

2.3.2 Although itself now mainly heavily altered in character, this wider surrounding area provides important dynamic views into and out of the conservation area that give increased legibility to the area's history. These views also provide visual framing to the architecture

of the conservation area. This enhances, and allows appreciation of, its continued visual, spatial, architectural and historic interest.

2.3.3 Both the immediate and wider surroundings of the conservation area are characterised by their primarily modern built form. This incorporates low-, mid- and high-rise buildings of a variety of styles, materials and scales, which share little overall coherency or cohesion. These buildings have a high presence in views into, across and out of the conservation area and along its edges and a strong visual impact on the conservation area itself. The taller buildings crowning the horizon are particularly prominent, being visible not only from the interior of the conservation area but also in views across (and beyond) it from area outside **[Plates 2.23-2.27]**.



2.23 Mixed modern built form to the west, north and northeast, visible beyond the former Technical Institute



2.24 Modern mid- and high-rise buildings in views looking east



**2.25** Modern high-rise building terminating linear views of the landmark buildings



**2.26** High-rise buildings to the north



**2.27** Contemporary tower blocks visible to the northeast of the conservation area

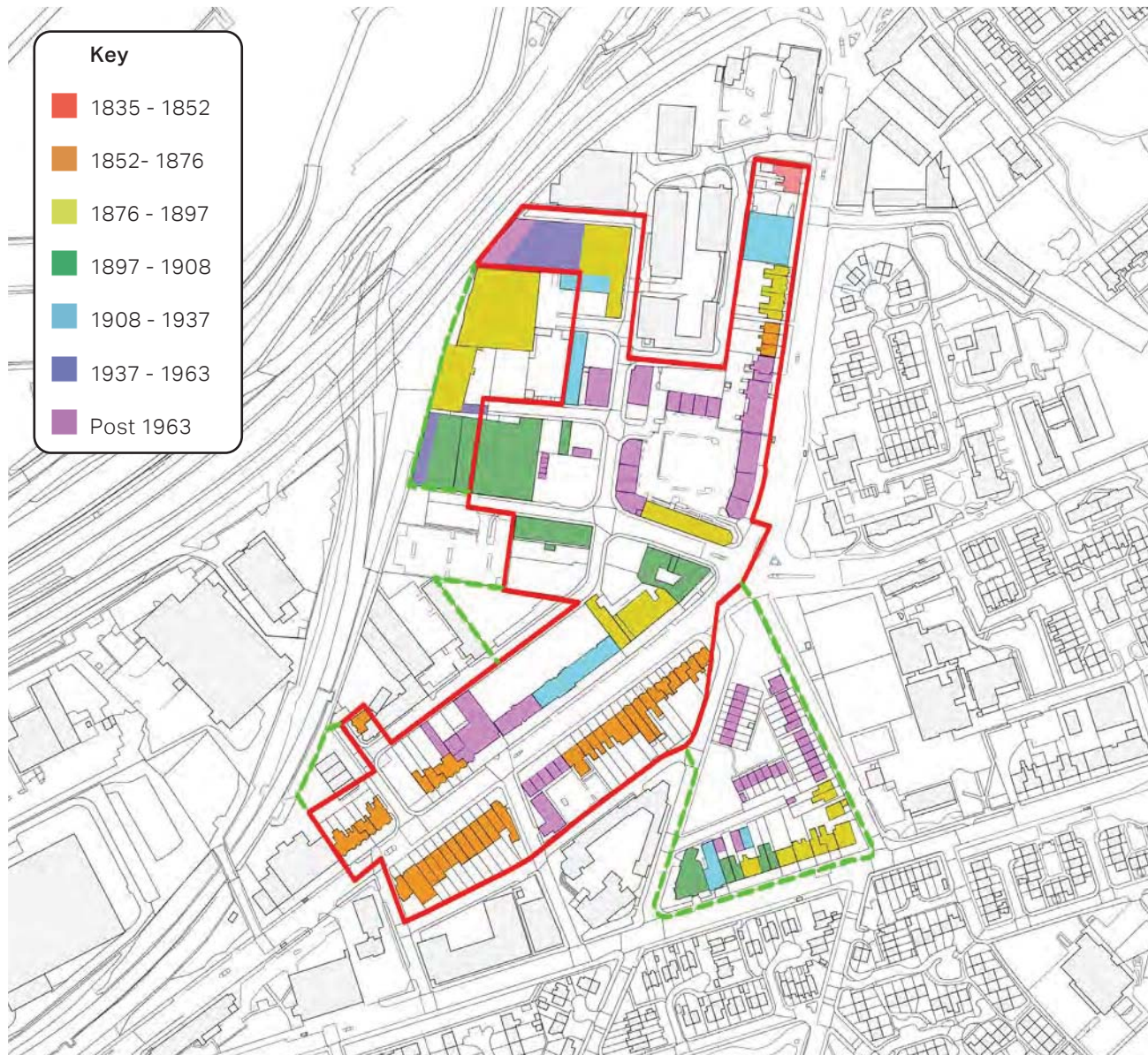
## 2.4 Issues

- 2.4.1 The area has been on Historic England's Heritage At Risk Register since 2012. At the heart of the conservation area, its landmark buildings are suffering from a high level of vacancy, fire damage and deterioration in built fabric, all further exacerbated by stalled development, which has left a physical and visual legacy of decline.
- 2.4.2 Across the conservation area, key issues include small-scale incremental change such as the loss of features including windows, doors and roofing materials and the intermittent loss of boundary treatments as well as poor-quality changes and alterations to prominent rear and side elevations. This coincides with the visual remnants of 20<sup>th</sup> century wartime damage and clearance, which have left gaps in the street scene, eroding its former cohesiveness and interrupting its strong building line.
- 2.4.3 Modern mid-rise development to Chester Road that is larger in scale than the traditional terraces that it has replaced has also had an impact. This has disrupted the historic grain and development pattern and undermined the prevailing character. There is a significant pressure on the conservation area for taller and larger-scale buildings.
- 2.4.4 More broadly, the area has now lost most of its private green spaces, caused by the hard landscaping of gardens for car parking as part of both the commercial use of residential buildings and their extension and subdivision as apartments. This has been further exacerbated by the addition modern development to Chester Road, which has had a notable visual impact, although, positively, several parks and school playing fields are present in the immediate surroundings.
- 2.4.5 Other issues include poor public realm, poor-quality signage, unauthorised advertisements, and problems with fly tipping and abandoned vehicles.
- 2.4.6 Outside of the conservation area, the large, intrusive, apartment blocks on its edge have also transformed its immediate and wider settings. These dominate the skyline and are overly predominant in views into, through and out of the conservation area.

### 3 Historic interest



## 3 Historic Interest



3.1 Primary Historic Phases

### 3.1 Introduction

- 3.1.1 The Empress Conservation Area lies within the wider area of Stretford and historically belonged to the township of Trafford, situated in Stretford's northeast. The histories of Trafford and Stretford have been closely linked since their emergence in the medieval period.
- 3.1.2 The conservation's area's historic buildings, however, date from the 19<sup>th</sup> and early 20<sup>th</sup> centuries, with phases of residential development coinciding with industrial expansion.
- 3.1.3 The area has also witnessed significant infill development and alteration subsequently, primarily reusing sites that suffered damage during the Second World War **[Plate 3.1]**.



## 3.2 Early History

- 3.2.1 The site of the Empress Conservation Area lay to the north of the important Roman road between the forts at Chester and Manchester, known respectively as Deva and Mancunium. Little evidence of early human activity has been found in the area, however, and it is probable that it remained undeveloped for much of its early history, perhaps as agricultural land.
- 3.2.2 Later, in the early medieval (Anglo-Saxon) period, the area may have been part of the parcel of lands received by Hamon de Massy, of the Massey family of Sale and later Dunham Massey, although this has not been confirmed. It is likely that in these later centuries the site continued mainly undeveloped, perhaps still in agricultural use.

- 3.2.3 The first consequential event for the Empress Conservation Area was the 'cutting' of the Duke of Bridgewater's Canal through the Stretford area in 1765. This brought a degree of development to Stretford but did not fundamentally alter the use of land. It was instead rural and agricultural activity that was intensified as market gardening grew to feed the growing industrial city of Manchester to the northeast. The Bridgewater Canal was an important part of the growth of this trade, allowing the easy transportation of goods directly to the city's markets in exchange for the city's night soil (an important source of fertiliser).



### 3.3 19<sup>th</sup> and 20<sup>th</sup> Centuries

3.3.1 The real development of the Empress area began only with the Victorian period. As in the 18<sup>th</sup> century, this change also came about because of new transport links, this time due to the construction of the Manchester South Junction and Altrincham Railway in the 1840s. The railway had important implications for the residential development of Stretford, bringing growing numbers of the middle classes out of the smog and dirt of the city to live in the clean-air idyll of what was still the surrounding countryside.

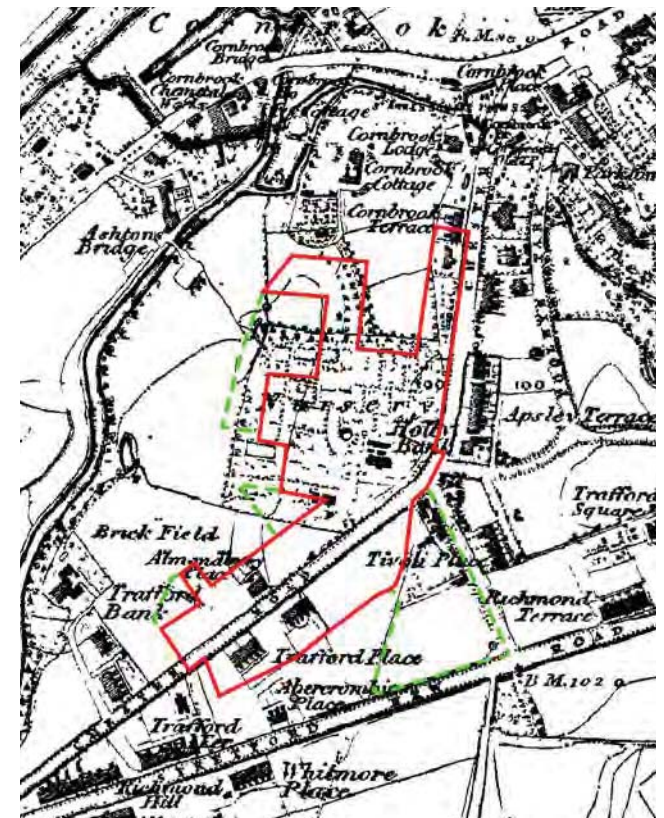
3.3.2 The earliest surviving buildings in the Empress Conservation Area are residential. The two semi-detached buildings on Chester Road at the north end of the conservation area are the surviving half of the mid-19<sup>th</sup>-century Cornbrook Terrace, the southern half having been razed either during or soon after the Second World War. Birkdale Terrace and Chester Terrace to the south

date from the late 19<sup>th</sup> century, Chester Terrace appearing to have been built as a close copy.

3.3.3 Development at the southern end of the conservation area echoes the timeframe of that in the north: small sections of the terraces facing onto Chester Road here were built in the mid-19<sup>th</sup> century, with the remainder completed by the end of the century, mainly by the 1880s. Residential development also continued along parallel streets to the northwest of Chester Road (although these were cleared in the 20<sup>th</sup> century).

3.3.4 The properties built on Chester Road at this time had a clear spatial quality, incorporating front and rear gardens, and were a mixture of short terraced rows, purpose-built terraces, and semi-detached villas, all designed for the lower middle classes. Those that developed along Turner, Leak and Harold Streets to the northwest, interspersed with the newly emerging industrial areas, were, by contrast intended for the working classes employed in the new

factories and works. Consequently, they were much denser and more urban in character, and were built hard to the pavement, with only small rear yards [Plate 3.2].



3.2 1848 OS Map

3.3.5 Early industrial development was concentrated in the northwest part of the conservation area, with works constructed to the side the railway network and the prominent Empress Brewery built on Empress Street (by the 1880s). One of the most prominent buildings in the area – the Essence Factory for Duckworths – was also part of this wider developmental trend, with the first building on its site erected in 1896. It was designed by the Blackburn architectural firm Briggs and Wolstenholme.

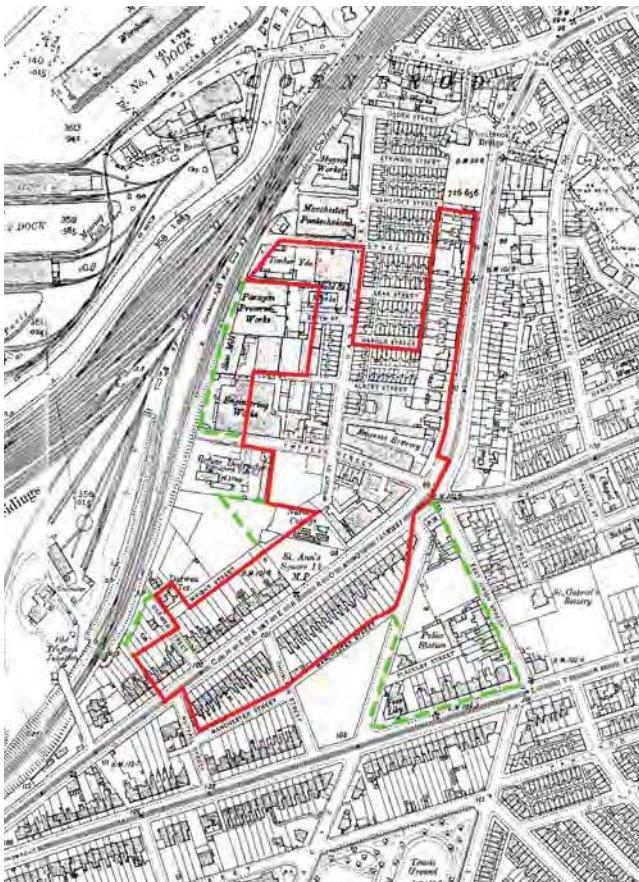
3.3.6 Also of significance for the Empress Conservation Area was the 1890s arrival of the Manchester Ship Canal, which stimulated the rapid industrial development of outlying areas, particularly along the immediate canalside. This effect was encouraged by the lack of land in the already built-up city for new large-scale works and was fed by the ease with which middle-class workers were now able to travel between Manchester and its new suburbs, using the new railway.

3.3.7 This development is evident in the 1896 Ordnance Survey map, which shows the Pomona Docks just to the north of the conservation area and several industrial works in the surrounding area.

3.3.8 Industrial development continued from the late 19<sup>th</sup> to early 20<sup>th</sup> centuries with further large-scale manufactories added along Chester Road and Empress Street that included the prominent Empress Mill and the National Works (both by 1908). This coincided with expansion to the industrial areas to the northwest of Chester Road, along Hadfield Road. Here a number of works of very varied scales were in place by the early 1930s, ranging from laboratories for the drug company Veno to timber yards and engineering premises and the manufacturers of preserves, tools, ‘smallwares’ (pots and pans) and biscuits.

3.3.9 Veno in particular had an important influence on the area in the early 20<sup>th</sup> century, its first works on Chester Road replaced by a new red brick and terracotta building in the

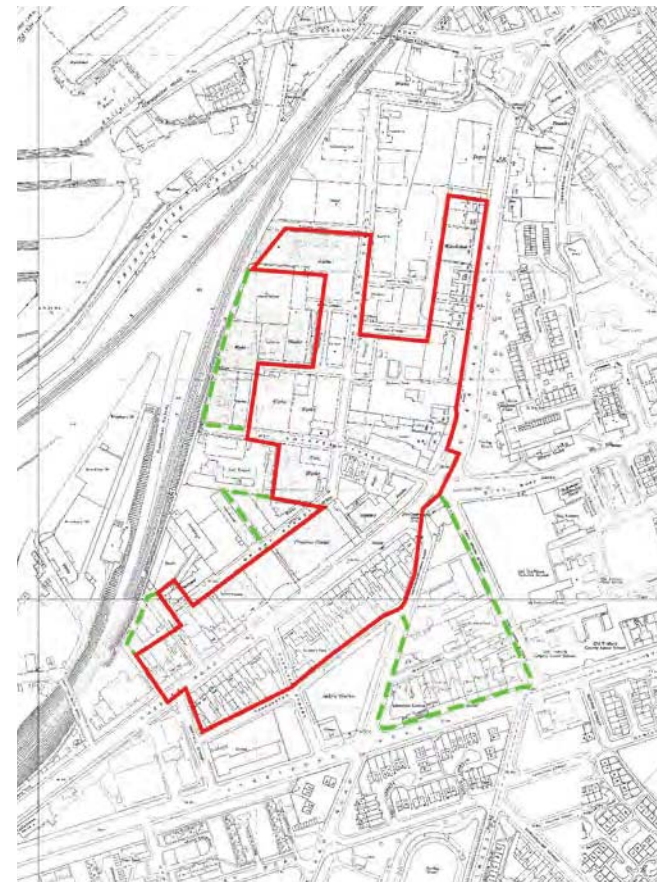
early 20<sup>th</sup> century. This was itself replaced on the adjoining site in the interwar period (the earlier premises becoming offices of Trafford Press), the art deco influences and now white façade of this 1930s building standing in positive contrast to the dark red hues of the earlier Essence Factory and other surrounding buildings **[Plates 3.3–3.5]**.



3.3 1908 OS Map



3.4 1931-1932 OS Map



3.5 1977-1982 OS Map

3.3.10 With the area by this time heavily developed, new residential development was very limited in the 20<sup>th</sup> century, the terraced streets to the northwest of Chester Road actually being cleared in order to be replaced by industrial units.

3.3.11 As industrial areas, Manchester, Stretford, and Salford suffered substantial bomb damage during the Second World War, most notably in the Manchester Blitz of December 1940. The Empress Conservation Area suffered particularly heavily due to its proximity to Trafford Park, where major production of war supplies was carried out. This, together with the ensuing decades of clearing, is likely to account for at least some of the empty sites found in the conservation area today, where buildings historically stood.

3.3.12 The large commercial buildings in the centre of the conservation area avoided substantial bomb damage but saw subsequent decline through the downturn in manufacturing that occurred across England from the 1950s onwards.

### 3.4 21<sup>st</sup> Century

3.4.1 The declining fortunes of the area in the later 20<sup>th</sup> and early 21<sup>st</sup> centuries have had a marked impact, with infill development creating a more fragmented street scene. A deterioration in the condition of its landmark buildings has also followed from the long-term vacancy of key and large industrial sites including the Essence Factory, the Veno Building and Empress Mill (some of these further damaged by fire). Whilst there has been interest in development, and in the regeneration of landmark buildings, these schemes have stalled or have been delivered to a substandard quality, most notably at the failed developments adjoining the Veno Building.

3.4.2 Many of the industrial buildings to the northwest have been altered or redeveloped (as is common for continued industrial uses), whilst a large proportion of the residential properties have been converted for office or commercial use.

3.4.3 Despite this change, however, the area retains both much of its heart and its potential vitality, as evidenced in the successful conversion of the Empress Brewery as a vibrant business centre **[Plates 3.6-3.10]**.

*All historic photographs from the Manchester Local Image Collection (<https://images.manchester.gov.uk>)*



3.6a Chester Terrace (1961)



3.6b Chester Terrace (2024)



3.8a Empress Brewery (1961)



3.6b Chester Terrace (2024)



3.7b Trafford Place (part) (2024)



3.8b Empress Brewery (2024)



**3.9a** *The Essence Factory (1961)*



**3.10a** *Trafford Press (1961)*



**3.9b** *Essence Factory (2024)*



**3.10b** *Trafford Press (2024)*

## 4 Character





## 4 Character

### 4.1 Summary

#### Overview

4.1.1 The Empress Conservation Area is situated 1.5m southwest of Manchester city centre and 2.5m northeast of Stretford. Its setting is urban, although a sense of spaciousness is derived from the width of Chester Road (which historically contained a tramline running along its middle). The area is formed of residential, commercial, and industrial development, the latter running adjacent to the Bridgewater Canal, railway and Metrolink. Beyond these are the River Irwell and Pomona Docks, an area proposed for significant regeneration.

4.1.2 The development pattern is focused on the linear spine of Chester Road, a busy secondary A-road, with a gateway at the junction of Chester Road and Bridgewater Way in the north and another on Chester Road in the south. Framing this principal spine are rows of two-storey, red brick, 19<sup>th</sup> century mainly terraced groups with slate

roofs, set modestly back from the road behind low boundary walls of stone and brick, with railings above. These coincide, and contrast positively, with the ornate landmark commercial buildings in the northwest, which lead into the primarily industrial land off the spine of Hadfield/Wright Street.

4.1.3 The development pattern converges at the area's heart around the junction of Chester Road and Empress Street, where its commercial and residential character is anchored by a significant set piece streetscape incorporating the Essence Factory, the Empress Brewery, 384 Chester Road and the National Works at the junction, with the Veno and Trafford Press Buildings and Empress Mill further out.

4.1.4 The execution of architectural styles and design varies across the area, as does scale, all related to differences in historic use. However, these buildings are drawn together not only by the street network but also by a common material palette, largely of red brick and terracotta,

which provides a cohesive hue across the area and its varied uses. The prevailing architectural style is generally reserved, with varying degrees of classical influence. All of the historic buildings show a classical emphasis on symmetry, proportion and regularity of parts.

#### Spatial Characteristics

4.1.5 The conservation area and its proposed extensions can be divided into four character areas, two originally domestic and the others industrial/commercial. These each have defining characteristics, some of which are shared with other character areas, whilst exhibiting a number of common features and elements that are shown across the conservation area as a whole.

4.1.6 The grain and development pattern vary across the conservation area, generally according to the different character areas. There is a finer grain to the domestic character areas and a looser (coarser) one to the Hadfield Street industrial area, where the buildings are larger and are generally surrounded by cleared areas, often substantial and now in

parking use. The grain is loosest at the landmark buildings, although the well-articulated bays and detailing at roof level maintain a clear sense of rhythm and a human scale, despite the height of the Essence Factory in particular.

4.1.7 Spatially, the area's spine is formed by Chester Road, its width, heavy traffic and busy junctions in effect dividing the conservation area into two. There are also clear distinctions in character, architecture and activity between Chester Road itself, the area to its north/west, and the area to its south/east.

4.1.8 To Chester Road, all properties present their principal frontages to the pavements and their rears to the back streets that run behind Chester Road to either side (or, to Trafford Mount, the similar service alleyway). A similar back street, here a cul-de-sac, is also contained in the proposed extension to the conservation area (at Blackley Street). Narrower than the busy Chester Road, these back streets traditionally had a service function

and now mainly support the rear parking areas. They afford short-range views of the rears of the terraces, the frequent loss of the rear boundary walls in favour of metal fencing, which is generally detracting, extending these views to the ground floor.

4.1.9 More substantial streets run out of Chester Road at Darwen Street in the north (also providing access to Wright Street) and Nuttall and Chorlton Streets in the west, the latter historically heavily trafficked as connectors linking the two arterial roads of Chester Road and Stretford Road. Side returns fronting onto side streets often match or even exceed the principal frontages in architectural embellishment, the architecture of the principal, secondary and rear frontages reinforcing a hierarchy of status from the principal road to the side streets (some, such as Darwen Street, evidently of a higher status than others) and, finally, the back streets.

## **Internal Views**

4.1.10 The linear character of the streetscape ensures that the principal and defining views running through and within the conservation area are also linear. These are comprised of both long-range views along the frontages and shorter-range views to those clusters that are concentrated at the connections between key streets.

4.1.10 The frontage views incorporate both domestic and industrial buildings and often include reveals or glimpsed views of the landmark industrial/commercial buildings at the conservation area's core.

## **Residential Character Areas**

4.1.12 The historic residential buildings include two purpose-built pairs of semi-detached dwellings, several short terraces that were each built as a single complete development, terraces that have been reduced in length by the loss of end dwellings, and a long row of houses (Trafford Place) that, as closer examination shows, was actually built sequentially as several

separate developments, marked by slight variations in height, detailing and brickwork, rather than as a single uniform terrace. All of these dwellings were built as two principal storeys, some with an integral attic floor that had windows only to the rear, all set below a slate-covered roof, usually pitched (and occasionally hipped or Dutch).

4.1.13 To the residential buildings there is a general uniformity and a loose classical style, marked by a mixture of symmetry and mirroring across pairs and terraces, regular patterns of door and window openings, measured proportions, and strong vertical and horizontal emphases. This gives a strong articulation to the bays, establishing a clear sense of rhythm. The elevations are in brick (originally without render or paint) and the pitched roofs mainly in slate (some, however, having inappropriate concrete tile replacements). Windows are vertical (rather than horizontal) – historically sashed – and in a regular and repeating fenestration pattern, with semi-circular fanlight windows above the principal doors. These

doors are historically set within decorated wooden architraves or doorframes. In the cohesiveness of their scale, materials, articulation and positioning within their plots, the domestic groups show an underlying cohesiveness and a general coherency, their regular form, scale and both building and plot lines giving a sense of enclosure to the streets to both the front and rear. There is a consistent human scale to these buildings and their plots, creating a fine urban grain that contrasts starkly with the landmark industrial buildings facing and adjacent.

4.1.14 Faced in red brick to the principal frontages, these residential buildings (some now in commercial use) are also fairly regular in style, although they vary in their use of canted bays. Interest and variation are instead added primarily through the use of restrained detailing, which also establishes and conveys a difference in architectural status and, therefore, in the expected social status of their original intended occupants. This includes horizontal stone banding,

classical portico-like detailing to the architraves, timber dentilled corbelling below the overhanging eaves, a simple use of decorative stone and coloured brickwork and, less commonly, pierced timber bargeboards. Chimneys are a distinctive feature, rising in a regular pattern either on or to either side of the ridgeline, often paired to serve adjacent dwellings. Some groups also have incised name stones.

4.1.15 The rears of the residential buildings are plain, faced in brown brick without adornment, their main defining features their traditional outriggers and their characteristic window patterns. These window openings are vertical and rectangular, reminding us of the original use of sashes, their sizes and proportions reflective of a traditional hierarchy from ground to attic floor (the latter the smallest, tucked below the eaves). The outriggers, where original, historic or sympathetic, are single- or double-storey, in exposed brick and with a fenestration pattern matching the principal building, their roofs pitched with a slate covering.

Rears show, however, a mixture of original or sympathetic outriggers, outriggers that have been substantially rebuilt or extended, and less traditional extensions and infill, which expresses their organic development as buildings have changed in use. Modern outriggers and other extensions are not always sympathetic to the historic character and scale - a number now dominate or even entirely conceal the rear elevation, whilst rising as high as the principal roof.

4.1.16 The residential buildings each sit in a generous plot that extends to both the front and rear. To the front are small garden areas, some still in use as gardens but many others hard surfaced for parking. A number retain brick and stone boundary walls with iron railings above, some with stone gatepiers. To the rear the houses were built with back gardens, now converted for parking or light industrial use and almost entirely with modern boundary treatments. There is, therefore, a general sense of spaciousness to the terraced rows, which is further enhanced by the back streets

to their rears and the occasional presence of gap sites opened up by the loss of the terrace ends.

### **Industrial/Commercial Character Areas**

4.1.17 The urban grain becomes much looser (or coarser) in the industrial areas, although this too varies, the plots and their buildings being largest at the landmark buildings and of a smaller size in the industrial area behind. Underlying this differing in plot size is a variation in building scale, although a human scale is maintained. The largest scale is found at the landmark cluster at the junction of Chester Road and Empress Street, where long linear buildings run parallel with the roads, their rears (where still extant) extending behind. A second cluster lies to its west, to either side of Empress Street, where Empress Mill and the complex opposite have substantial footprints.

4.1.18 This scale is, however, achieved without exceeding a mid-rise height. Even to Chester Road the tallest landmark buildings reach only five storeys, their height further emphasised by complex roof forms

and gables, dormer windows and other detailing at roof level, and by the contrasting scale of the low-lying terraced buildings facing and adjacent. Empress Mill is likewise of five storeys, its height emphasised by its sheer walls and flat roof, but the multi-phase complex facing is of only two storeys, although it rises higher in its "north lights" sawtooth roof. Beyond these two clusters, the industrial buildings are much lower in form, the historic buildings being of two principal storeys (some with a basement below). Modern industrial buildings tend to be lower, generally of one and two storeys.

4.1.19 The industrial buildings also vary in character, to a much greater degree than the residential buildings. However, there is again a general symmetry and regularity of parts across the buildings, which in the most utilitarian buildings is accidental, dictated by the industrial processes taking place and the apparent absence of any perceived need for ornamentation. In the 'polite' landmark buildings to Chester Road, which is much ornate, this functional need coalesced with

classical design principles, which favoured symmetry, proportion and regularity of parts.

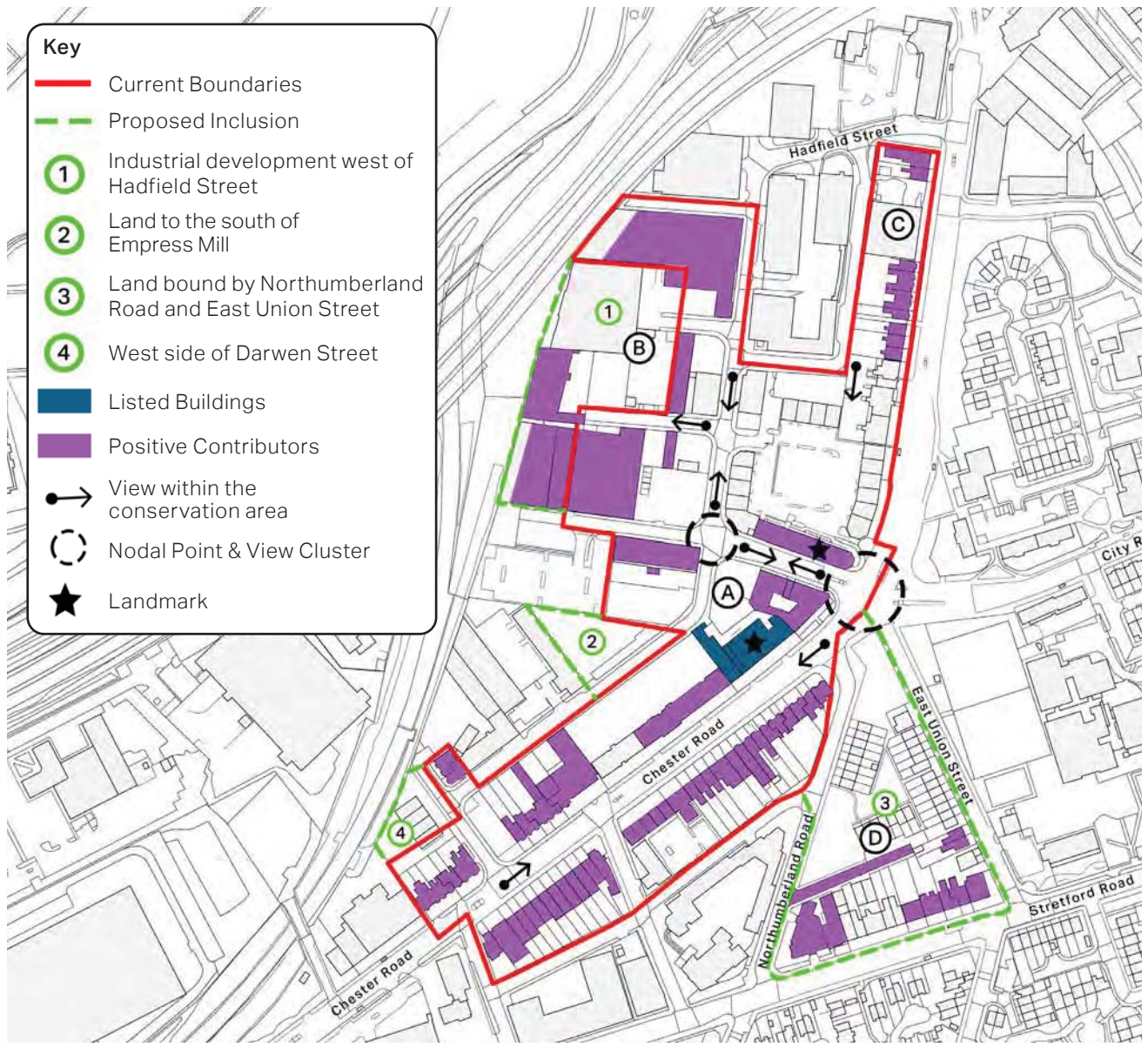
4.1.20 Associated with this is a substantial variation in the degree of embellishment, which likewise generally aligns with the original scale of the industrial buildings and their plots (the Veno and Trafford Press Buildings originally much larger but today cleared of all but their frontage blocks). To the 'polite' landmark buildings on Chester Road the style is generally loosely classical but this is not uniform and the Essence Factory, for example, combines elements of the Neo-Renaissance and Neo-Gothic. The Edwardian/interwar Trafford Press Building and the interwar Veno Building, by contrast, introduce distinctive early-20<sup>th</sup> century readings of the classical tradition. They combine this with a materiality and colour palette unusual within the conservation area, the pale copper-coloured faience embellishments of the Trafford Press Building and the white frontage of the Veno Building adding a sudden burst

of contrasting colour against the otherwise cohesive colour palette of red, terracotta and brown to the landmark buildings to their immediate north. The collective effect is to pronounce and enhance the central role of this cluster of landmark buildings as the heart and focal point of the conservation area. Empress Mill (where again only the main building survives), is the exception of the landmark buildings. Whereas the Chester Road landmark buildings were intended as highly visible, elaborate, showpieces on this principal road into the city centre, there were no such concerns with Empress Mill, where only the needs of manufacturing dictated its appearance. As such, Empress Mill is a landmark building mainly in its scale and resultant townscape presence but not in its architecture, and in both style and form it shows a much stronger relationship with the similarly utilitarian 'back street' buildings surrounding, which form Character Area B.

## Landscaping

4.1.21 Spatially, the front gardens of the well-appointed terraced houses offer the only soft landscaping within the conservation area, with all of the rear gardens lost, creating a much harder appearance. There are no public green spaces within the conservation area itself (although much enhancement is derived from those immediately adjacent), and no street trees. The area is instead heavily defined by highways and, ultimately, by traffic and parking. Its public surfaces are generally a mixture of modern tarmac and concrete slabs with varied lamp standards and standardised highways street furniture (including bollards that do much to create safe cycling but jar with the area's historic character).

4.1.22 There are some intermittent localised areas of historic surfaces, including stone setts to the Essence Factory and the Empress Brewery and to Northumberland Road and Blackley Street (both within the proposed extension). Historic street furniture is limited to embossed street name signs in metal and timber and a historic bollard to Empress Mill **[Plate 4.1]**.



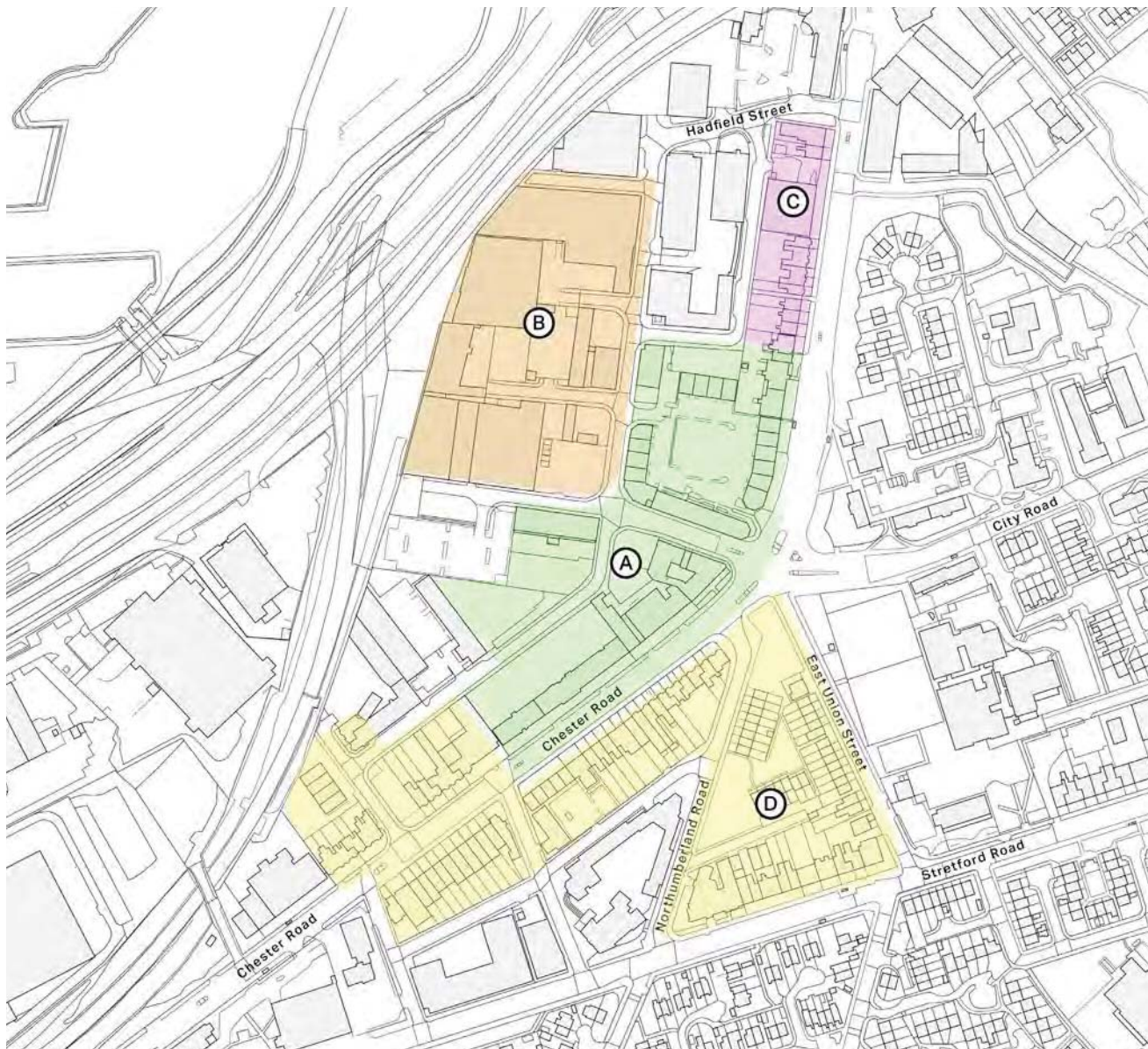
**Character Areas**

4.1.23 The conservation area and proposed extensions are split into four character areas:

- Character Area A: Essence Factory and Commercial Heart
- Character Area B: Hadfield Street and Industrial Belt
- Character Area C: Chester Road Residential (North)
- Character Area D: Chester Road Residential (South)

These are outlined in detail below **[Plate 4.2]**.

4.1 Townscape character map



**Key:**

- (A)** Essence Factory and Commercial Heart
- (B)** Hadfield Street and Industrial Core
- (C)** Chester Road Residential (North)
- (D)** Chester Road Residential (South)

4.2 Character Area Boundaries and proposed extension

## 4.2 Character Area A: Essence Factory and Commercial Heart

### Listed Buildings

Essence Factory, Empress Street (Grade II) (1)

### Positive Contributors

384 Chester Road (2)  
National Works, Empress Street (3)  
Empress Brewery,  
Chester Road/Empress Street (4)  
(Former) Trafford Press, Chester Road (5)  
Veno Building, Chester Road (6)  
Empress Mil, Empress Street (7)

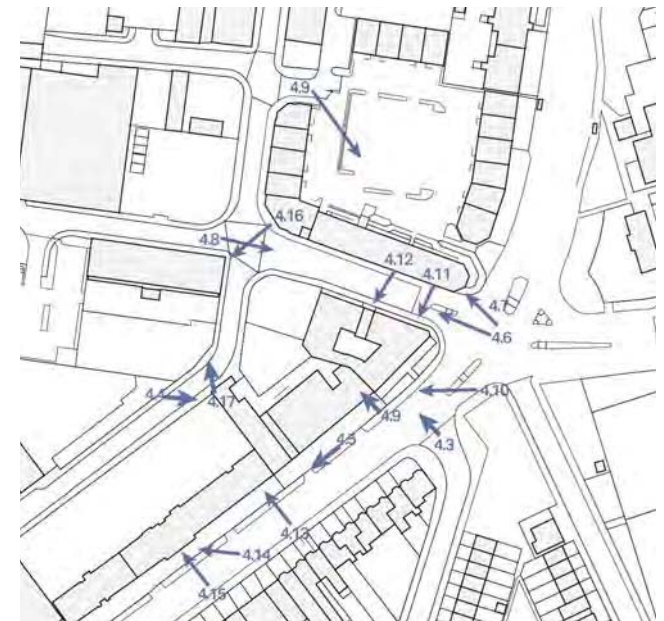
4.2.1 As outlined above, this area is the heart of the conservation area. This is evident both spatially, with the convergence of its principal roads, providing significant legibility to its urban pattern, and visually, where its ornate landmarks anchor the surrounding historic terraces. As a cluster of commercially focused showpiece buildings, this area is of greater scale and mass than the surrounding residential terraces and introduces significant architectural flair.

### Essence Factory, Chester Road (west side)

4.2.2 At the centre is the Essence Factory (1896, Grade II Listed), built for Duckworth & Co to the designs of Briggs and Wolstenholme of Blackburn. Red brick with terracotta, the façade is defined by semi-circular arched windows with a combination of transom, mullion and tracery detailing, with oriel bays leading up to a mixture of towered and octagonal turrets to the centre and corners.

4.2.3 This is a fine design reflecting the combined warehouse and office use, the latter requiring a sense of grandeur and commercial status for its clients (and its use in advertising materials). The building is orientated in an L-shape back to Wright Street, where the factory element retains its former courtyard, boiler and engine houses, stables, and entrance lodge. These smaller structures are curtilage listed, retain cast iron windows and escape stairs and, although inherently more restrained, utilise brick detailing. Enclosing the group is a historic boundary wall,

that to the Chester Road frontage in brick and terracotta with iron gates and some historic stone setts retained to the forecourt. Despite its importance, the building is vacant and deteriorating [Plates 4.3-4.5].



Character Area A photograph positions of views





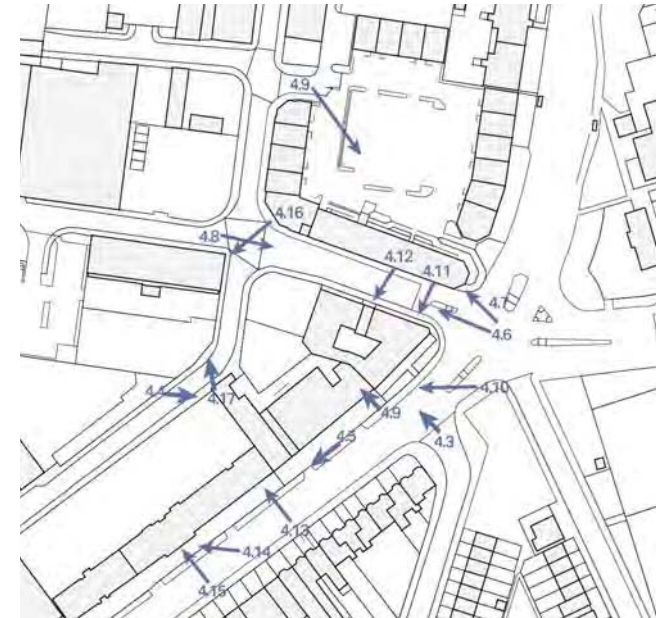
4.3 Essence Factory



4.4 Rear elevation and outbuildings to the Essence Factory, all Grade II listed, Wright Street



4.5 Ornate boundary wall and railings, with retained stone setts, at the Essence Factory



Character Area A photograph positions of views

## **Empress Brewery, 380 Chester Road (west side)**

4.2.4 Facing the Essence Factory from the opposite side of Empress Street is the former Empress Brewery (positive contributor). This was one of the earliest industrial and commercial buildings in the area, built following the establishment of the Empress Brewing Company in the 1880s, and was in operation as a brewery until 1955. It is a key example of an 'ornamental brewery,' reflecting a greater use of ornamentation than earlier breweries (which had been designed largely by engineers).

4.2.5 This ornamental character is reflected in the use of a bay rhythm defined by windows in stepped brick surrounds with arched and segmented headers, relief panels below incorporating herringbone patterns, and dentilled cornicing. The windows to the upper floors are taller, and wider, incorporating rounded headers and stone detailing to the taller elements. Brick banding in tones of red and blue engineering brick increases the visual quality of the building beyond its comparatively simple

form. Of critical importance to the townscape is the retention of the chimney and the incised 'EMPRESS BUILDING' name tablet.

4.2.6 The building was converted to commercial use as a business centre in 1992 with a series of sensitive smaller-scale extensions that enclose the street-scene to the sides and rear. Whilst generally built hard up against the pavement edge or set behind small front landscaping with brick boundary walls, the business centre incorporates a surface level car park to the interior courtyard, which provides some relief and also enables public views to the rear elevation of the brewery, where there are key elements such as the winch platform. On Chester Road some further relief is provided by soft landscaping **[Plates 4.6-4.9]**.

4.2.7 Historic setts are also present to the frontage on Chester Road (although it is not clear whether these are original) but the boundary walls and railings are modern (although sympathetic).

## **Wider Group**

4.2.8 The Essence Factory and former Empress Brewery are significant landmarks in the townscape individually and as a group. Their scale is enhanced by the sheer verticality of their facades which, even when they incorporate a small set back, do much to create powerful enclosure. This is particularly strong on Empress Street, where the brewery is hard to the pavement edge. In addition, the Brewery chimney and Essence Factory turrets all puncture the roofline, rising high above.

4.2.9 Situated around these two anchor points are 384 Chester Road, the Veno, Trafford Press and National Works buildings and Empress Mill. Whilst generally lower in scale, they complete the set piece composition at the heart of the area and provide significant legibility to its commercial growth. Together, these buildings span a period of time from the very late 19<sup>th</sup> into the early 20<sup>th</sup> century.

4.2.10 The National Works building and 384 Chester Road define the junction of Empress Street and Chester Road, creating a sense of enclosure with the former brewery opposite. Also in

red brick, no. 384 is slightly more decorative with details including a protruding central bay topped by a pointed arch, flanked by a regular pattern of windows in segmented gauged brick headers and stepped detail to the upper floor, reminiscent of the Empress Brewery and incorporating classical pediments to the corner. There is fine detailing to the principal entrance, and what appears to be an original ornamental glazed and timber doorcase **[Plates 4.10-4.11]**.

4.2.11 National Works is slightly plainer, with a flat parapet, but nonetheless has a highly characteristic fenestration pattern and, again, segmented gauged headers and a stepped brick detail **[Plate 4.12]**.

4.2.12 The Trafford Press building, in red brick with distinctive faience decoration, was originally built to house the Veno drug company prior to the construction of the adjoining building, the Veno Building. This latter building is particularly distinctive given its white art deco façade (originally red brick and painted in the post-war period),

which stands in positive contrast to the red hues of the remainder of the area's buildings **[Plates 4.13-4.15]**.

4.2.13 Empress Mill dates to the early 20<sup>th</sup> century and is a characteristically rectangular block with relief provided by tall window reveals topped with a stepped or corbelled brick detail, the windows separated by stone bands. A poor-quality roof terrace, staircase and gantries have, however, been added to the rear in recent years **[Plates 4.16-4.17]**.

4.2.14 Despite its central importance to the character and appearance of the conservation area, this area is suffering significantly as a result of vacancy, fire, deterioration in built fabric and the impact of stalled development. Veno, Trafford Press and Empress Mill are vacant, Empress Mill and Trafford Press showing clear signs of fire damage. In addition, the half-completed development adjoining Veno to the south, and along its rear elevation, has a significantly detrimental impact on the area, both visually,

as a result of condition, and spatially as a poor-quality break in the street scene.

4.2.15 These issues are exacerbated by poor-quality hard surfacing, overgrown shrubbery and the ever-present Heras fencing and hoarding. Wright Street in particular also suffers from fly tipping and abandoned vehicles. The condition of the land to the rear, along Wright Street is also very poor, despite the retention of key elements including gatepiers to the former yard behind Empress Mill.

4.2.16 These buildings feature in a variety of views, most prominently linear views along Chester Road in both directions and wide views from East Union Street and City Road. In addition, views from Chester Road to the west along Wright Street take in the Veno and Trafford Press Buildings, whilst views of the rear of the Essence Factory and Trafford Press and Veno Buildings can be appreciated from the car park to Empress Street. Consequently, any development inside their plots must not dominate these buildings, from either the rear or the front.



4.6 Former Empress Brewery, Empress Street



4.7 Signage at the former Empress Brewery (although not original) is critical to the legibility of the area's history



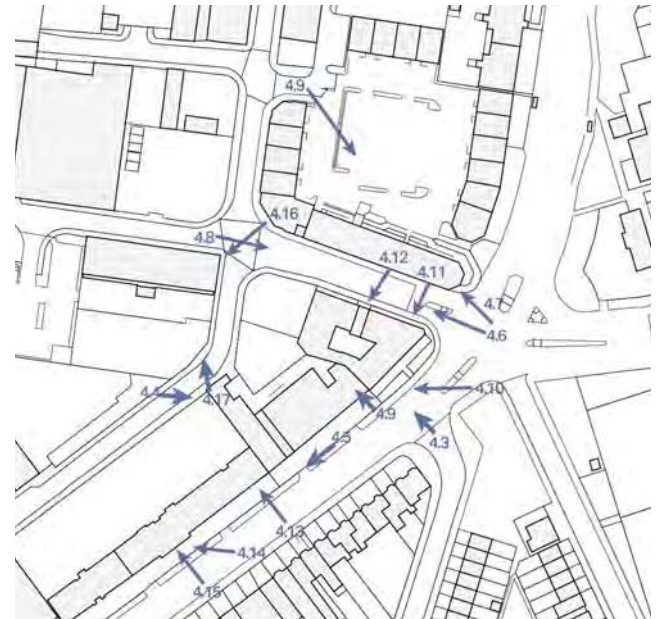
4.8 Empress Brewery, side elevation and business centre addition



4.9 Empress Brewery, site interior with historic and modern buildings



4.10 384 Chester Road



Character Area A photograph positions of views



4.11 Fine door joinery and detail at 384 Chester Road



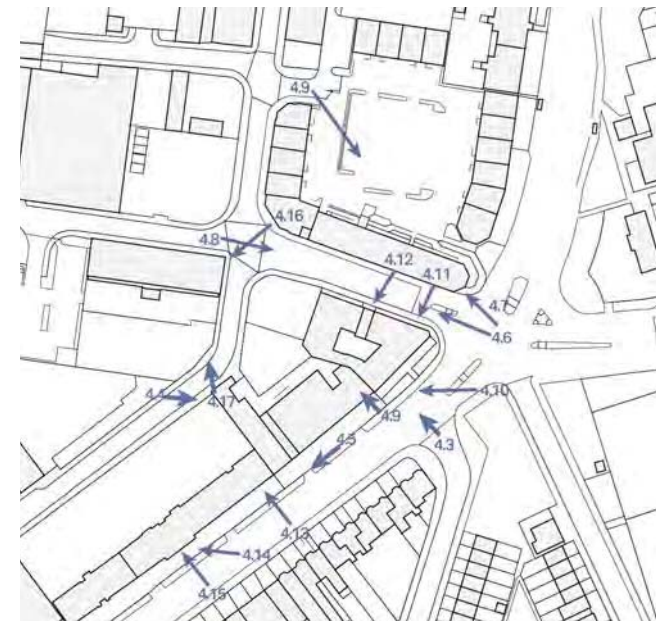
4.12 National Works, Empress Street



4.13 Trafford Press, Chester Road



4.14 Veno Building, Chester Road



Character Area A photograph positions of views



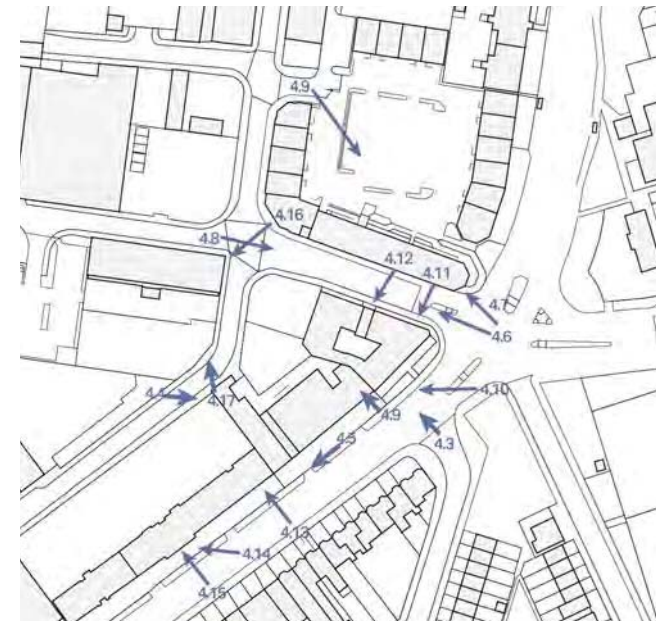
4.15 The art deco detailing of the Venio Building is unique in the area



4.16 Empress Mill, Empress Street



4.17 Historic gatepiers survive to a former yard adjoining Empress Mill on Wright Street



Character Area A photograph positions of views

## 4.3 Character Area B: Hadfield Street and Industrial Belt

### Listed Buildings

N/a

### Positive Contributors

(Former) School Furniture Works, Hadfield Street/Turner Street (8)  
Hadfield Street Works, 14 Hadfield Street (9)  
Old Engine Works, 2 Lund Street (10)  
1 Lund Street (11)  
Former Pomona Engine Works, west end of Lund Street (12)  
Industrial complex to the north side of Empress Street (13)

4.3.1 Occupying the northwest part of the conservation area, and leading off the commercial heart, is an area now given over solely to industrial uses. Historically this area also incorporated residential terraces running from east to west that were denser and more urban in their form than the larger and more spacious properties on Chester Road. Their clearance in the 20<sup>th</sup> century has removed the legibility of much of the historic development pattern between Chester and Hadfield

Roads. Industrial uses, and the associated development pattern, are, however, retained to the west, with buildings and working yards situated in historic blocks along and off Turner Street, Smith Street and Lund Street. This retention of its industrial use is an important part of the character of this area and that of the conservation area as a whole.

4.3.2 These industrial buildings are generally lower-lying, smaller in scale and less ornate than the landmark buildings around the area's heart. The urban grain is also looser, with boundaries also typically more permeable, allowing free views into many of the sites. This juxtaposition creates a powerful contrast with Character Area A immediately adjacent, this area bringing its own distinct character as a more utilitarian industrial belt, its character enhanced by the light industrial uses that continue today.

4.3.3 There also many later buildings and/or examples of modern re-fronting, with a high proportion of hard surface yards and car parks; this is not unusual for an area that maintains an industrial use. Indeed, this intermittent and

varied quality is characteristic and expressive of its organic evolution. It is primarily areas where modern development is overbearing or is vacant, as at 69 Hadfield Street, that cause the highest detriment. However, poor boundary treatments also pose an issue, with many examples of intrusive metal palisade and security fencing and gates.

4.3.4 Positively, Hadfield Street provides views of the heart of the conservation area and in particular Empress Street and the former Empress Brewery.

### 14 Hadfield Street (west side) and return to Turner Street

### 16 Hadfield Street (west side) and return to Smith Street

4.3.5 A number of historic working premises are retained within this area, although with evident alteration. This includes the former School Furniture Works (positive contributor), 16 Hadfield Street, which fronts both Hadfield and Turner Streets. Two storeys high and dating from the late 19<sup>th</sup> century, this is constructed primarily in red brick, with gauged arched headers over small windows; it introduces

some decoration using contrasting engineering and buff brick bands, further relief being provided by recessed bays with stepped detail. Particularly strong is the gabled bay pattern to Turner Street with brick detail and roundel windows (which is continued in the final three bays, which are of a slightly later date) **[Plates 4.18-4.19].**

- 4.3.6 This adjoins the Former Hadfield Street Works (positive contributor), 14 Hadfield Street, which continues some elements of detail from the School Furniture Works, although stepping up in scale, and wraps around onto Smith Street. Dating to the 1890s, this building initially manufactured nuts and bolts and was later an engineering works, although the surviving frontage block is only a fragment of the earlier complex **[Plate 4.20].**

### **1 Lund Street (south side)**

- 4.3.7 Further south is 1 Lund Street (positive contributor), which is likely to have originated as a chemical works, built in red brick with a slate roof. Despite alteration, and some rebuilding, it retains its simple industrial character

as a long, rectangular, block with strong gable ends. Although now read as a detached block, this building was historically part of an extensive complex of adjoined turn of the 19<sup>th</sup> and early 20<sup>th</sup> century buildings that once occupied the entire space between Empress Street and Lund Street. These were mainly removed in the later 20<sup>th</sup> century, leaving only no. 1.

### **2 Lund Street (north side)**

- 4.3.8 Opposite, across Lund Street, is the Old Engine Works (positive contributor), occupied by Glancy Fawcett. This appears to date to the 1930s and may have been associated with the adjoining Paragon Preserve Works (which has since been replaced by a modern flat-roofed industrial development). Although altered, particularly by its replacement windows, and lacking the finer decorative details of other earlier buildings, it makes a positive contribution to the area through its industrial form and its materiality.

- 4.3.9 The building along its east, fronting onto Hadfield Street, is modern and was historically the site of terraced housing **[Plates 4.21-4.22].**

### **Lund Street**

- 4.3.10 Whilst substantial clearance has occurred in its southwest, there are also retained earlier buildings to the west end of Lund Street.
- 4.3.11 At the road's end, the complex now occupied by Empress Autos is the former Pomona Engine Works (positive contributor). This has retained its brick boundary wall, the original entrance position and all of its earliest buildings (the principal building, the north range, boiler house and chimney) all extant by 1894, plus an incorporated warehouse (present by 1915 and built in the rear of the Empress Street complex adjacent). Early/mid-20<sup>th</sup> century additions that once filled the enclosed yard have been removed, reinstating the original footprint of the yard, enclosed within the boundary wall. The site is shown on the 1908 map as a sawmill adjoining the Paragon Preserve Works and as laboratories for Veno in the 1930s **[Plate 4.23].** All buildings are in brick,



the main block painted and with a replacement roof and windows; the boiler house chimney is an important townscape feature.

### Empress Street Complex

4.3.12 To the southern side of Lund Street is the rear of the Empress Street industrial complex (positive contributor), which once filled the townscape block between Empress Street, Hadfield Street and Lund Street but has since been almost entirely cleared in its east. This retains its earliest building (early 1890s and once occupied by the Empress Machine Tool Works), pre-1915 extensions to either side and, on the east side, part of the early/mid-20<sup>th</sup> century extensions added after 1915 (which is contemporary with 1 Lund Street, added as part of the extension of the complex through various accretions and additions).

4.3.13 Although re-fronted and rebuilt to Empress Street, the older buildings all appear survive behind, the two sawtooth ("north lights") roofs a key townscape element and the historic brick rears facing onto Lund Street in

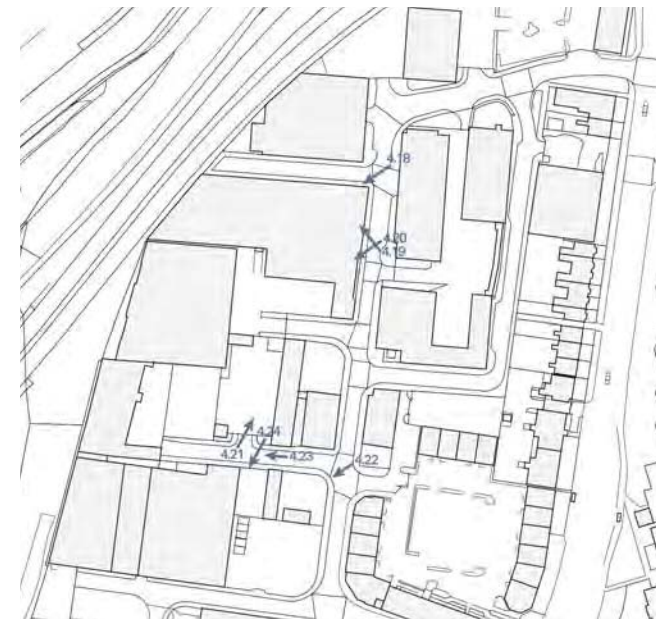
the north. Most of the smaller ancillary structures once situated to their rear and fronting onto Lund Street have also been lost but a single building – historically a warehouse – has survived and is now incorporated in full or part into the Empress Autos site adjacent [Plate 4.24].



4.18 Former School Furniture Works, Turner Street and Hadfield Street



4.19 Hadfield Street frontage of the Former School Furniture Works



Character Area B photograph positions of views



4.20 Former Hadfield Street Works



4.21 Old Engine House, Lund Street



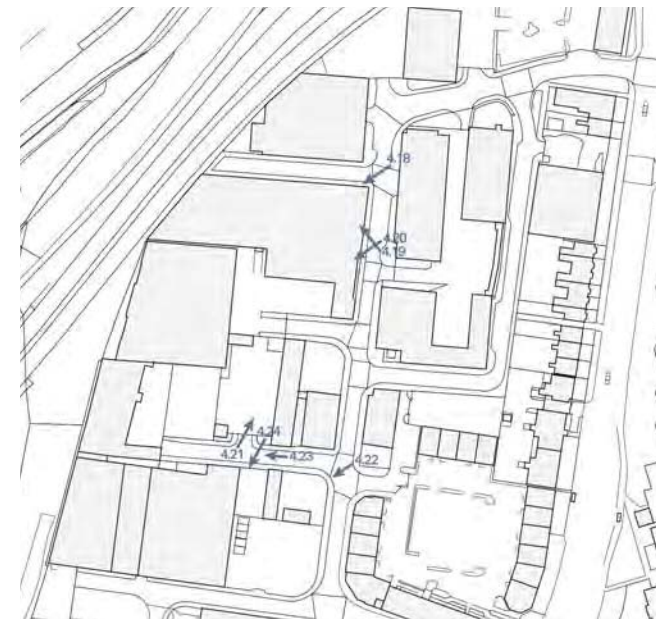
4.22 1 Lund Street



4.23 Remaining fragments of earlier manufactories off Lund Street, including the chimney of the former Pomona Engine Works



4.24 Rear of the Empress Street complex, facing onto Lund Street



Character Area B photograph positions of views

## 4.4 Character Area C: Chester Road Residential (North)

### Listed Buildings

N/a

### Positive Contributors

Cornbrook Terrace,  
328-330 Chester Road (even) (14)

Birkdale Terrace,  
342-352 Chester Road (even) (15)

Chester Terrace,  
354-358 Chester Road (even) (16)

4.4.1 The remainder of the buildings in the area are residential and are focused along the spine of Chester Road, separated to the north and south by the commercial development at the area's heart. The buildings are also punctuated by smaller gaps in the streetscape, created by the loss or shortening of terraces and adding a degree of variety and visual interest.

4.4.2 The dwellings built along Chester Road were much higher quality than those constructed in Character Area B for (presumably) manufacturing workers. They

originally included a mixture of villa-like dwellings and short terraces, the high-status villas since demolished. Intended for the middle classes, all of these buildings were consequently much larger than those built in the Hadfield Street area, with more spatial quality (and incorporating front and rear gardens).

4.4.3 The surviving terraces (some now truncated) demonstrate generous proportions and a good degree of architectural detailing.

4.4.4 Character Area C, the northern section of the conservation area, provides the gateway to Empress from the north. This is less intact than Character Area D, and more fragmented, but some legibility of its historic interest remains, nonetheless. Modern residential development to the immediate northwest of Character Area C, just outside of the boundaries of the area, helps to ease the transition into the conservation area from the heavily trafficked junction **[Plates 4.25-4.26]**.

### Cornbrook Terrace, 328-330 Chester Road (west side)

4.4.5 The conservation area begins at Cornbrook Terrace, 328-330 Chester Road (positive contributor), the surviving half of a formerly larger terrace, built prior to 1851 (probably in the 1830s) and representing one of the earliest residential buildings in the area. Two storeys high, in Flemish bond red brick, its two remaining single-fronted dwellings form a mirrored group and have pared-back detailing focused on the use of alternating panels of projecting and recessed brickwork, stone cills and (to the recessed panels) stone bands, and segmented gauged brick headers to the windows. The windows themselves have the vertical proportions of sash windows but are poor-quality modern casement replacements in an inappropriate design, although retaining their stone cills and, at ground floor at least, historic internal shutters. The timber architraves to the doors are poor modern replacements, as are the doors, although both retain their transom openings, one seemingly

with the historic transom glazing itself. Both dwellings retain their front chimney stacks in matching red brick, with stone chimneypots, that in the south of double width (having originally served two properties) **[Plate 4.27]**. Although of a proportionally wide massing, the frontage is recognisably Georgian in its pared-back classical character, symmetry and understated classical references. The frontage of this terrace appears to have provided the template for Chester Terrace, but has since lost its original architraves, its sash windows and its eaves detailing. A truncated narrow rebated panel in its south, where the terrace previously continued, evidently formed the model for that to Chester Terrace and presumably once similarly contained the terrace name stone.

4.4.6 To the sides, the southern wall is a modern replacement (replacing what was originally an internal party wall), the chimneybreast retained and the southern half exposed outside the wall. The wall itself is in modern red brick and has a non-symmetrical crow-stepped

profile, capped with copings. The north wall is obscured at ground floor by a later lean-to addition but at first floor is mainly of brown brick, with evidence of what may be infilled windows, its profile non-symmetrical (reflecting the hipped roof above). A modern billboard covers much of the exposed brickwork and is detracting. Above, the tiles to the low-pitched roof appear to be concrete, in place of the original slates, and the truncation of the terrace has resulted in a non-mirrored roof form, this being a hipped or Dutch roof in the north but only pitched in the south.

4.4.7 Externally, both dwellings retain their sandstone steps and low walls adjacent, although the iron railings are missing. The historic boundary walls to the front have been replaced with low-quality low walls in modern brick, devoid of copings, although the historic iron railings appear to have been reused in the northern property (but are lost in the south). To the sides, the north side wall is formed in historic brickwork and runs the full depth of

the plot, incorporated as part of the wall to the garage, but is graffitied and in a poor condition. The southern wall, with its flat copings, is entirely modern (and again in poor condition), with the garden areas to the front overgrown and their paths surfaced in a poor concrete. Both buildings appear to be in a deteriorating condition **[Plate 4.28]**.

4.4.8 To the rears, the exposed parts of the properties appear less altered. These are plain, faced in brown brick, and articulate the terrace's Georgian origins in the pattern of its three levels of windows, those at attic level much smaller and squeezed beneath the eaves. The window openings have the shape of sash windows but the windows themselves are poor-quality modern replacements. Various small lean-to extensions are visible.

4.4.9 A 20<sup>th</sup> century commercial repairs garage runs along the northern side of the terrace and continues across the former garden behind, wrapping around the north of the building rear. This add-on undermines the domestic character of the building

and, when combined with the advertisement hoarding above and the modern public realm to the street, provides a poor gateway to the area. The rear garden to the south appears overgrown with vegetation, with poor-quality insubstantial shelters built against the plot boundary, which is enclosed behind various poor-quality ad hoc gates and fences.

### Opportunity Site

4.4.10 Of detriment to this area is a plot of land between 342 Chester Road and no. 330 (part of Cornbrook Terrace) to its immediate north. Previously the site of a large villa, it now includes poor quality and utilitarian industrial units in its west, accessed from the Hadfield Street area. These turn their back on the main road, with an area of overgrown shrubs and railings running along the frontage. This area in the east is a prime development opportunity, which should seek to sensitively stitch the road back together **[Plate 4.29]**.

### Birkdale Terrace, 342-352 Chester Road (west side)

### Chester Terraces, 354-358 Chester Road (west side)

4.4.11 Further south, the dwellings are in better condition. This includes a cohesive run of properties along Chester and Birkdale Terraces (positive contributors), both terraces surviving in their full lengths. Separately and as a group, these are the conservation area's most intact terraces to the street-facing frontages.

4.4.12 Whilst drawn together by a cohesive scale, their siting behind small front gardens, and the overarching use of brick, the two terraces differ, and contrast positively, through their detailing. Both are of two principal storeys.

4.4.13 Birkdale Terrace (positive contributor) is defined by the steep roofed and front-facing gables to its end bays, enhanced by decorative painted pierced timber bargeboards at eaves level. Built as a sequence of mirrored pairs of single-fronted dwellings, its paired doors are set back in recessed bays, below

slate-covered single-pitch roofs, bargeboards matching those at eaves level and gauged segmented brick arches. Articulation across the façade is provided at ground floor by canted bays with slate-covered hipped roofs and the repeated use of buff gauged segmented brick arches to the window openings, creating a strong sense of rhythm and uniformity. Narrow bands of blue brick running horizontally unite the group and a stone nameplate sits above the doors to nos. 346 and 348.

4.4.14 In addition to retaining its decorative detailing, the terrace has timber panelled doors to all six of its dwellings and timber sash windows to four (the exceptions being nos. 344 and 346). Above, stacked chimneybreasts survive between nos. 350 and 348 and nos. 346 and 344, and two narrower breasts to the side of no. 342, with those to no. 352 rebuilt to replicate the original detailing. The majority of the hipped roof retains its slate covering but this has been replaced with concrete tiles to the gable ends,

where a finial survives at the apex of the southern front-facing gable (that at the north now lost).

4.4.15 Externally, each house is accessed via a short step of stone steps, without railings, set on courses of blue brick. All properties retain a front garden area, and whilst these have been hard landscaped, which is detracting, they all have some degree of planting along the inside of their street boundary walls. All properties also retain intact boundary walls of red and blue brick and stone copings, with a complete set of stone gatepiers with conical capitals. Their railings and gates also appear to be historic **[Plates 4.30-4.31]**.

4.4.14 Originally residential, several of the buildings are now in commercial and office use which, although preserving their overall condition, has prompted out-of-character alterations to the rear. They still retain their domestic character to the street, however, although there are some rooflights to the front

pitch and inappropriate signage to the windows and in the front garden to no. 346, which is detracting.

4.4.15 Chester Terrace (positive contributor) was built with a frontage matching that of Cornbrook Terrace (which has since undergone alterations). Although it too has undergone modern alterations, including the loss of some original features, it survives much more intact than that of Cornbrook Terrace, with its original style, detailing and character fully legible. Of a proportionally wide massing and flat profile, the frontage is recognisably Georgian in its pared-back classical character, symmetry and understated classical references.

4.4.16 Again, the row is two storeys high, in Flemish bond red brick, two of its single-fronted dwellings forming a mirrored group and the third divided by a narrow rebated panel bearing the terrace name stone. There is again pared-back detailing focused on the use of alternating panels of projecting and recessed brickwork, stone cills and (to the recessed

panels) stone bands, and segmented gauged brick headers to the windows. The window openings are unaltered and retain timber sashes, of a mixture of types (some with top-opening casements) and are mostly later. The doors are timber panelled but no. 354 has been altered by the replacement of its ornate timber architrave, the stone replacement devoid of the original fine detailing, the transom window altered in shape and the doorcase removed to accommodate a much wider double-leaf door.

4.4.17 Above, the roof is again low and the terrace retains its dentilled cornice eaves detailing. Part of the roof retains its slate covering but the remainder may have been recovered in concrete tiles, whilst only one chimneystack – apparently rebuilt – survives to the front.

4.4.18 Chester Terrace's brick boundary walls appear historic, as do the piers (though the left-hand side has lost its coping stones, and the right-hand side is a matching replacement) **[Plate 4.32]**. Behind these, however, the gardens have

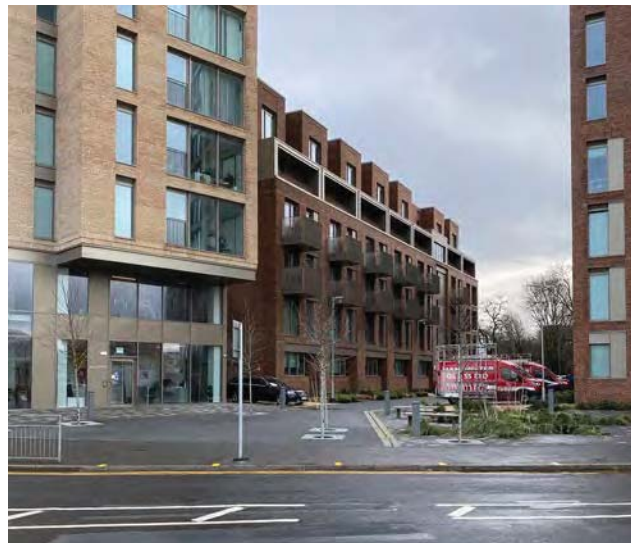
been lost to hard landscaping, with softening provided only by a remnant hedge to no. 354.

4.4.19 Adjoining Chester Terrace to the south is a modern apartment block. This development is detracting to the conservation area through its inappropriate and intrusive height, style and layout, which the provision of soft landscaping fails to mitigate its impact. Out of scale and overly tall for the area, this contrasting angular block towers over the historic terraced buildings adjacent and makes no concession to the local architectural traditions.

4.4.14 To the rear of the terraces the gardens have mainly been converted into large areas of hardstanding for parking, with sections of demolished or deteriorating boundary walls, whilst those to Cornbrook Terrace contain ad hoc industrial units of limited quality. Whilst many of the terraced properties retain outriggers (closet wings) these have generally been altered, particularly by the replacement of windows and the application of render.



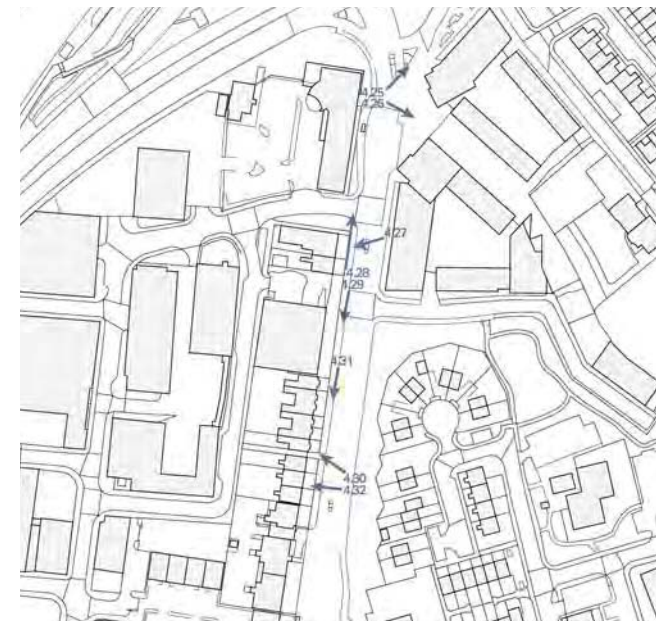
4.25 Modern apartment development to the north of the conservation area



4.26 Modern apartment development to the north of the conservation area



4.27 Cornbrook Terrace



Character Area C photograph positions of views



**4.28** Historic and modern boundary walls, the latter with reused railings, and poor surface materials at Cornbrook Terrace



**4.29** Running along the east of modern industrial units, an area of detracting and overgrown land that breaks the Chester Road frontage



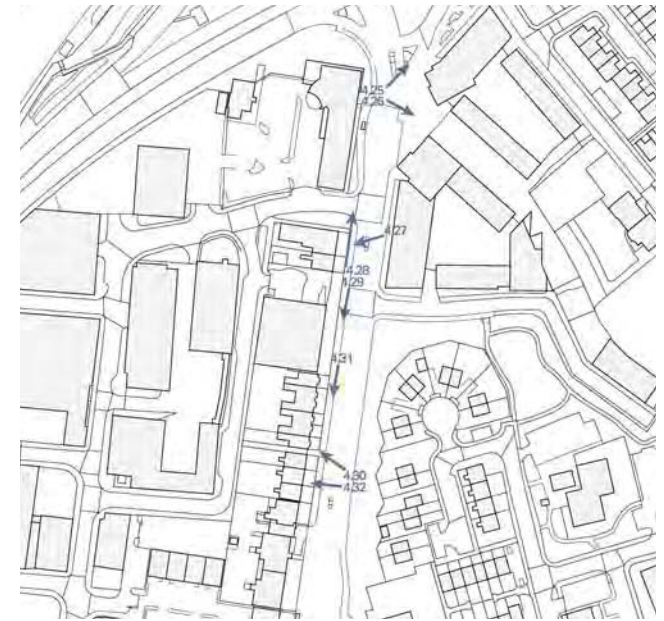
**4.30** Birkdale Terrace



**4.32** Chester Terrace



**4.31** Historic gatepiers at Birkdale Terrace



Character Area C photograph positions of views



## 4.5 Character Area D: Chester Road Residential (South)

### Listed Buildings

N/a

### Positive Contributors

Former Almondbury Place (believed), 448-450 Chester Road (17)

Dover Terrace, 452-460 Chester Road (even) (18)

Darwen Terrace, 4 and 6

Darwen Street (19)

Trafford Mount, 464-470 Chester Road and 1 Darwen Street (20)

Trafford Place, 429-457 Chester Road (odd) (21)

Winfield Terrace, 473-497 (odd) Chester Road and 2a Chorlton Street (22)

4.5.1 The residential streetscape to the southern end of Chester Road is more intact and long stretches of terraced properties contrast positively with the area's set piece industrial/commercial development. The gateway to the area from the south is defined by properties in better condition than those to the north but is generally incidental, with a sudden transition

in scale from modern commercial development and the multi-storey car park adjoining West Point to these low-lying domestic terraces.

4.5.2 The character area itself has a strong presence in views from the south, where its principal frontages channel the view along Chester Road. The rears of the terraces lining the west side of Chester Road also form part of views along Wright Street, although these currently contain a number of detracting features. The terraces to the west side of Chester Road are also visible from outside the conservation area, in glimpsed views from Stretford Road along Nuttall and Chorlton Streets.

4.5.3 Four distinct sets of terraced buildings survive along the Chester Road frontage. Trafford Place, Dover Terrace, Trafford Mount and Winfield Terrace are all positive contributors and date from between the 1850s and 1870s. A fifth group, Darwen Terrace, is set slightly to the northwest on Darwen Street and is unique amongst the surviving buildings in remaining

recognisable as a purpose-built pair of semi-detached dwellings. The semi-detached pair known as Almondbury Place appears to have survived as 448-450 Chester Road but as a result of extensive remodelling is no longer legible as this.

4.5.4 Whilst there are variations between, and even within, the individual terraces, which brings visual interest, there is a general consistency of scale and material and a regularity of style and detailing that unites the character area. This includes the two-storey and single- and double-fronted scale, the red brick frontages to the principal elevations, the prominent canted bays, and the slate tiles to the double-pitched roofs, punctuated by brick chimneys. There is a regular rhythm, expressed in the pattern of fenestration and door openings, and strong articulation to the bays, giving a cohesiveness. There is also a uniformity of form, with principal roofs consistently aligned to the road, the principal frontages sheer beyond their canted bays

(where these are present), stacked chimneys running backwards towards the roof crest, and rear outriggers running perpendicular to the principal frontage.

- 4.5.5 Architectural features are loosely classical and include pilasters (in various styles), timber architraves to the doors, with fanlight windows above, dentilled eaves detailing, and decorative corbelled cornicing (particularly to side bays). Classical proportions are generally observed, with strong horizontal emphases.
- 4.5.6 Other architectural detailing includes stone banding, plinths, overhanging eaves, and, to some terraces, canted bays at ground floor. Door and window openings are surmounted by arched and flat gauged segmented headers of brick, and although the windows have been replaced there are some good examples of timber four-panelled doors. Most buildings have stone steps (of varying heights) up to the front door, a number with historic iron handrails still surviving.

- 4.5.7 To the frontages, all terraces were built with garden areas, enclosed behind stone walls with stone copings and railings above, punctured by small gates (the small size of these openings precluding stone gatepiers in all but one instance, at Trafford Place). Although now punctured where gardens have been converted for parking, and in places rebuilt, this stone boundary treatment with stone copings provides an important sense of enclosure along this length of Chester Road and it is important that this is retained. There are several examples of historic iron railings, whilst the hedges behind the walls provide important soft landscaping within the street scene, particularly where front gardens have been lost to either low maintenance surfaced forecourts or hard surfaced car parks.

### **Trafford Place, 429-457 Chester Road (east side)**

- 4.5.8 Trafford Place (positive contributor) is the largest of these terraces and as the primary architectural form alongside the commercial set pieces defines much of the townscape from the junction of Northumberland Road to Chorlton Street. The terrace is two storeys high, in red brick with slate roofs with prominent rooflights and, to one property, unsympathetic dormer windows. Key elements run across the whole terrace, drawing it together, and include stone banding and overhanging eaves on dentilled timber brackets. Some brick chimneys survive but many have been truncated or lost **[Plates 4.33-4.36]**.
- 4.5.9 There are distinct designs across the terrace, representing its construction over different phases. The northernmost section is three bays wide with a central recessed bay set between two double-height canted bays, before the terrace transitions to two bays wide, initially with single-height canted bays. This section is followed by

a largely flat frontage inset with doors incorporating simple pilasters and rounded fanlights on corbels, and sash windows set into tall reveals with segmented gauged brick headers.

4.5.10 To the front, the houses are all set behind modest front gardens incorporating stone boundary walls and railings. Most of the railings appear to be later replacements as do many sections of stone walls (although broadly sympathetic). The gardens have been laid in full or part with hardstanding (although this is not for parking use and many still retain elements of soft landscaping). Only no. 453 has historic stone gatepiers.

4.5.11 The rear of the terrace is heavily altered. Most retain outrigger (closet wing) projections but many of these have been rebuilt, infilled, or extended further **[Plate 4.37]**. Those that are rebuilt or extended are most sympathetic where they observe the traditional gabled, pitched roof, character of the earlier outriggers and replicate their fenestration patterns.

Some extensions deviate from this traditional form, infill the outriggers, or step forward heavily, dominating the rear frontages. Some are also particularly poor in materials, construction or condition. Across the rear, windows are modern replacements and there is a cluttering of cameras, satellite dishes and ground-floor window bars, with some examples of dry verges and plastic fascias. Whilst most rears retain their exposed brickwork some have undergone full or partial rendering, whilst to the northernmost properties there are examples of non-original louvred gablets, which run up to the apex and have slate coverings.

4.5.12 Where there were once well-appointed gardens running the depth of the plots to Manchester Street there are now extensions, hardstanding car parks and vehicular gates, which are not only unsightly but also introduce a rival active frontage. Modern metal palisade fencing has also replaced what would have historically been brick boundary walls, introducing a new permeability that undermines the historically private character of these spaces.



4.33 The long frontage of Trafford Place



4.34 Historic gatepiers to Trafford Place



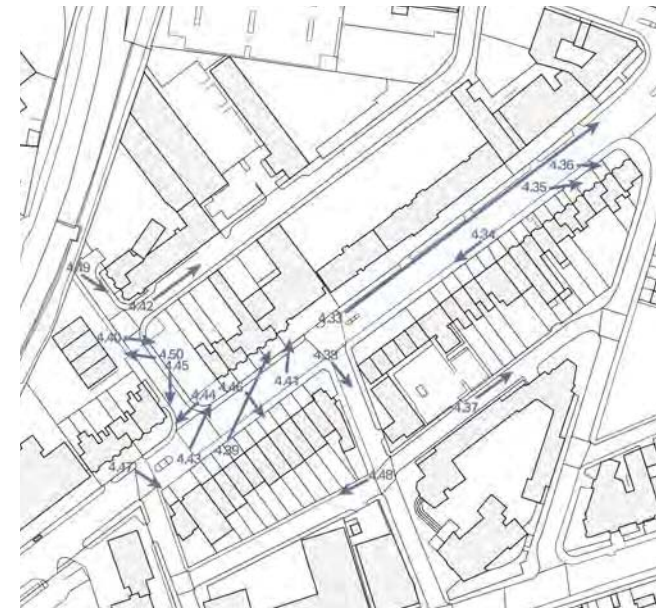
4.35 Stone boundary wall to Trafford Place



4.36 Canted bay detail at Trafford Place



4.37 Rear elevations, Trafford Place



Character Area D photograph positions of views

**Deva Court, corner of Chester Road (west side) and Chorlton Street**

4.5.13 Deva Court is a modern addition to the southern end of the terrace (positive contributor). Although its brickwork is evidently modern its Chester Road frontage defers well to the scale, composition and detailing of the historic street scene, being two-storeys high and using details such as Flemish bond brickwork, overhanging eaves, and segmented gauged brick headers. However, details such the squashed window openings could be improved, particularly when viewed in relation to the larger Victorian window openings to the remainder of the terrace. To the rear, a separate three-storey range is visibly different despite its similar materiality and colour palette, having less generous proportions to its three storeys and lacking the classical detailing of the street-facing range **[Plate 4.38]**.

**Dover Terrace, 452-460 Chester Road (west side)**

**Former Almondbury Place, 448-450 Chester Road (west side)**

4.5.14 Dover Terrace (positive contributor) is situated to the west side of Chester Road. It is two storeys high, in Flemish bond red brick with a slate roof, where rooflights are found on the front pitch. The terrace alternates between two and three bay wide properties, with single-storey canted bays, doors with rounded brick headers and fanlights, and sash windows under segmented gauged brick headers. Overhanging eaves are set on dentilled timber brackets and some chimneys remain (although these may have been altered). Some soft landscaping is retained to the frontages, behind low stone boundary walls and railings **[Plate 4.39]**.

4.5.15 Dover Terrace is now shorter than historically. To the southwest, no. 462 has been lost (as articulated by the visibly modern brick of the rebuilt gable wall, which is entirely blind) and its site is now occupied

by a car park. To the northeast, a further dwelling was once situated where the modern infill extension is now located. This infill extension now unites no. 452 with nos. 448-450 adjacent.

4.5.16 To no. 460, what was formerly the party wall separating it from no. 462 has been rebuilt in modern red brick, which continues as a flank wall running along the original brickwork to the southern wall of the outrigger behind. This whole wall is blind (with no doors or windows), with the former chimneybreast (now truncated) rendered and painted white, and an intrusive white dry verge beneath the roof. The area to its south is a hard landscaped car park only partly softened by the flowerbeds in its west, the original stone boundary wall retained to Chester Road but rebuilt in red brick with copings and railings to Darwen Street, all curving with the street at the southwest corner. A vehicle gate sits to the west, the car park transforming the original domestic character of the group to Chester Road **[Plate 4.40]**.

4.5.17 Adjoining Dover Terrace to the north is a red brick building (positive contributor) that is believed to be a remodelling of Almondbury Place, a pair of semi-detached villa-style properties built prior to the 1840s. This, however, has been refronted, introducing a modern classically inspired ground-floor frontage, whilst a glazed extension has been inserted along its south, abutting no. 452 **[Plate 4.41]**. Further terraced dwellings that once abutted the north wall of no. 448 no longer survive and their former site is currently occupied by the stalled development adjacent to the Veno Building. Should the opportunity arise, this site should be integrated with the Veno Building rather than redeveloped to a height that would tower over the terraces adjacent.

4.5.18 To Chester Road, the former gardens to Almondbury Place (nos. 448-450) have been combined as a single parking area with a non-original bricked surface. The historic

boundary treatment has been lost and replaced by modern gatepiers and poor-quality modern railings enclosing two wide vehicular gates.

4.5.19 To the rears, several properties retain their historic outriggers, of varying proportions, but nos. 448 and 454 are dominated by modern single-storey brick-built storage buildings that fill the entirety of their linear plots, fronting onto the pavement. Shorter pitched glazed buildings are bolted onto nos. 450 and 452, with the former garden areas in front combined and used for storage. Some buildings have also been rendered and painted **[Plate 4.42]**.

4.5.20 Across the groups, the gardens of both Dover Terrace and Almondbury Place have been hard landscaped for parking or, in the north, light industrial storage, with the historic boundary treatments replaced by modern metal fencing and gates. Only no. 458 retains any remnant soft landscaping.

**Trafford Mount, 464-470 Chester Road (west side) and 1 Darwen Street**

**Winfield Terrace, 473-497 Chester Road (west side) and 2a Chorlton Street**

4.5.21 The southern boundary of the conservation area is bookended by Trafford Mount to the west and Winfield Terrace to the east (both positive contributors). Although not identical, they are united as a group by their red brick frontages, segmented gauged brick headers to the window and door openings, classical detailing to the end and side entrances, slate roofs with brick chimneys, and stone steps leading up to their doorways. Both are characterised by classical detailing including round headed arched doorways with fanlight windows above, horizontal stone banding, and overhanging eaves with dentilled timber eaves detailing.

4.5.22 Trafford Mount (positive contributor) is two storeys high and a mixture of single- and double-fronted properties, well-articulated by a continuous row of single-storey canted bays. The single double-bayed property is the most altered,

with a lightwell and basement windows added, the architrave lost, the steps rebuilt and their wall and railings replaced by modern walls of excessive scale, which also lack the detailing of the originals.

- 4.5.23 Trafford Mount's other dwellings are less altered to their frontages and retain their timber architraves to the doors, with classical volutes (scrolling), although the doors themselves are modern. To Darwen Street the side gable incorporates a classical timber entrance with a round arched opening and a timber portico roof with matching corbelled eaves detailing, with the architrave and transom light retained (the door again modern). The timber cornice and corbelling to the Diocletian attic window above match the classical details of the entrance (with both historically matched by the side bay to no. 462 opposite, since demolished). A historic street sign with embossed lettering is attached at the corner of the gable.
- 4.5.24 Across the terrace, all windows are unsympathetic modern uPVC replacements, rooflights have been

added and all chimneys have been lost. It is not clear whether the rendering and painting of the plinth is historic or a modern alteration. This trend continues to the rear, where historic outriggers (closet wings) have been rendered and there is a poor-quality access gate and drive from the street **[Plates 4.43-4.45]**. Although still in residential use the terrace has lost all front gardens for hardstanding, with sections of stone walling removed in the south, where there is very poor-quality tarmac and extensive visual clutter (including bins).

- 4.5.25 Winfield Terrace (positive contributor), also two storeys, is situated on the opposite side of Chester Road and is architecturally humbler. Changes in height, brickwork and detailing across the terrace highlight its phased construction, built in stages as several small abutting groups rather than as a single purpose-built terrace.
- 4.5.26 The terrace is unified as a group by its red brick frontage and its regularity of design and (overall) scale, all but one of the buildings being single-fronted. With a more restrained design,

only one building has canted bays, this being a double-fronted property facing Trafford Mount, the others having a much simpler fenestration.

- 4.5.27 The terrace has again undergone change and most windows have been replaced, as have many doors, some fanlights have been blocked, and several chimneys have been lost, with rooflights also visible to the street. Some properties have also lost all or part of their boundary walls and railings, undermining what was originally a continuous enclosure to the street, with their front gardens bought into parking use **[Plate 4.46]**.
- 4.5.28 The northern end bay (to Chorlton Street) has been rebuilt, its use of poor-quality stretcher bond conflicting with the historic Flemish adjacent. Here, the side-bay frontage to Chorlton Street is a modern imitation of the end gable to Trafford Mount, whilst an inappropriate glazed extension fronts onto Chester Road.
- 4.5.29 The rears are dominated by extensions of an inappropriate height and scale, which are generally rendered, and have unsympathetic

modern windows, air conditioning units, rooflights and some overly dominant solar panels. These rears are again visible through permeable boundary treatments of a poor quality, particularly where metal security fencing is used. These expose the hard surfaced car parks that have replaced the earlier gardens, often erasing the original boundaries between the different plots to create single larger spaces **[Plates 4.47-4.48]**.

4.5.30 The terrace retains low stone boundary walls to Chester Road, although some sections are missing for hardstanding; hard surfaces dominate despite some soft hedgerows.

#### **Darwen Terrace, 4-6 Darwen Street (northeast side)**

4.5.31 Darwen Terrace (positive contributor) is only one of two surviving examples of a pair of purpose-built semi-detached dwellings (with the other, Almondbury Place, no longer recognisable as such). It is also the sole Victorian building surviving to

Darwen Street, this street appearing to have always been much less heavily developed than the housing to Chester Road and the Hadfield Street area. The elaborate side-bay entrances to Darwen Street at 464 Chester Road and (since lost no. 462) also suggest that it was of a comparatively high status.

4.5.32 Faced in red brick to the street-facing frontages (and brown brick otherwise), the building has a distinct decorative pattern that is orientated around a projecting flat double-height central bay topped with a broken pediment and flanked by single-storey canted bays. The windows to the upper storey, which include surviving timber sashes to no. 4, are set into recessed panels with stepped detailing and include some timber. historic timber street sign with embossed lettering is attached to the east elevation and both houses retain steps up to the front door, one with historic iron handrails. The building also retains a low brick boundary wall with stone copings and stone piers **[Plate 4.49]**.

4.5.33 An extensive industrial addition runs out of the building to its rear (northeast) and continues to the west to form an enclosed yard. These additions, which lie outside of the conservation area, appear to have originally been of an interwar date, with modern recladding to the street frontage and the apparent refronting of the courtyard range. This yard is accessed through a continuation of the historic brick boundary wall of Darwen Terrace, which runs to the northwest along what was originally the site of a second Victorian building (possibly a coach house, now demolished). This section of wall retains its gatepiers with conical stone copings, its stone copings and its metal railings, although metal sheeting has been applied across the railings and the gate is modern.

4.5.34 In front of the building, views run along Darwen Street and past the side returns of the terraces at its end towards the two-storey terraces of Chester Road.



4.5.35 Whilst the industrial buildings to its rear remain in use, Darwen Terrace appears vacant and requires some enhancement externally. There are detracting features including unsympathetic modern replacement windows, metal bars, roller shutters and security cameras and its front gardens are heavily overgrown.

4.5.36 Although they lie outside the boundary, the light industrial use of the yard and buildings to the immediate rear of Darwen Terrace is important to the character of the conservation area and its tradition of mixed-use residential and industrial activities.

4.5.37 Whilst Darwen Street appears always been less densely developed, the loss of the Victorian buildings to the immediate north of Darwen Terrace, and the truncation of the terrace to its immediate east (where the property fronting onto Darwen Street has been replaced by a car park), increases the building's feeling of separation from Chester Road.

4.5.38 This effect has, however, been reduced by the new housing situated in the proposed extension to the southern side of Darwen Street (and occupying a much larger footprint than the earlier, Victorian buildings), which reinstates the streetscape and the sense of enclosure and acts as a bridge to the road's western end. This good-quality housing has had the effect of uplifting this part of Darwen Street and now forms the end point of views to the west along Wright Street [Plate 4.50].

#### Issues

4.5.39 Character Area D shows evidence of a number of intrusive works and unsympathetic alterations, with scope for enhancement. To the built form these include, but are not limited to:

- Intrusive basement conversions with the introduction of balustrades and lightwells, causing an opening up of the front elevation and disrupting the otherwise regular proportions of the terrace.

- Prominent air-conditioning units.
- Poor-quality rear additions.
- Loss of pilasters between bays.
- Removal of chimneys.
- Excessive or overly visible rooflights.
- Cladding/rendering of eaves, stone banding and front steps.
- Rendering and painting of brickwork to side and rear elevations.
- Introduction of plastic fascias.
- Addition of dry verge systems (dry-fixing) at eaves level.
- Poorly sited rooflights, security grilles, air conditioning units and solar panels.
- Services inappropriately sited to front and side elevations.
- Inappropriate signage to front and rear elevations.
- Poor-quality palisade and security fencing to the rears, as visible at Manchester Street and Wright Street.
- Loss of front gardens for parking spaces.
- Poor-quality rear parking areas.
- Loss of front and rear boundary treatments.



4.38 Deva Court



4.39 Dover Terrace



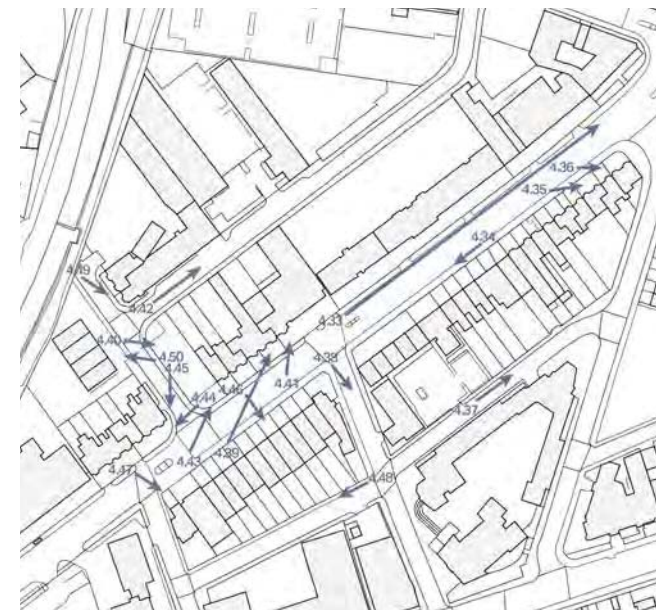
4.40 Rebuilt end gable of Dover Terrace



4.41 Refronted older building, likely Almondbury Place



4.42 Rear elevations of Dover Terrace and Almondbury Place, with detracting features



Character Area D photograph positions of views



4.43 Trafford Mount



4.44 End gable, Trafford Mount



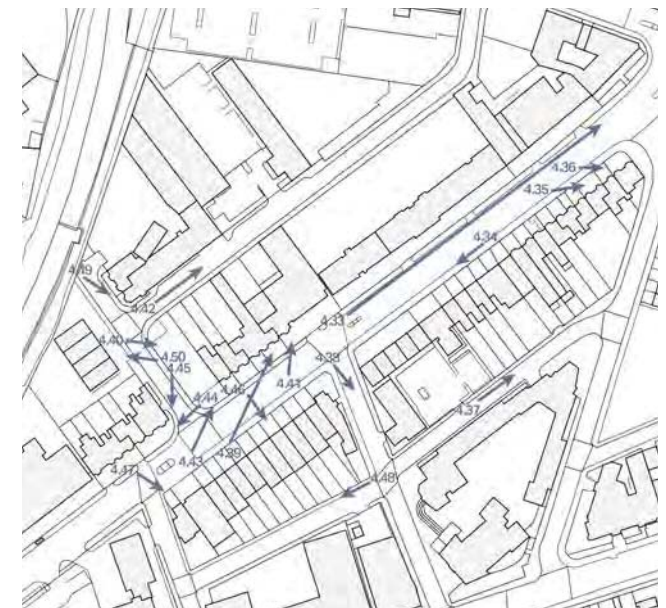
4.45 End gable and rear elevations of Trafford Mount



4.46 Winfield Terrace



4.47 Winfield Terrace



Character Area D photograph positions of views



4.49 Darwen Terrace



4.48 Rear elevations of Winfield Terrace

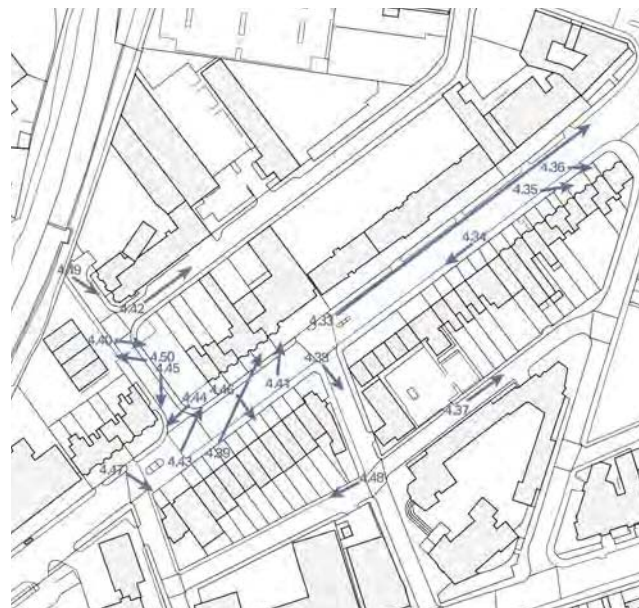
4.5.40 To the public realm in Character Area D, other detracting elements include, but are not limited to:

- Poor-quality public realm.
- Inappropriate long-term provision for cyclist safety.

There is a need for improved permanent solutions for the demarcation of the cycle lane that are sensitive to the conservation area.



4.50 Good quality modern housing, Darwen Street



Character Area D photograph positions of views

## 4.6 Proposed Extension of Character Area D to Northumberland Road, East Union Street and Stretford Road

### Listed Buildings

N/a

### Positive Contributors

Blackley Street, road setts (23)  
Old Trafford Technical Institute,  
540 Stretford Road (24)  
522-524 Stretford Road (25)  
518-520 Stretford Road (26)  
Hullard Hall Terrace, 516  
Stretford Road (27)  
18-20 East Union Street (28)

### Northumberland Road

4.6.1 Redevelopment has removed the historic development pattern and undermined the urban grain and rhythms of Northumberland Road and East Union Street. Both, however, offer views of the landmark buildings, focused on the Essence Factory, from their northern ends, Northumberland Road also providing framed views towards the sawtooth roofs of Empress Street.

4.6.2 The northeast end of Northumberland Road is now a large gap site, set behind graffitied hoardings and overgrown with trees and other vegetation. In front of this, a widening of the pavement contains an area of historic setts (their provenance unconfirmed), with young trees. This faces a similar set of setts to the opposite side of the street (which borders the sides of houses on Chester Road). To the rear of the gap site are three matching groups of townhouses (one with an adjoining apartment block), which front onto

Northumberland Road, Blackley Street and East Union Street respectively [Plate 4.51].

4.6.3 A second substantial gap site sits to the other side of the Northumberland Road, also overgrown. Across this the townhouses to East Union Street and Blackley Street are visible, the latter's frontage facing north and its rear (and parking bays) to the street.

4.6.4 The west side of Northumberland Road is initially occupied by the rears of the Victorian houses to Chester Road, backing onto Manchester Street. A modular five- and six-storey apartment building of multiple blocks then runs along the southern side of Manchester Street and the western side of Northumberland Road, with a further detached block fronting onto Stretford Road. Faced in brown brick, grey cladding and white render, and with parking to the ground floor, this is out of character in scale, materiality, height and style.

## **Blackley Street**

- 4.6.5 Blackley Street is a surviving rear service street and, uniquely, retains its surfacing of stone setts to the highway (positive contributor). Its north side is now dominated by the rears of its modern townhouses, which have small enclosed gardens, set behind parking spaces that are open to the street and appear to have absorbed the pavement.
- 4.6.6 To its east end are the rears of the properties to East Union Street, including the modern townhouses and the historic nos. 18-20 (positive contributor). Its southern side is similarly occupied by the rears of the buildings to Stretford Road, which include several positive contributors (for which see below). To both the east and south, poor boundary treatments, the insertion of dominant modern garages, and extensive hard landscaping for parking, all detract [**Plate 4.52**].
- 4.6.7 At the corner with Stretford Road are the rear and side return of the former Technical Institute (positive contributor). The side return matches the frontage in materials

style, albeit on a humbler scale and without the more elaborate ornamental detailing, whilst the rear gable ends are devoid of decoration, with a later extension particularly plain [**Plate 4.53**].

## **Old Trafford Technical Institute**

- 4.6.8 The former Old Trafford Free Library and Technical Institute (positive contributor) occupies the corner plot, with its principal frontage to Stretford Road. This is a landmark building both architecturally and in townscape terms. Built in red brick and terracotta, it is contemporary with the Essence Factory and echoes its materiality, style and detailing, although its turret has been lost. To its front, to Stretford Road, is a forecourt with hedge and red brick wall with stone copings [**Plate 4.54**].

## **526-534 Stretford Road**

- 4.6.9 Adjacent is a short terraced row of three-storey Edwardian buildings now converted for commercial use at ground floor. The frontages are heavily altered, and some rebuilt or refronted, with a marked loss

of detailing. All, however, retain their basic form and scale and their original gablets (although the roof coverings are modern). The frontages require enhancement, with the modern shopfronts poor, but the first floors above show a degree of interest (and there is also interest to the rear elevations, visible from Blackley Street) [**Plate 4.55**].

## **522-524, 518-520 and 516 Stretford Road (all positive contributors)**

- 4.6.10 This townscape group is comprised of two groups of terraced houses and a larger detached property, all being high-status red brick houses of two and three storeys. These are fine examples of upper-middle-class housing, their architectural detailing unrivalled in the residential buildings of the conservation area, the detached no. 516 (Hullard Hall Terrace) being especially ornate. This detailing includes stone architraves, overhanging eaves, brick chimneys (some with caps), dentilled eaves detailing, a mixture of single-storey bay windows

and two-storey canted bays, and brick chimneys. There are also red brick boundary walls, some with stone copings, and some original gatepiers **[Plate 4.56]**.

4.6.11 To the rear of 516 Stretford Road, fronting onto East Union Street, is a former outbuilding of Victorian and Edwardian age, now converted and altered and in vehicle repair use **[Plate 4.57]**.



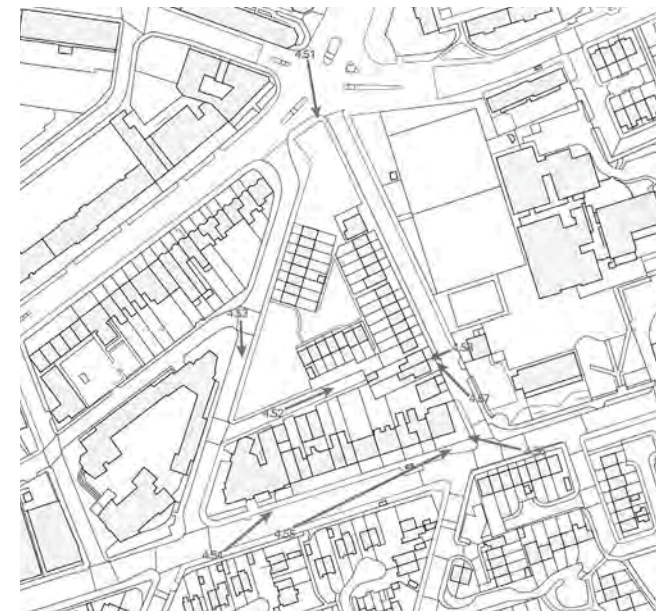
**4.51** Northumberland Road gap site and modern town house and apartment block development behind



**4.52** Rear of houses to Stretford Road



**4.53** Side and rear of the former Technical Institute



Proposed extension to character area D, photograph positions of views



4.54 Former Technical Institute



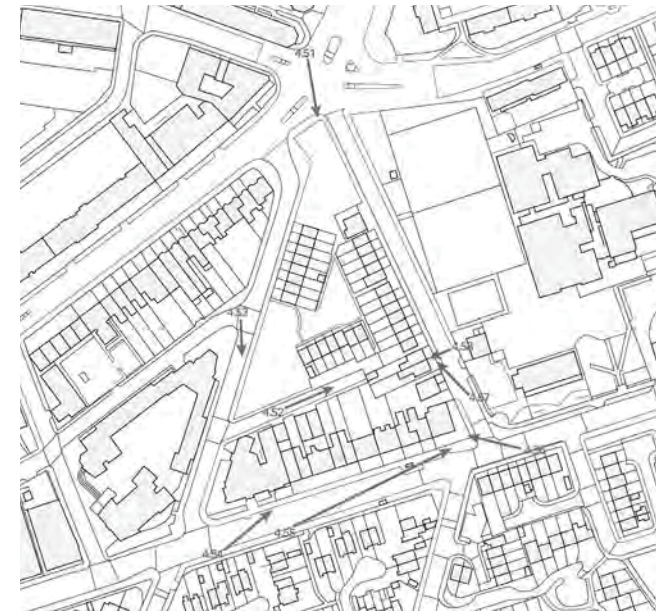
4.55 Stretford Road, nos. 526-534 in the foreground



4.56 522-524, 518-520 and 516 Stretford Road



4.57 East Union Street garage and nos. 18-20



Proposed extension to character area D, photograph positions of views



## East Union Street

4.6.12 East Union Street is heavily altered, retaining little of its historic character, but enhances the conservation area through the tree-lined school playing field that occupies its northwest. This amplifies the landscaping at the junction and adds visual amenity, forming a relief from the dense development and heavy hard landscaping that dominates the conservation area itself.

4.6.13 The east side of East Union Street is visually permeable, with open views through the fencing that encloses the playing field, playground and car park of the primary school, which sit in front of its modern, contemporary, buildings. An interwar brick substation sits between a sports court and the car park, visibly distinct from the contemporary buildings behind. To its own south are the wrought iron railings built for the original interwar school (Old Trafford Boys' Secondary School), which sit on a low stone plinth and have brick piers. The railings curve at the street's corner, where a modern gateway entrance has been inserted,

and continue along Stretford Road, where a modern nursery building has been added on the site.

4.6.14 The southwest corner of East Union Street is dominated by the cleared gap site at the junction with Chester Road, where trees again rise behind the graffitied hoardings and there are views towards the modern townhouses to Northumberland Road. A matching set of townhouses then runs along East Union Street, longer and divided into two blocks, having an additional five-storey apartment block to its immediate north.

4.6.15 The development of modern townhouses to East Union Street, Blackley Street and Northumberland Road is of three principal storeys, with dormer attic windows to the pitched roof above, primarily built in a red/orange brick. Much of its detailing is, however, out of character with the conservation area and the scale (towering over the historic buildings adjacent). The lack of variation between and

within groups is also detracting, these impacts all increased at the flat-roofed apartment block.

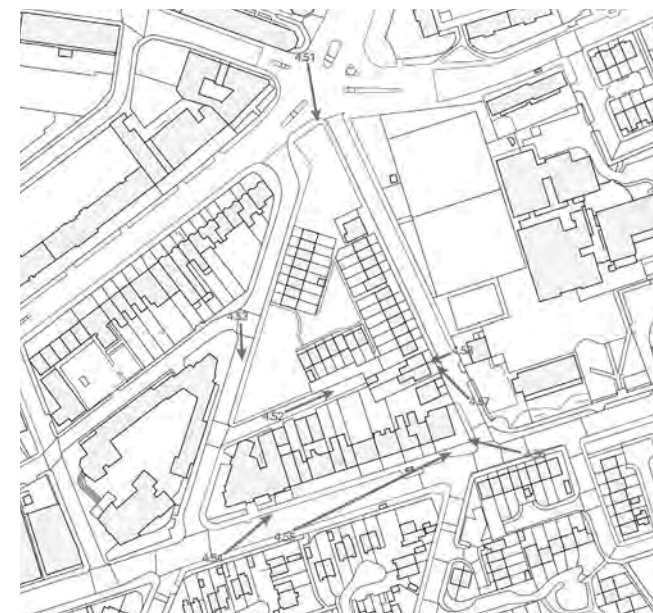
4.6.16 A green space enclosed between the townhouse blocks, to the rears of East Union Street and Northumberland Road, appears to be for residents only, and is not visible from any of the three streets.

## 18-20 East Union Street

4.6.17 These two adjoining two-storey late Victorian or Edwardian houses (nos. 18-20, positive contributor) survive from what was originally a short terrace of three. Much plainer and smaller in scale than 516-520 Stretford Road, they are faced in red brick with simple blue banding, their key ornamental features being two slate-covered hipped roofed bay windows, one with a surviving iron balustrade (albeit incomplete) **[Plate 4.58]**.



4.58 18-20 East Union Street



Proposed extension to character area D, photograph positions of views

## 4.7 Features of Historic Character

- 4.7.1 The character of the conservation area is defined by its architectural detailing, fabric and features. The period over which the conservation area developed, its mixed industrial and residential uses, the associated variety in scale, and the varied architectural status of its buildings, gives a pleasant variety in typology, architectural style and materials to its historic buildings. This reflects and articulates its historic development, creating a varied and distinctive townscape.
- 4.7.2 This variety is however underpinned by a number of golden threads that together create the distinctive character and appearance of conservation area – as reflected on the following pages.
- 4.7.3 Consequently, there should be no further loss of the interesting classical detailing, traditional palette of materials, and exposed brickwork – often with details or ornaments –

that characterises the conservation area and gives it its distinctiveness and special interest.

- 4.7.4 Further interest is derived from historic street name signs in cast iron and painted timber and the survival of areas of historic setts to forecourts/yards, pavements and the highway.



## Doorways

The main entrances to the grander terraces typically have a classical architrave (moulded doorcase) in timber, varying with the style of the building. On occasional terraces the doors sit in simple wooden frames but still have fanlight windows above. A number of properties retain their original entrance steps, some also having brick covered entrances. Many of the original timber panelled doors have survived and there are also examples of good modern replacements that have matched the historic panelled design.



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### Key:

1. Architrave with portico roof (Chester Terrace)
2. Simple frames (Birkdale Terrace)
3. Architrave with decorative corbels (Trafford Place)
4. Brick porch (Trafford Mount)
5. Simple architrave (Dover Terrace)
6. Original timber panelled door (Trafford Place)
7. Modern brick porch (Winfield Terrace)
8. Original steps and handrails (Trafford Mount)
9. Original timber panelled door (Trafford Place)



7.



9.



8.

## Windows

Historically the residential buildings were characterised by two-over-two sash windows to the larger openings, and either casements or one-over-one sashes to the smaller, all framed in timber. Industrial buildings had casement windows, often with opening lights, generally with metal frames but sometimes in timber. The grander buildings were characterised by fanlights, one and two storey bay windows and canted bays to the residential. Buildings, whilst oriel windows are also found on the landmark industrial buildings.

Some original glazing survives and there are sympathetic modern replacements.



### Key:

1. Two-over-two sash window (Birkdale Terrace)
2. One-over-one sash window (Birkdale Terrace)
3. Fanlight (Trafford Place)
4. Coloured glazing (Essence Factory)
5. Wooden framing (Essence Factory)
6. Decorative glazing (Essence Factory)
7. Metal framing (Essence Factory)
8. Security framing (Essence Factory)
9. Industrial windows (Empress Machine Works)



## Roofs

Roofs to the residential buildings are pitched, sometimes with gable ends, traditionally with a natural slate covering and often with steps in height across the terrace. There are brick chimneys on the pitch or ridgeline, traditionally with chimneypots of stone or clay, whilst the eaves overhang and have dentilled cornices or decorative bargeboards. Industrial buildings show a mixture of pitched, hipped, flat and sawtooth roofs, some with gable ends, gablets and even turrets.



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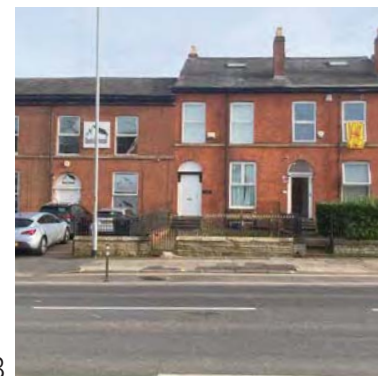
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### Key:

1. Pitched gable end (Birkdale Terrace)
2. Overhanging eaves with dentilled cornice (Dover Terrace)
3. Saw tooth roof (former Empress Machine Works)
4. Gable end (Empress Brewery)
5. Domestic chimneys (Dover Terrace)
6. Mixed industrial roofs (Essence Factory)
7. Bargeboard fascia (Birkdale Terrace)
8. Stepped height (Winfield Terrace)
9. Pitched gable ends (Former School Furniture Works)



7.



8.



9.

## Decorative Detailing

Detailing is most commonly in stone, brick (matching or contrasting) and ceramic brick tiling, with occasional use of timber. There are also examples of blue engineering brick, red terracotta and coloured faience (glazed terracotta) to the grander industrial buildings. Other features include break forward bays (often canted), oriel and Diocletian windows, dentilled eaves detailing and cornicing, stone aprons, and classical details such as pediments, colonettes and pilasters.



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### Key:

1. Timber dentilled cornice (Trafford Mount)
2. Blue brick banding (former School Furniture Works)
3. Stone name tablet (Chester Terrace)
4. Stone pediment (Empress Mill)
5. Segmented arches (Birkdale Terrace)
6. Segmented bays (Trafford Place)
7. Segmented arches (Former School Furniture Works)
8. Brick banding (Essence Factory)
9. Timber bargeboard fascia (Birkdale Terrace)



7.



8.



9.

## Boundaries

Original boundary treatments and sympathetic replacements are built of buff sandstone or red brick, with shaped coping stones above, and were historically finished with railings. Many have gatepiers in brick or stone, in varied styles, often engraved with the historic property name, many retaining their elaborate tops. The historic boundary treatments to the industrial buildings range from the decorative to the functional.



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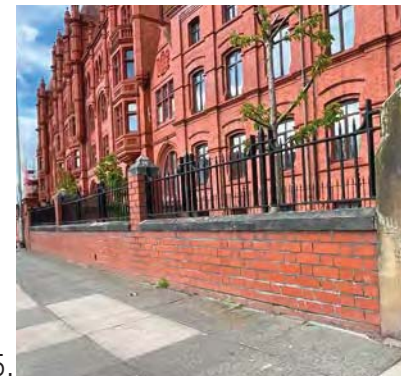
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Key:

1. Engraved gate pier (Darwen Terrace)
2. Stone gate piers (Essence Factory)
3. Coping stones (Trafford Place)
4. Stone gate piers (Birkdale Place)
5. Red brick wall (384 Chester Road)
6. Stone wall (Trafford Place)
7. Red brick wall (Birkdale Place)
8. Red ceramic wall (Essence Factory)
9. Stone gate piers (Chester Terrace)



7.



8.



9.



## Industrial Buildings

These show variety in their form, scale and detailing. Decorative details include friezes, pediments, corbelling, colonettes, pilasters and shaped windows, although much of the fenestration is industrial in character and some buildings are entirely plain. There is some presence of industrial features such as vehicle and loading holes and chimneys and, to a lesser degree, gate lodges, winch platforms and loading doors.



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Key:

1. Name tablet (Empress Brewery)
2. Decorative panels (384 Chester Road)
3. Industrial chimney (Essence Factory)
4. Vehicle door (Former School Furniture Works)
5. Gate lodge (Essence Factory)
6. Building name in brick (National Works)
7. Pilasters (Essence Factory)
8. Loading hole (Hadfield Works)
9. Metal stairs (Essence Factory)



7.



8.



9.

## Materiality

The predominant material is brick, in warm red, red/orange or red/brown, typically in Flemish bond, with detailing in cream/buff stone or tan brickwork. Several of the industrial buildings are faced in red terracotta or coloured faience, whilst others incorporate blue engineering brick. This materiality is matched in the boundary treatments, whilst windows and doors are typically in timber, with some examples of metal framing to the industrial buildings. There are also occasional examples of historic sett surfacing. Historic street signs are in timber, with embossed lettering, or metal, all painted, and building name stones in sandstone.



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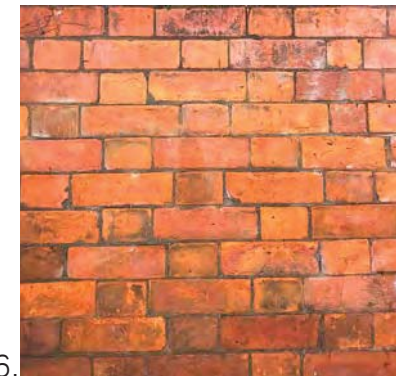
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### Key:

1. Brick boundary wall (Darwen Terrace)
2. Faience (Veno Building)
3. Brick and ceramic wall (Essence Factory)
4. Red/brown brick (Darwen Street)
5. Sandstone wall (Deva Terrace)
6. Red/orange brick (Essence Factory)
7. Blue engineering brick (Former School Furniture Works)
8. Brown brick (Hadfield Street Works)
9. Setts (Essence Factory)



7.



8.



9.

## 5 Views Within the Conservation Area



## 5 Views Within the Conservation Area

5.0.1 Views within the area are defined primarily by the linearity of its streetscape. This creates both longer-range views along the frontages and clusters of shorter views at the important nodal connections between key streets.

### 5.1 Linear

#### Chester Road

5.1.1 Linear views are particularly important to the southern end of Chester Road. Here, the view is framed and channelled by the terraces to either side in a manner highlighting their uniformity and their regular, comparatively small, scale. The more enclosed and intact nature of the frontage creates long views up and down the street that incorporate the long terraced blocks and the commercial development around the Essence Factory, all of which are essential to an understanding of the area's historic evolution. This view is enhanced by the degree to which the built form stacks up around the Essence Factory, creating an interested layering effect that marks

the importance of the central heart of the conservation area and allows appreciation of the architectural dominance of the Essence Factory and other surrounding landmarks. This also creates a framed view channelled to the west between the buildings of Empress Street, its focal point the sawtooth roofs of the north side of Empress Street – a key townscape feature in shorter-range views.

5.1.2 This appreciation of the streetscape is dynamic and includes greater reveals to the area's landmarks on approaching the junction of Chester Road and Empress Street where, for example, the chimney of the Empress Brewery comes into view. This chimney is an important landmark within the conservation area.

5.1.3 Views along the northern section of Chester Road are undermined by its fragmented street scene and the curvature of Chester Road, which isolates it from the remainder of the area. Nonetheless, however, there are important reveals to the Empress Brewery when moving

south, as well as reveals to the intact run of Birkdale and Chester Terraces once past the gap in the street scene to the south of Cornbrook Terrace.

5.1.4 Negatively, modern tall buildings in the north, immediately beyond the conservation area, have the effect of extending the view, introducing competing focal points in the backdrop to the landmark buildings. Their dominance in the view compromises the visual impact of the conservation area's own landmark buildings. It also undermines the stark differences in scale that would otherwise establish a prominent visual contrast between these historic industrial/commercial buildings of scale and the small terraces immediately facing and abutting **[Plates 5.1-5.3]**.



**5.1** Linear views are strong along Chester Road, picking up the small scale, uniformity and relatively continuous frontage



**5.2** Views along Chester Road are channelled towards landmarks, which stack up on the streetscene, with distant tall buildings overly prominent



**5.3** Views south along the northern part of Chester Road are more fragmented, but do provide important lead ins to the Essence Factory and Empress Brewery building

## Hadfield Street and Surroundings

5.1.5 Linear views are also evident on Hadfield Street and are particularly important in underpinning the area's industrial heritage. Low-scale brick industrial buildings with smaller footprints are common here and are in notable visual contrast with that small number of landmark industrial buildings focused around Empress Street. Whilst the broadly lower building height of this area limits any dominance from individual buildings, views along Hadfield Road do pick up the broad industrial development pattern with its low-lying, long, red brick blocks.

5.1.6 The Essence Factory is the dominant building and focal point in these linear views, which should not be undermined.

5.1.7 There is likewise a particularly important reveal to the Empress Brewery and Empress Mill when moving south, with the former's chimney an important landmark feature puncturing the horizon.

5.1.8 Further north, the chimney of Empress Brewery is a key feature, and on moving south the dynamism of the streetscape gradually reveals more of the brewery façade,

which coalesces with Empress Mill and the rear of the Essence Factory to create a commercial and industrial set piece.

5.1.9 The strong axial pattern of the streetscape where it survives along Lund Street and Turner Street also creates important views along their industrial frontages, including the well-articulated gables of the former School Furniture Works on Turner Street. Views along Lund Street similarly take in the retained historic former Pomona Engine Works and its red brick chimney **[Plates 5.4-5.7]**.



**5.4** Views south on Hadfield Street hint at the important commercial cluster within the conservation area whilst revealing the Empress Brewery chimney



**5.5** Key parts of landmarks such as the Brewery and Empress Mill open up when moving south on Hadfield Road



**5.6** Linear views along Turner Street frame the highly characteristic articulated gables



**5.7** Linear view to along Lund Street exposes historic industrial complexes to its south and end

## 5.2 Nodes

5.2.1 The convergence of the area's principal roads creates key nodal points in the development pattern. This serves to bring the area together, creates nodes of built form in the immediate street scene and provides views outwards, aiding legibility of the overarching pattern. This is particularly important at the junctions of Empress Street and Chester Road and of Empress Street with Hadfield/Wright Street.

5.2.2 These two junctions create a dynamic experience on moving through the area's most prominent commercial and industrial built form. It is important that they are considered together as part of a connected urban pattern that enables static views on the junctions themselves and create reveals when moving between the two.

### Empress Street and Chester Road

5.2.3 As the heart of the conservation area, this junction is a critical townscape element. Views to the south and west bring the area's commercial, industrial, and residential buildings together into a cohesive streetscape. This streetscape is defined by the significant presence of the Essence Factory and Empress

Brewery in the immediate street scene, with views into the industrial heartland to the west.

5.2.4 The streetscape is marked by the corner of Empress Brewery, the sawtooth roof form of the Empress Street complex to the west and residential terraces to the south, the latter primarily in the long frontage of Trafford Place (views north being restricted by the street curvature) **[Plates 5.8-5.11]**



5.8 Reveals when moving out onto the junction of Chester Road and Empress Street open up views along the cohesive residential frontage



**5.9** Key landmarks, such as the Empress Brewery dominate the junction of Chester Road and Northumberland Road



**5.11** The full force of The Essence Factory when moving onto Chester Road from Northumberland Road



**5.10** Views south from the junction of Chester Road and Northumberland Road bring together the commercial and residential built form



## Empress Street and Hadfield Street

5.2.5 Moving through the linear and enclosed streetscape of Empress Street, the next junction opens up views across the industrial heartland to the northwest (Character Area B). Whilst less distinct than the commercial heart, due to the lack of landmarks beyond the immediate mass of Empress Brewery, this is nonetheless characterful, picking up on the looser and more ad hoc nature of this industrial area.

5.2.6 Elements of note include sawtooth roof forms and the long linear red brick development blocks orientated north-south, all of which combine to articulate the character area's industrial heritage.

5.2.7 Views back to the junction with Chester Road are enhanced by the enclosure provided to the street that is by Empress Brewery and the National Works building. Further enhancement is provided by the rear elevation and roofscape of the Essence Factory, these shorter-

range views of the landmark buildings combining with views over the industrial heartland to stitch together the area's commercially focused history.

5.2.8 Whilst the residential built form is not visible, the increase in soft landscaping and tree coverage that terminates the view suggests a non-industrial use, giving legibility to the townscape **[Plates 5.12-5.15]**.



5.12 Views back to Chester Road from the junction of Empress and Hadfield Streets



5.13 Views north on Hadfield Street are low lying but include important industrial typologies



5.14 Views over the industrial belt include important industrial sawtooth roof forms



5.15 The prominent rear elevation and yard of the Essence Factory from the junction of Empress and Hadfield Streets

## 6 Setting of the Conservation Area



## 6 Setting of the Conservation Area

6.0.1 The setting of the conservation area enables an appreciation of its key built and urban forms and reinforces the legibility of its historic growth. This legibility is primarily related to the geographical context that enabled its relatively rapid growth as an industrial and residential area, or which serves to link it to the wider pattern of growth in the area.

6.0.2 Of particular importance are the historic transport infrastructure, including the docks and railway, the presence of neighbouring residential areas, and the area's close proximity to Trafford and Manchester.

### 6.1 Wider Setting

6.1.1 The elevated platform of the Cornbrook Metrolink Station dates from 1999 and has introduced a new vantage point that was not present historically. This allows views of and across the conservation area from the northwest, showing the rear and roof of the Essence Factory in particular, set against the industrial buildings of Character Areas A and B **[Plate 6.1]**.

6.1.2 In addition, the very upper parts of the Empress Brewery chimney and Essence Factory have some visibility when viewed from the Pomona Metrolink station. This view does not, however, provide any meaningful legibility of the conservation area's character, as the landmark buildings are very diminutive in the view and the surrounding context is so heavily modern **[Plate 6.2]**.

6.1.3 Otherwise, the way in which the area has evolved has largely severed the visual – and, in many instances, physical – links between the conservation area and that area in the northwest that incorporates the Bridgewater Canal, the Pomona Docks and the Manchester Ship Canal. As a result, any contribution to the special interest of the conservation area derived from its setting is generally very localised.



6.1 View of the conservation area from the Cornbrook Metrolink platform



6.2 View from Pomona Metrolink station (zoomed in to identify the relevant features)

## 6.2 Local Setting

6.2.1 There has also been a significant level of redevelopment in the conservation area's more immediate setting. Redevelopment in the immediate surroundings is of mixed age and form, comprising low-rise, small-scale, light industry, 20<sup>th</sup> century estate-style housing and, more recently, modern apartment blocks, initially mid-rise and now, increasingly, high-rise. With no consistent character to redevelopment, the character of the area has been compromised, undermining its sense of distinctiveness, place and identity. This has also removed the historic grain, street plan and development that together gave a consistent rhythm and regularity of character, united the area through a general cohesiveness and gave a strong sense of enclosure to the street through its consistent frontages and boundary treatments.

6.2.2 The spread of tall buildings is particularly detracting, undermining the traditional social and economic character of the area, its architectural traditions and its sense of distinction and differentiation, as part of Trafford, from Manchester city centre. These buildings have an overly dominant presence on the horizon and in views, transforming the spatial and visual character of the conservation area. With no concession to the area's traditional architecture or its historic character, they are detracting to the conservation area in their inappropriate scale, mass and height, and in their unsympathetic materials and architectural style, which the use of setbacks and the provision of soft landscaping fail to mitigate.

6.2.3 The tall development of Manchester city centre is visible throughout the area and forms an ever-present, detracting, backdrop to views of and through the conservation area.

### **Chester Road**

6.2.4 The immediate continuation of Chester Road to the north and south is marked by modern development. To the south there is some interest in the longer-range views along the road that pick up both the floodlights of the cricket ground and the smaller scale-built form. However, the modern development to the conservation area's immediate south is particularly detrimental as a result of its large and scale and relatively poor architectural quality. This southern exit from the conservation area could not in itself be considered a gateway, the historic form simply stopping and the character of the area changing rapidly to one of much looser grain and higher scale.

6.2.5 To the immediate southeast of the conservation area, on the east side of Chester Road, the Westpoint development dominates the horizon and is highly detracting. This tall apartment block is a dominant feature at the end of views south through the conservation area, dominating

the immediate background to the view and introducing a competing, visually poor, focal point in the backdrop to the terraces. This building towers over these smaller and lower scale terraces whilst stepping forward to the edge of the pavement, further increasing its visual prominence by breaking the otherwise consistent frontage. The unadorned brown brick of its blind staircore has become the end point and even the focal point in views south along Chester Road, with unbroken rows of continuous glazing to its side giving an office-like feel, whilst the shallow footprint behind the frontage gives a poorly

proportioned appearance and a sense of hollowness to the street's edge [Plate 6.3].

6.2.6 Westpoint also includes a multi-storey car park, which similarly fronts onto Chester Road. Its position set back behind a driveway and area of lawn reduces but does not remove its detracting visual impact in middle-range views along Chester Road and does not mitigate its impact at short range. In its austere design and materials and its scale and mass, rising above the terraces adjacent, this inappropriate building is also highly detrimental to the conservation area [Plate 6.4].

a sudden transition from the tightly packed, street-fronting, terraces of the conservation area to this set back, much more permeable, development of modern housing is discomfiting. The effect is to reduce the legibility of the conservation area itself [Plate 6.5].



6.3 Continuation of the road south is generally poor, with larger scale buildings of limited quality

6.2.7 To the northeast, the terraces and villas to the northeast side of Chester Road are gone, and the 20<sup>th</sup> century development that replaces it is set away from the main road in an estate pattern, removing the tight historic grain, street plan and development pattern that maintained the regularity of rhythm along Chester Road. Although the soft landscaping that separates this from Chester Road does add to the environmental quality, such



6.4 Large car park to the south of the area on Chester Road

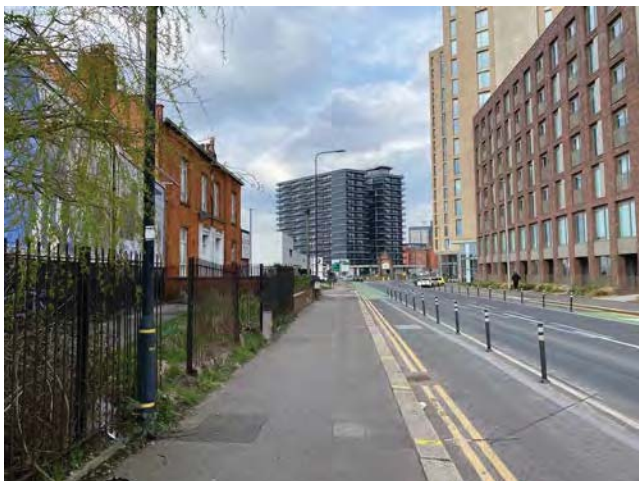
- 6.2.8 This estate also includes two 15-storey tower blocks, one set back within the interior but the other, Empress Court, facing onto Chester Road and substantially out of scale with its surroundings. This dominates views looking north through the conservation area, undermining its landmark buildings, introducing a discordant element on the horizon and establishing a competing focal point, all further exacerbated by its inappropriate white colour palette.
- 6.2.9 To the immediate north of this estate is the Botanica apartment and townhouse development, which



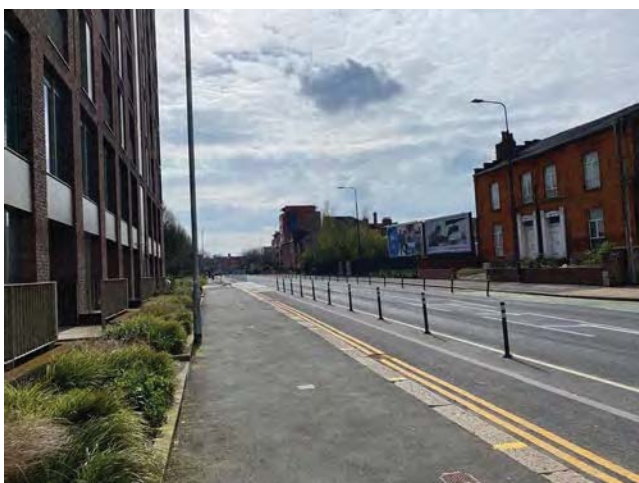
6.5 The context of the area to the northeast of Chester Road is wholly changed

at its highest rises to 19 storeys and at its lowest seven. Occupying an extensive site, again fronting directly onto Chester Road, the development reinforces the area's spatial relationship with Manchester city centre but is highly intrusive and out-of-keeping with the low-rise historic built form facing (at Cornbrook Terrace) and to its immediate south. Its three tower blocks are positioned directly adjacent to Chester Road, where they tower over their surroundings to the south and west: efforts to reduce the sense of mass fail to mitigate the impacts of its excessive proportions and scale. This similarly compromises the legibility of the boundary between Manchester city centre, with its high-rises, to the immediate north of the Manchester Arch and Trafford to its immediate south, which has a more suburban character and lower, less intensive, built form. It is important that future development does not bring any further undermining of this administrative, spatial and character boundary and differentiation **[Plates 6.6-6.7]**.

- 6.2.10 Immediately beyond the Manchester Arch, the Vox (two 15-storey buildings) and Trilogy (three buildings, nine- and 12-storeys high) mark the beginning of the high-rise development situated in Manchester city centre. Their mass, scale and height overwhelm the small Victorian buildings that survive to Chester Road.
- 6.2.11 This area to the immediate north does offer intermittent views to the chimney of the Empress Brewery and the Essence Factory from the junction at Bridgewater Way. This, however, is a heavily trafficked area that truncates the pedestrian experience of the historic environment. The approach is further marred by the poor condition of Cornbrook Terrace and its extension and advertisement hoarding, and the immediate break in the street scene beyond **[Plate 6.8]**.



6.6 Modern development to the north



6.7 Modern development to the north (note the chimney to the background)



6.8 The northern gateway is marred by advertisement hoarding and the current condition of Cornbrook Terrace

## Stretford Road

6.2.12 Some higher levels of historic context survive to the southeast of the area, focused on Stretford Road. This includes the early 20<sup>th</sup> century Technical Institute (positive contributor) as well as a run of well-appointed 19<sup>th</sup> century terraces (nos. 516, 518-520 and 522-524, all positive contributors) and a surviving set of wrought iron gates (from a mid-20<sup>th</sup> century school). Only the rear and side elevation of the Institute have any visibility from the conservation area itself, being visible along Northumberland Road from the rears of Trafford Place on Manchester Street.

6.2.13 This area developed at the same time as Empress and there was historically connecting development between the two, focused primarily on East Union Street. This incorporated terraces, a bowling green and a police station (all now lost), whilst Northumberland Road was primarily occupied by a printing works (also since demolished).

6.2.14 There has been substantial modern redevelopment in the surroundings

and along these connecting streets, with East Union Street retaining only one of its pre-20<sup>th</sup> century buildings (18-20 East Union Street, positive contributor). Between these two streets, however, Stretford Road retains much more of its historic character as does, to some extent, the smaller Blackley Street.

6.2.15 Despite this undermining of their own historic character, East Union Street and Northumberland Road do provide key views into the conservation area. This is a dynamic experience where landmarks are gradually revealed on approaching Chester Road, whilst the upper sections of the Essence Factory also slowly come into view along Northumberland Road.

6.2.16 The approaches on East Union Street and Northumberland Road are particularly important, bringing into view the Essence Factory and Empress Brewery. They are, however, marred by the current condition of the vacant plot at the corner of the junction, on the former site of the Northumberland Public House **[Plates 6.9-6.14]**.



6.9 Minor visibility to the terraces and former Technical Institute on Stretford Road from Manchester Street, with the exposed elevations altered



6.10 The roof of the Essence Factory is visible from the west end of Northumberland Road





**6.11** The roof of the Essence Factory has a clear presence at the conservation area boundary along Northumberland Road adjoining Manchester Street



**6.12** The facade of the Empress Brewery terminates views along East Union Street



**6.13** Empress Brewery, including the chimney, becomes increasingly prominent moving west on East Union Street



**6.14** On East Union Street the area's landmarks open up at the gateway to the conservation area

## 7 Assessment of Condition and Other Threats



## 7 Assessment of Condition and Other Threats

7.0.1 The conservation area has been on Historic England's Heritage At Risk Register since 2012, indicating significant concerns over its decline and continued trend. The issues specific to the area include a combination of smaller-scale loss of detail with larger-scale redevelopment, shifting land uses, and stalled development.

### 7.1 Loss of Spatial Quality

- 7.1.1 The residential parts of Chester Road once had a much greener quality than they do today, with the houses having gardens to the rear (set behind tall brick boundary walls) and modest landscaped plots to the frontage, visible across the low boundary walls. This would have been in stark contrast with the denser urban blocks to the northwest (since demolished).
- 7.1.2 This green character has been lost and the environment is significantly harder and more urban. Across the conservation area a number of front gardens and almost all of the rear gardens have been hard landscaped to create off-road parking areas and, in some examples, light industrial storage. This is associated with the extension and subdivision of former single-family homes for apartment use and with a shift in use from residential to commercial across many other parts of the historic terraces **[Plate 7.1]**.

- 7.1.3 There are some exceptions, including a number of intermittent hedgerows and the pleasant landscaping to the front of 384 Chester Road, which does much to reinforce this earlier character.
- 7.1.4 The residential quality of areas to the northwest of Chester Road has been wholly lost following 20<sup>th</sup> century demolition of their terraced streets and their replacement by industrial uses.



**7.1** Loss of gardens, and alteration to rear elevations along Winfield Terrace

## 7.2 Loss of Detail and Alteration

7.2.1 Management of the historic dwellings over time, including conversion from single-family dwellings to apartments and commercial premises, has resulted in a high level of small-scale change, the loss of historically appropriate detailing and the introduction of inappropriate modern materials. This is primarily evident in the rendering of previously unrendered brick walls, the addition of cladding, a loss of ornamental architectural features, the replacement of slate

roofing tiles with concrete, the introduction of modern doors and windows in a mixture of styles and materials (including uPVC), the removal or truncation of chimneys, the loss of clay chimneypots, and the addition of rooflights and dormers [Plate 7.2].

7.2.2 Other inappropriate changes include the addition of solar panels, air conditioning units and basement windows and lightwells.

7.2.3 There has also been a high level of alteration to the built form to the domestic terraces. This has included the rebuilding of end gables and bays, such as those to Winfield and Dover Terraces, with a lower quality brickwork and bond pattern [Plate 7.3]. Particularly detrimental is the alteration to rear elevations, where the historic pattern of outriggers has seen significant infill, rebuilding and replacement, with major changes to fenestration [Plate 7.4]. The alteration of prominent rear elevations to the terraces is particularly detrimental.



7.2 Loss of boundary wall and garden to Cornbrook Terrace



7.3 Changes to windows, and a rebuilt end bay, to Winfield Terrace



7.4 Poor quality rear elevation and land to Dover Terrace

7.2.4 In addition, all of the rear gardens have been lost for hard surfacing.

7.2.5 Whilst some boundary walls have also been removed or shortened to the building frontages, to allow for off road parking, these generally remain a prominent feature in the domestic terraces and the landmark industrial buildings to Chester Road. On closer inspection many of these appear to be replacements, with changes in brick finish and stone size and intermittent railing designs, but broadly they maintain the overarching sense of a uniform frontage. Gatepiers have, however, been a particular loss to residential properties, although present in full at Darwen, Birkdale and Cornbrook Terraces, with partial or one-off survivals at Trafford Mount, Chester Terrace (damaged) and Trafford Place. There are also examples of poor replacement boundary walling and gatepiers, which has resulted in a loss of detailing and the introduction of lower quality materials, as at Cornbrook Terrace and the Veno Building.

7.2.6 Boundary treatments are particularly poor to the rear of many of the terraces, where the walls have often been removed entirely to allow vehicular access for both parking and commercial uses. These have generally been replaced with poor-quality fencing and vehicular gates, with metal palisade and security fencing being particularly detrimental and creating a highly defensive aesthetic. This is not only visually poor in itself but also exposes the utilitarian hard landscaped parking areas within to general view, whilst undermining the enclosure that traditionally characterised these small back streets.

## 7.3 Fragmentation of the Street Scene

7.3.1 The historic street scene across the area is fragmented as the result of a combination of wartime bombing, 20<sup>th</sup> century clearance and stalled development. This has created not only areas of redevelopment but also gap sites, the latter generally being visually poor.

### Redevelopment

7.3.2 Redevelopment to the residential areas is generally restricted to the main frontages, where gap sites are most prevalent. This, however, has introduced buildings of inappropriate scale and height both inside the conservation area and within its immediate setting. This detracts from the character of the residential character areas, undermining their traditional human-scale of two and three storeys and compromising their generally consistent roofline and roofscapes. In particular, whilst the modern apartments adjoining Chester Terrace do maintain the enclosure to Chester Road by respecting the

established boundary line they are out of scale and in this way truncate the streetscape.

7.3.3 The main impact of redevelopment is evident in the industrial area northwest of Chester Road. Whilst the industrial uses of this area will evidently result in more ad hoc built form, the loss of the historic terraced streets has removed the historic street pattern and associated sense of enclosure to the degree that most of these former sites are excluded from the conservation area. There is also some poor-quality built form fronting Hadfield Street.

7.3.4 Of particular concern, however, is the site adjoining Cornbrook Terrace, where this redevelopment encroaches onto Chester Road. The built form here is very poor and is sited in such a way as to turn its back on Chester Road. Whilst this limits the visual impact of these buildings to Chester Road it has created a frontage of poor railings and overgrown landscaping, breaking the otherwise continuous frontage and degrading the quality of the environment **[Plate 7.5-7.6]**.



**7.5** Modern development turns its back on Chester Road with poor environmental quality



**7.6** Poor quality redevelopment to Hadfield Street following the loss of terraces

## Gap Sites

- 7.3.5 Gap sites are a primary concern. As well as the stalled site of development at Veno (see below), the former industrial courtyard is vacant behind Empress Mill, legible only through its retained historic gatepiers to the Wright Street frontage.
- 7.3.6 There are also key gap sites adjoining the National Works building and the end of Dover Terrace, whilst its poor fencing and overgrown shrubbery means that the commercial site adjacent to Cornbrook Terrace also reads as vacant to Chester Road **[Plate 7.7]**. These sites were all historically residential.
- 7.3.7 Of further concern is the vacant land at the junction of Chester Road and Northumberland Road (outside the current boundary), which was historically the site of the Northumberland Public House.



7.7 Car park to the rear of the National Works building, historically occupied by terraces

## 7.4 Vacancy and Stalled Development

- 7.4.1 Of the seven principal landmark buildings only the Empress Brewery, 384 Chester Road and the National Works buildings are occupied. The Veno and Trafford Press buildings, Empress Mill and the Essence Factory are all vacant (although plans are in place for the conversion of the latter to religious use).
- 7.4.2 This vacancy is causing significant deterioration in their own condition and the visual quality of their immediate settings through a lack of maintenance. Failure to secure their sites has resulted in incidences of fire, arson and fly tipping at both Empress Mill and Trafford Press.

7.4.3 Related to this is stalled development. Plans to convert Empress Mill and the Veno and Trafford Press buildings are all incomplete, with associated concerns over substandard construction. This has not only left the buildings vacant but has also left a legacy of half-completed construction, with associated deterioration in their plots **[Plates 7.8-7.11]**.

7.4.4 Whilst most of the terraced properties seem to be in use, Cornbrook Terrace appears underused and Darwen Terrace potentially partly or fully vacant.



7.8 Fire damage to Empress Mill



7.9 Deterioration at the Trafford Press building





7.10 Deterioration and Heras fencing to the Venetian Building



7.11 Stalled development adjoining the Venetian Building

## 8 Boundaries



## 8 Boundaries

8.0.1 Section 69(2) of the Planning (Listed Buildings and Conservation Areas) Act 1990) requires local authorities to review the boundaries of their conservation areas from 'time to time' to determine whether any additional areas should be brought into their boundaries. The most recent review was completed in 2016. A new review has been carried out as part of the production of this Appraisal, which as a result proposes to bring three further areas into the conservation area. These are the industrial area to the west of Hadfield Street (1), the land to the south of Empress Mill (2), the area bound by Northumberland Road, East Union Street and Stretford Road (3) and the southwest side of Darwen Street (4).

### **Character and Appearance of the Conservation Area and its Proposed Extensions**

8.0.2 The Empress Conservation Area is of special interest as a rare survivor of the dually residential and industrial communities that sprang up in Trafford in the second half of the 19<sup>th</sup> century as its new transport connections transformed its earlier, mainly rural, middle-class suburbs into dense urban areas. This duality to Trafford's historic development is expressed and preserved within the Empress Conservation Area and its immediate surroundings, where manufactories, industrial offices/warehouses and rows of terraced housing are all found within a comparatively small geographical area. These buildings

show a full range of ages from the Victorian period to the modern day, highlighting the way in which this essentially Victorian character continues to define the area and shapes its evolution into the future.

8.0.3 Three of the proposed additions share in, and further articulate, this character that makes Empress of special interest **[Plate 8.1]**.



8.1 Current and Proposed Conservation Area Boundaries

## (1) Proposed Addition to the West of Hadfield Street

8.0.4 This area forms part of a visual group with the landmark buildings further to its east/southeast and has the potential to contribute to the wider regeneration of the area. Whilst many of the historic buildings have been lost there is some survival of these manufactories, warehouses and works and the area retains its intrinsic working character and the integrity of its development pattern. Some of these buildings are already included within the conservation area and it is proposed to bring the remainder of this industrial land into the boundaries to reflect its historic interest.

8.0.5 This area's surviving historic buildings have architectural and historic interest in their own right whilst also highlighting the diverse character of the factories, mills and warehouses that existed and continue to exist side-by-side and the associated diversity of their activities and products. Whilst the historic buildings are less

ornate than those on Empress Street and at the junction with Chester Road, they remain critical to an understanding of the area's functional evolution. Together, these sites run right up to the railway tracks that were vital in the emergence of this industrial heartland, articulating its extent and integrity (both past and present).



8.2 Land to the rear of Empress Mill

8.0.6 Of particular note are the former Pomona Engine Works, which retains its chimney and its primary range (although altered), and continues to have an important townscape function, and the substantial complex to Empress Street to its immediate west, where the "north lights" are also an important townscape feature [Plate 8.3].



8.3 Historic manufactories to the west and south of Lund Street, including the chimney of the former Pomona Engine Works

## (2) Proposed Addition to the South of Empress Mill

8.0.7 This is a vacant, and deteriorating, triangular plot of land to the rear of Empress Mill, fronting Wright Street. At present its condition is detrimental to the quality of the local environment, offering a prime enhancement opportunity. This site has visual prominence, adjoining one of the key buildings in the area, and is also of historic interest.

8.0.8 This site has a historic, functional and spatial relationship with Empress Mill adjacent, which was once a much more extensive industrial complex, its buildings and courtyard extending over this land to the south, which also appears to have contained its entrance. This historic function is still legible in the retention of prominent gatepiers and sections of boundary wall and on Wright Street. Whilst this land is now vacant it forms part of the immediate setting of the surviving Empress Mill building both physically and visually and through their associative relationship. Both Empress Mill and this land

to its south have the potential to contribute positively to the area's regeneration.

8.0.9 The association with Empress Mill, retention of historic fabric and potential for the area's enhancement warrant the area's inclusion within the boundaries.

## (3) Proposed Addition between Northumberland Road, East Union Street and Stretford Road

8.0.10 This proposed addition forms a discrete townscape block bound by roads to all sides and fronting onto the major arterial roads of Stretford Road and Chester Road to either end. This once comprised a civic focal point to the wider area and contains not only the former Technical Institute and several architecturally high-status houses, all positive contributors of architectural and historic interest, but also a parade of shops converted from dwellings. **Although now containing some modern development, the area retains** a recognisable special architectural and historic interest, which is tied heavily to that of the

existing conservation area, the two areas having historically formed part of a single united social and architectural landscape.

8.0.11 Developed at the same time as the conservation area, and likewise comparatively domestic in character, the townscape group surviving to Stretford Road and East Union Street was formed as part of a wide common townscape and streetscape extending through the area and also including the conservation area. It was part of the same community and network of streets, shares in its architectural form in the historic buildings and brings architectural and historic interest, particularly in its two listed buildings/groups.

8.0.12 In addition, several of these buildings exhibit a comparatively ornate level of detailing, the Technical Institute complementing the higher status industrial properties, primarily the Essence Factory, and its finest dwellings showing a level of ornamentation not seen within the conservation area itself (which has suffered the loss of its grander buildings).

## Northumberland Road

8.0.13 At the road's north, with a broad frontage to Chester Road, is the substantial piece of vacant land historically occupied by the Northumberland Public House, terraced housing to East Union Road and various light industrial/ horticultural buildings. This was removed from the conservation area in 2016 but occupies a key, prominent, position at the north gateway into the conservation area and, as such, has a strong visual, spatial and historic relationship with the group of landmark

buildings directly opposite. It has the potential to be a landmark site, features prominently in views along Chester Road from the north, has a strong presence in views to the east from both Empress Street and Chester Road, and forms a major component of the setting to the landmark buildings opposite, the heart of the conservation area. This site is, therefore, a key development opportunity with substantial potential to make a positive contribution to the conservation area and to support its improvement and enhancement [Plates 8.4-8.5].

8.0.14 The northern end of Northumberland Road also makes an important contribution to the setting of the northern gateway into the conservation area, the group of landmark buildings at its heart and the residential terrace to the northern end of Chester Road. From the northern gateway into the conservation area views run down Northumberland Road in a southeasterly direction, funnelled by the wide street mouth (which adds a valuable sense of spaciousness to the otherwise heavily enclosed Chester Road). These views are framed to either side by the historic setts and trees that sit at the road's end, bridging the Chester Road terraces in the west and the currently vacant land to their immediate east. As the sole area of historic setts to the pavement this area is of great importance to the historic character of the area and should be protected, whilst the trees add much-needed soft landscaping to a conservation area otherwise entirely devoid of street trees.



8.4 Land bounded by Northumberland Road and Chester Road



8.5 Land bound by Northumberland Road and Chester Road (former Northumberland Public House)

8.0.15 The middle section of Northumberland Road is also altered, with modern residential development and a second vacant site **[Plate 8.6]**. The latter has the potential to make an important contribution to the setting of, and encounter with, both the secondary and rear frontages of the former Technical Institute and the rears of the Edwardian and Victorian dwelling buildings to its side,

which at present are visible across the gap site. Behind it is Blackley Street, this narrow service road running towards East Union Street parallel with Stretford Road and historically also serving the police station that fronted onto East Union Street. It is an important survivor of Victorian and Edwardian Trafford, maintaining the character that has now been lost on Manchester Street and referencing the similar

side streets that once ran through the blocks of housing to the east of Hadfield Street, all since destroyed. Blackley Street is also the only street within the conservation area and its proposed extensions to retain its historic sett surface to the highway (the only other setts, on Northumberland Road, being to the pavement). There is, therefore, notable historic interest **[Plates 8.8-8.9]**.



**8.6** Modern apartments to Northumberland Road, viewed from Blackley Street



**8.7** Modern housing to Northumberland Road and Blackley Street, current gap site in front



**8.8** Blackley Street



**8.9** Blackley Street, rear of houses to Stretford Road



8.0.16 Also visible across the gap site and Blackley Street are the relatively unchanged rears of the buildings to Stretford Road. Although they too have parking inserted in former gardens behind, as in the conservation area itself, they retain much more sympathetic boundary treatments. The rear elevations of the buildings show far less alteration than those present in the conservation area, maintaining their historic character and demonstrating a far greater degree of intactness and authenticity. There is some visibility of these rears from Chester Road, glimpsed between the modern townhouses to Northumberland Road and East Union Street.

### **Former Technical Institute, 540 Stretford Road (west side)**

8.0.17 The former Technical Institute (positive contributor) – or, more properly, former Old Trafford Free Library and Technical Institute – has substantial streetscape presence and frames and closes views along Northumberland Road

from the north, with direct lines of sight to and from the terraced properties at the east end of Chester Road [**Plate 8.10-8.13**]. Architecturally impressive and with a notable prominence within its corner plot, it shares its form, style, materiality and detailing with the Essence Factory, the two being contemporary (both dating from the very last years of the Victorian period). Its forecourt with hedge and brick boundary wall provides enclosure to the street whilst its tower (although missing its turret) retains its clock.

8.0.18 As a library, vocational educational provider and lecture room the Technical Institute would have played a substantial role in the lives of the people living and working in the Empress Conservation Area right up until its closure in the 1970s, giving it status as a social as well as architectural and spatial landmark. Although it has undergone some change to the exterior – most notably the loss of

its turret – it could, furthermore, potentially meet the criteria for national listing [**Plates 8.6-8.7**].

### **516-534 Stretford Road (west side)**

8.0.19 The Technical Institute and the group of terraced dwelling buildings to its north, continuing around the corner of East Union Street, have substantial special interest as an island of Victorian and Edwardian built form surviving in an otherwise transformed context to Stretford Road and its connecting streets.

8.0.20 To the north of the Institute is a partially intact row of three-storey Edwardian dwellings with central gables, those in the north rebuilt or refronted. All have heavily altered exteriors, with detailing lost to rendering and modern shopfronts inserted, but retain their basic form, their original domestic character still apparent and the symmetry of the group maintained.



8.10 Proposed addition to Stretford and Northumberland Roads



8.11 Former Technical Institute, Northumberland Road frontage



8.12 Former Technical Institute, rear (gap site behind)



8.13 Former Technical Institute, principal frontage to Stretford Road

8.0.21 This is abutted by nos. 522-524 (positive contributor), 518-520 (positive contributor) and 516 (positive contributor) – three groups of high-status red brick houses of two and three storeys, of three designs, those in the north being especially ornate **[Plates 8.14-8.15]**. These continue the materiality of the conservation area, combining red brick fabric with stone dressings and echoing its terraces in their dressings, overhanging eaves, brick chimneys, eaves detailing, and mixture of single-storey bay windows and two-storey canted bays. They are, however, much finer examples of these Victorian terraces, with their terracotta detailing, tall chimneys (some with pots) and elaborate stone architraves, some with granite

colonettes, unrivalled within the residential character areas of the conservation area. They also retain their original red brick boundary walls with stone copings, some with the original stone gatepiers and several with hedges, all of the front gardens still in their original use (although some have been hard landscaped) and with attractive original steps leading up to the entrances.

8.0.22 Retaining their domestic character to a degree not seen within the existing conservation area, and despite some later alterations, these groups have a high architectural and historic interest.



8.14 Stretford Road groups



8.15 Edwardian buildings to corner of Stretford Road and East Union Street

## 18-20 East Union Street

8.0.23 This domestic character continues onto East Union Street, beyond what appears to be an outbuilding of Victorian and Edwardian age, now converted and in vehicle repair use. Two adjoining houses (nos. 18-20, positive contributor) survive out of a short terrace of three and there is again a sudden change in status and detailing relative to the residences adjacent (as also occurs in other parts of the conservation area).



8.15 Edwardian buildings to corner of Stretford Road and East Union Street



8.16 East Union Street

8.0.24 Of a late Victorian or an Edwardian date, these two houses are smaller in scale and much plainer than 516-520 Stretford Road but observe the general underlying consistency of the conservation area in its red brick materiality and loosely classical style. The buildings are of two storeys, in red brick with simpler blue banding and have canted windows with slate-covered roofs, one retaining its iron balustrade above (although not complete). There is again a red brick wall (both retained and replacement), with shrubbery behind that adds visual interest [Plates 8.16-8.17].



8.17 Victorian pair and later buildings adjacent, East Union Street

8.0.25 The domestic character then continues in the row of modern townhouses, which rise taller (at three storeys) but maintain the set back and common building line, the brick materiality and an appropriate depth, although the design of their frontages as driveways is out of character. To their south is the gap site extending from Northumberland Road **[Plate 8.18]**.



**8.18** East Union Street

## Blackley Street

8.0.26 Blackley Street is a rare survivor of the rear service streets that once served the housing in this area, much of which has since been demolished, and, therefore, has historic interest. Its surviving setts to the highway itself are unique within the area (those to Northumberland Road being only on the pavement) and form a positive contributor in their own right.

8.0.27 This aspect along and across Blackley Street also exposes the comparatively unaltered rears of the buildings to Stretford Road. Although they too have parking inserted in former gardens behind, as in the conservation area itself, they retain much more sympathetic boundary treatments. The rear elevations of the buildings also show far less alteration than those present

in the conservation area (which are heavily altered), maintaining much more of their historic character and demonstrating a far greater degree of intactness and authenticity. There is also some visibility of these rears from Chester Road, in a glimpsed view across the current gap site, framed between the modern townhouses of Northumberland Road and East Union Street.



**8.6** Modern apartments to Northumberland Road, viewed from Blackley Street



**8.7** Modern housing to Northumberland Road and Blackley Street, current gap site in front

## Other Proposed Additions

### (4) Proposed Addition to the southwest side of Darwen Street

8.0.28 Whilst clearly legible as modern, and avoiding the pastiche, the block is sympathetic to the character and appearance of the conservation area. Although their roof forms are more modern in style, these buildings otherwise represent a good-quality modern rendering of the conservation area's traditional loosely classical terraced form and its prevailing materials and detailing. They also maintain and reenforce the traditional domestic character of Darwen Street and form part of the setting to the historic buildings facing and to the side. Their inclusion would bring the whole of Darwen Street within the conservation area, regularising its boundary.





**Donald Insall Associates**  
Chartered Architects and Historic Building Consultants

**Conservation Area Appraisal and Management Plan**

**Volume 2**



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# 1 Introduction



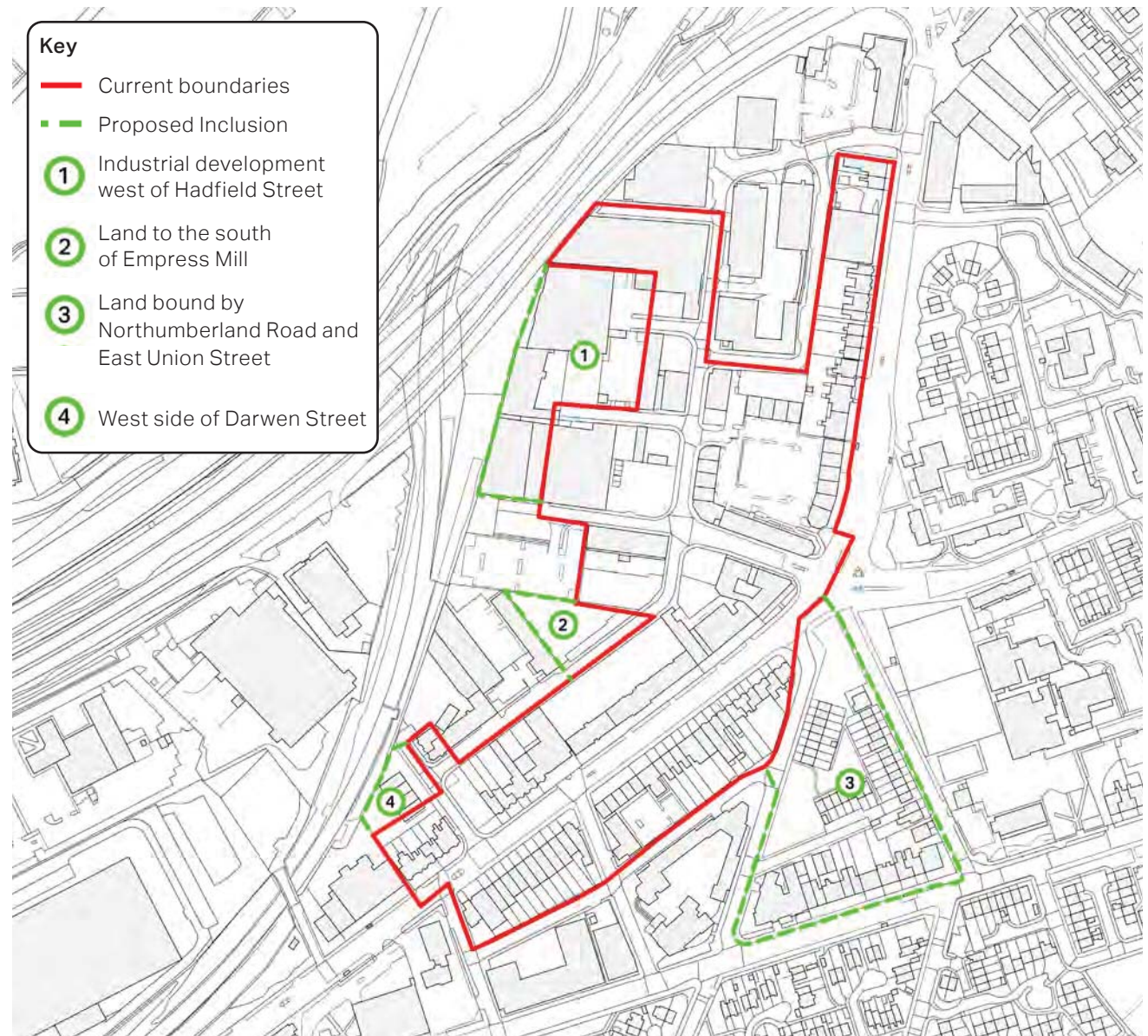
# 1 Introduction

## Introduction

This Appraisal and Management Plan form part of a suite of documents relating to the Empress Conservation Area within the Metropolitan Borough of Trafford and also including a Design Code **[Plate 1.1]**.

First designated in 1995, the conservation area was added to Historic England's Heritage at Risk Register in 2012. A Conservation Area Appraisal and Management Plan were undertaken by Purcell in 2016.

These documents are an update of reports prepared by Purcell in 2016. It has been commissioned following joint funding from Trafford Metropolitan Borough Council, Historic England and Homes England to produce a Conservation Area Regeneration Plan (CARP) for the area, which has been delivered by Donald Insall Associates, LDA Design and Lambert Smith Hampton.



1.1 Current and Proposed Conservation Area Boundaries

## Policy

Conservation areas are 'areas of special architectural or historic interest, the character and appearance of which it is desirable to preserve or enhance' (section 69 of the Planning (Listed Buildings and Conservation Areas) Act 1990) and are recognised for their distinctive townscape, including their buildings, streets and public realm. The National Planning Policy Framework (NPPF) states that 'when considering the designation of conservation areas, local planning authorities should ensure that an area justifies such status because of its special architectural or historic interest and that the concept of conservation is not devalued through the designation of areas that lack special interest'. Furthermore, Section 69 of The Act requires local authorities to, from time to time, review the character and boundaries of its conservation areas.

Once designated, conservation areas are considered as 'designated heritage assets'; any proposals for change or development must assess the effect that they might have on the character and appearance of the area. This is reinforced by local policy, including The Core Strategy (2012) and Revised Unitary Development Plan (2006) which are currently being replaced with the Trafford Local Plan, and the recently adopted Greater Manchester Places For Everyone Joint Development Plan Document (2023).



## Aims of the Management Plan

This Management Plan is underpinned by the Appraisal (Volume 1) and outlines a series of policies that ensure the preservation and enhancement of the character and appearance of the conservation area.

It is essential that any Management Plan supports a modern community and considers both social and economic factors. In view of this, stakeholder engagement and public consultation were undertaken in Spring 2024 which have informed the development of these policies. It is also essential that these policies remain flexible and are reviewed and monitored on a regular basis to ensure that they stay relevant to the changing ambitions, issues and priorities of Trafford Council, the Greater Manchester Combined Authority and the people who live in, work in and visit the area. The policies within this Management Plan align with existing policy but seek to provide more focused and detailed guidance relating to the conservation area and the challenges it faces with the overriding ambition of securing the long-term conservation and revitalisation of its special character.

It is not the remit of this document to address matters associated with the wider management of the community.

A number of key objectives should must be worked towards in order to achieve the aim of preserving and enhancing the conservation area:

- Understand the significance of the conservation area and preserve and enhance its character and appearance.
- Encourage the proactive conservation, repair, reuse and management of the area's historic buildings and their sites and boundary treatments.
- Engage with owners and ensure that they understand the benefits of meeting their responsibilities in respect of repair, reuse and sustainability.
- Address issues of neglect, vacancy and stalled development.
- Stimulate local interest in the protection of the conservation area and its enhancement through high-quality development, for present and future generations.
- Contribute to the economic and social prosperity of Old Trafford in particular and Trafford Borough more broadly.

The policies and recommendations in Section 4 include, therefore, opportunities for both long-term and short-term change.

## 2 Opportunities



## 2 Opportunities

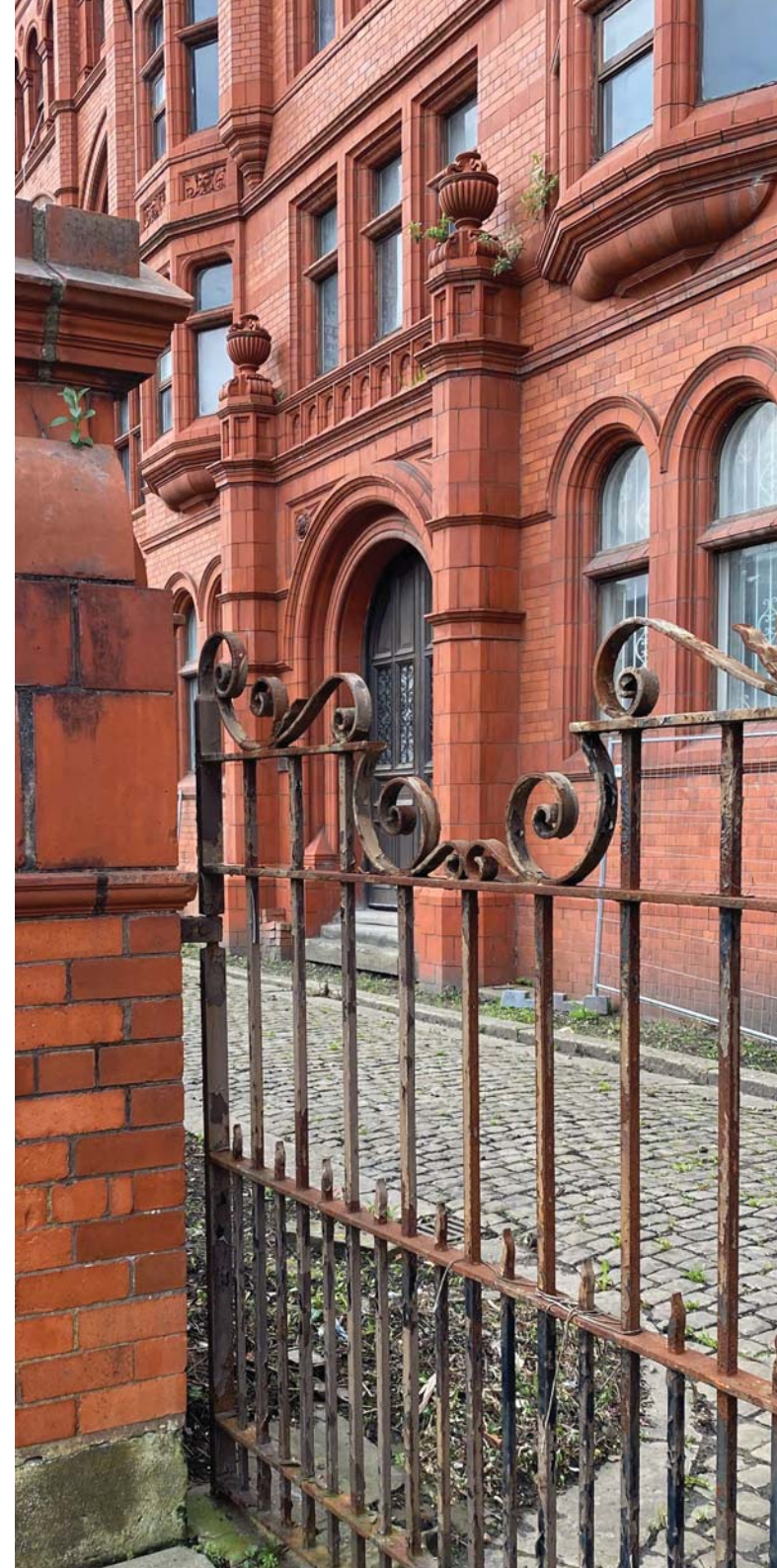
2.i This section highlights the key opportunities for the conservation area, both in terms of its significance and in the risks that it faces (as examined in more detail in the Appraisal, for which see Volume 1).

### Summary of Significance

The Empress Conservation Area holds special historic and architectural interest as an industrial and working-class settlement, which continues to be articulated today in its urban pattern, architecture and uses. The area grew in the industrial booms of the 18<sup>th</sup> and 19<sup>th</sup> centuries – supported by the rail and canal infrastructure which remains today – and was central to the broader evolution of Stretford, Trafford and Manchester more widely. This importance is represented in the proximity of red brick terraced houses, focused on Chester Road, to larger-scale factories and other industrial buildings, many of which continue to operate today. This includes landmarks such as the former Empress Brewery, now an active business centre, as well as the Essence Factory, Veno Building, Former Trafford Press offices and Empress Mill, all clustered around Chester Road and Empress Street. These buildings and uses both contrast and complement

with one another to tell the story of the area's growth and its continued importance to Trafford.

The area's setting has been substantially altered with predominantly modern development in its immediate and longer-range surroundings. However, a sense of connection to Manchester and Trafford is retained (albeit through primarily modern forms), whilst approaching the area from a small pocket of 19<sup>th</sup> century-built form on Stretford Road provides critical dynamic views that give some legibility to the area's history as well as framing its architecture.





## Risks

The area has been on Historic England's Heritage At Risk Register since 2016, indicating significant concerns about its condition and continuing vulnerability. The character and appearance of the conservation area is embedded primarily in its historic built form and the core issues include:

- Small scale incremental change including loss of detailing and features, poor quality interventions and examples of general decline.
- Unsympathetic larger-scale redevelopment.
- Shifting land uses.
- Gaps in the street scene eroding its sense of cohesiveness.
- Decay and stalled (re-)development of the landmark historic buildings at the conservation area's heart.
- Loss of much of its former green quality, prompted by large-scale loss of grand villas and terraces in the immediate context and, more specifically, by the hard landscaping of front and rear gardens/yards for car parking.

## Opportunities

Across the conservation area as a whole, there are opportunities to improve the condition of buildings, preserve and reinstate their historic features, reinstate boundary treatments and reinforce a strong frontage along the pavement edge. Successful examples also show how the re-greening of gardens, yards and car parks can enhance the visual amenity of the conservation area.

**Character Area A** is at the heart of the conservation area and its occupied buildings provide positive examples of accommodating modern uses in historic buildings. The scale, presence and architectural quality of the landmark vacant buildings present a strong opportunity for their adaptive reuse to reinforce local identity, inform placemaking and stimulate interest in the wider revitalisation of the conservation area.

**Character Area B** offers strong opportunities for the preservation of its character and enhancement of its condition through the protection of its historic buildings and the removal or enhancement of detracting elements.

Key to enhancing the contribution made by historic buildings to the special qualities of the conservation area is the removal of detracting alterations and additions, alongside the preservation and reinstatement of historic fabric, features and elements. Whilst security provisions are important there are opportunities to improve the appearance of features such as roller shutters and window bars, or to replace these with less detracting alternatives. Some buildings also offer opportunities for their return to an improved state of external repair and, therefore, presentation.

There are further opportunities to preserve and enhance the character of the conservation area through improvements to the boundary treatments to buildings of all characters, which could also help to screen the very functional parking and loading areas contained within. Improvements to the public realm of the conservation area, such as better quality surfacing, could also enhance its character, as would the management of issues including, but not limited to, flytipped waste, abandoned vehicles and inappropriate signage to railings.

Within **Character Areas C and D** there is the opportunity to reinstate a sense of uniformity between set groups through the preservation and reinstatement of historic features and elements and the removal of detracting alterations and additions – the changing use of these buildings between commercial and residential offers a moment in time to seek these improvements.

Gap sites in all of the character areas present opportunities for well-considered development that makes a positive contribution to the conservation area and provides much needed housing on urban brownfield land. There are also opportunities to improve the highway and public realm, particularly to Chester Road, to enhance the setting of the heritage assets, improve the human experience and reinforce the conservation area's historic character. Road and pavement surfaces and markings, signage/wayfinding, protective pedestrian barriers and street furniture could all be upgraded as part of a more holistic unified scheme to improve the streetscape, with new interpretation offering an opportunity to promote and support connectivity and engagement with the historic environment more widely. The climate emergency is an urgent concern and the conservation area

offers the potential to respond positively through the adaptive reuse and retrofitting of vacant buildings and by ensuring the highest quality, sustainable designs for the development of gap sites.

There are also opportunities for Trafford Council to put in place proactive tools that could help to ensure that the historic environment is protected in the future and that potentially harmful works are avoided. This will include the adoption of an Article 4 Direction to restrict permitted development rights and utilising powers such as Repairs and Urgent Works Notices or compulsory acquisition to prevent further harm to buildings through vacancy and neglect.



### 3 Management Plan



## Management Plan

- 3.i The Management Plan seeks not only to provide policies for Trafford Council in the development management process but also to inform owners about general upkeep and maintenance and to assist both owners and developers alike when preparing proposals for change.
- 3.ii This Management Plan should be read in conjunction with the Appraisal. It sets out actions to maintain and enhance the special character of the area, as defined in the Appraisal. These documents will support the positive and active management of the conservation area through the development management control process, including support for appeals.
- 3.iii The Management Plan is divided into sets of management policies, each underpinned by a series of overarching strategic aims centred on the preservation and enhancement of the Empress Conservation Area. These are based upon the understanding of the conservation

area outlined in the Appraisal (Volume 1) and summarised in Section 2 above.

- 3.iv It concludes with a series of recommendations, and it is the intention that these be brought forward through a collaborative approach between Trafford Council, the local community (including owners, local interest groups and developers) and various funding bodies, as and when opportunities arise.

### 3.1 Methodology

This Management Plan builds on work carried out by Donald Insall Associates for the preparation of the Empress Conservation Area Appraisal. Site surveys were carried out in 2024-25, during which time photographs were taken and features of the conservation noted. Historic research was carried out using maps and photographs and a summary of the special interest of the area was prepared.



## 3.2 Article 4 Directions

The accompanying Conservation Area Appraisal identified the possibility of an Article 4 Direction that would prohibit the loss of original features and the further dilution of the conservation area's positive contributors, comprised of both the characteristic terraced properties and non-residential buildings **[Plate 3.2]**.

This would remove owners' permitted development rights and place further restrictions on the style and material of replacement features (including doors, fanlights/transoms, windows, and boundary walls and railings). Cumulatively, these are character defining features within the conservation area that require protection in order to retain its special architectural and historical interest.

There are two types of Article 4 Directions proposed for the conservation area to restrict development:

1. Directions under Article 4(1) affecting other buildings
  - 1) dwellinghouses in the conservation area not fronting onto highways, waterways and open spaces
  - 2) other properties in the conservation area (e.g. commercial buildings)

*These directions require approval from Secretary of the State*

2. Directions under Article 4(2) affecting dwellinghouses in conservation areas.  
*These directions do not require approval from Secretary of the State*

The Town and Country Planning (General Permitted Development) (England) Order 2015; Schedule 2 Permitted Development.

Permitted Development rights proposed to be removed under the following development categories;

### **PART 1 - Development within the curtilage of a dwellinghouse**

Classes A, B, C, D, F, G & H

### **PART 2 Minor operations –** (All Classes A-D)

### **PART 7 Non-domestic extensions, alterations**

Classes E & F

### **PART 11 Heritage and demolition**

Classes B & C

### **PART 14 Renewable energy**

Classes A & J



**Key**

- Current boundaries
- - - Proposed Inclusion
- Article 4 Direction
- Listed Buildings

3.1 Area covered by the proposed Article 4 Direction

### 3.3 The Conservation Area

The following aims and policies are applicable to all four character areas of the conservation area. Additional aims and policies specific to the individual character areas are provided below.

#### A Vacancy and Demolition

**Aim:** To retain the character of the built form of the conservation area and the visual amenity of its built environment and ensure heritage assets and the appearance of the conservation area are kept in good condition.

**Policy A.1** Building owners must keep their buildings and grounds in good condition to prevent long-term maintenance issues and decay. A clear legal process exists to compel owners to undertake essential repairs to listed buildings. A clear legal process also exists to compel owners to undertake essential repairs to unlisted buildings where their preservation is important to the character and appearance of the conservation area.

**Policy A.2** Owners are required to ensure that development sites, undeveloped sites or under-used or vacant buildings are properly secured and maintained.

**Policy A.3** Short-term temporary uses that would engage the local community with the conservation area should be sought for vacant buildings.

**Policy A.4** Where buildings and sites appear to be suffering from long-term vacancy or under-use Trafford Council should support owners to bring these back into a good state of repair and sustainable use.

**Policy A.5** The proposed demolition of an unlisted building within a conservation area may amount to substantial harm or, alternatively, to less than substantial harm to the conservation area. It should be noted that unlisted buildings may, in addition, be non-designated heritage assets. Any such application should provide a justification proportionate to the building's relative significance and its contribution to the significance of the conservation area as a whole. In reaching decisions Trafford Council will permit such demolition only where the harm to heritage significance is outweighed by public benefits. Public benefits must be clearly established and evidenced.

**Policy A.6** Where demolition is acceptable this must form part of a redevelopment of the site that is sympathetic to, and reinforces, the character and appearance of the conservation area.

## B New Development

**Aim:** To preserve key views and promote the considered design that will preserve and enhance the character and distinctiveness of the conservation area.

**Policy B.1** Detracting buildings, structures and spaces undermine the character and appearance of the conservation area as a result of, for example, unsympathetic additions and alterations, inappropriate character, or a loss of historic fabric with negative impact. Their enhancement or redevelopment is supported subject to the alignment of proposals with the development policies outlined within this plan.

**Policy B.2** Opportunities should be taken to improve the appearance of buildings in order that they complement and enhance the character and appearance of the conservation area.

**Policy B.3** Each proposal for change should be informed by an assessment of the existing building and its wider context in line with the requirements of national guidance. Proposals for change will be assessed on a case-by-case basis.

**Policy B.4** All new development must be of high quality and take inspiration from the established architectural style of the conservation area. It should respect the character and appearance of the conservation area and must be of appropriate scale, density, height, form, massing, layout, plot position, materials, colours, composition and detailed design.

**Policy B.5** The redevelopment of vacant or under-used sites in a manner that stitches the streetscape back together and reinforces historic urban grain will be supported. Original building plots should be retained where their relationship with the pattern of development and other plots makes a positive contribution to the character or appearance of the conservation area.

**Policy B.6** Exemplary contemporary design is not prohibited providing it is well designed, sympathetic to its historic context, of an appropriate scale, uses good quality materials, is well articulated and detailed, and is carefully considered to respond to the character and appearance of the conservation area.

**Policy B.7** Rooflights should not be installed in locations that impact on the aesthetic value of the principal elevation or streetscape and should not be disproportionately large compared to the established fenestration. Conservation rooflights should be installed rather than standard rooflights.

**Policy B.8** Ducting, ventilation, satellite dishes, solar panels and other functional features should not be installed on primary elevations of a property. Additions to rear and ancillary street elevations should be unobtrusive to protect the visual amenity of the conservation area.



**Policy B.9** Roof extensions and alterations that alter the form and massing of the roofs will not be permitted as part of the redevelopment of existing buildings.

**Policy B.10** Any development should respect the skyline in the relevant character area(s) of the conservation area and the key views identified in the Appraisal and any additional views identified during the planning application process.

**Policy B.11** Not all sites will be suitable for tall buildings. The Tall Buildings content of the Trafford Design Code (SPD 7) should be observed for any proposed buildings of six storeys or more.

**Policy B.12** Development proposals must protect existing street patterns, open spaces, walls, railings, materials, street furniture and other elements that are an integral part of the conservation area.

**Policy B.13** Where parking is proposed it must be delivered on-site. Bin stores and cycle stores should be provided internally. Where this is not feasible, they must be sensitively located away from the main street-facing frontage and screened in well-designed stores.

**Policy B.14** Any future development within the setting of the conservation area must preserve (or better reveal) those elements of its setting that make a positive contribution to its overall character. Development in its setting that would negatively intrude into the key views identified in the Appraisal and any further views identified during the planning application process will not normally be permitted.

## C Repair and Maintenance of Historic Buildings

**Aim:** To ensure that buildings are appropriately maintained in order to prevent the negative impact that poor upkeep and maintenance can have on the character and appearance of the conservation area.

**Policy C.1** It is essential that the buildings benefit from maintenance on a regular basis. This should include the following (as appropriate to the building):

- Re-painting of external joinery.
- General minor repairs to windows, including the replacement of glazing where necessary.
- Repointing using appropriate mixes (ordinarily lime mortar for Victorian and Edwardian buildings).
- Removal of debris from gutters, replacement of broken or missing downpipes and unblocking of drainage gullies and grates.

- Removal of redundant fixtures and fittings such as old lights, wires or alarm boxes and the securing of trailing or dangling cables.
- Repairs, including the repair of missing or slipped roof slates (or other coverings) to prevent water ingress.
- Regular checks on the condition of terracotta, which is vulnerable to weather damage and plant roots, removing any vegetation growth and carrying out repairs as necessary.

**Policy C.2** Any repair work should be carried out on a like-for-like basis using the appropriate traditional materials and techniques; existing materials should be used as far as possible, supplemented by salvage materials from a reputable source or, where necessary, new like-for-like replacements.

**Policy C.3** Original architectural detailing and features should not be removed or replaced, unless on a like-for-like basis. Other features or architectural detailing that make a positive contribution to the appearance of a building or structure and the conservation area should be retained, and reinstated where lost.

**Policy C.4** Alterations or developments that would result in a loss of historic features, architectural detailing or character to the principal facades will not be permitted; additions and extensions to these frontages will be generally resisted.

**Policy C.5** Where original or historic timber doors or windows or their architraves (surrounds) survive these should be retained. If refurbishment is required this should be done in a like-for-like manner, replacing only the minimum fabric necessary to make the repair. The materials should, wherever possible, match the original. If thermal upgrading is required, secondary glazing with a frame that follows the glazing bars of the external window should be used.

**Policy C.6** If the replacement of doors or windows is proposed, whether the existing is of timber or uPVC, any new units should be in timber and represent a significant improvement over the existing. Where windows are replaced, they should respect the size and form of the original opening(s) and glazing bars, and should be of an appropriate traditional design. Replacement doors and windows should not detract from the established character of the building.

**Policy C.7** The re-pointing of brickwork or masonry should be carried out using traditional lime mortar.

**Policy C.8** Only damaged or missing roof slates and ridge tiles should be repaired; these should be replaced using Welsh or Westmorland slate or an equivalent natural slate. Where roofs have been re-covered in cement roof tiles or other modern materials the replacement of these using Welsh or Westmorland slate or an equivalent natural slate will be encouraged.

**Policy C.9** Paint or render should not be applied to external walls where this does not already exist or has not existed historically; where render is being replaced this should have a traditional slight textured finish (to maintain character and visual interest).

**Policy C.10** Where rainwater goods require replacement these should be in aluminium or cast iron, using a dark colour palette (dark green, black or dark grey).

**Policy C.11** All sites should have front and rear boundary treatments in brick or stone with appropriate coping, to provide enclosure to the streets. Where boundary treatments are missing, or are removed as part of re-development, these should be reinstated in a historically sympathetic manner. The similar replacement of poor-quality modern boundary treatments will be encouraged.

**Policy C.12** The enhancement of any car parking and refuse storage areas visible from the street will be encouraged.

**Policy C.13** Ancillary historic features and elements located to secondary facades or within the wider site should be retained, including:

- Boundary walls, gate piers, railings and gates
- Historic surfaces
- Historic street name signs

## D Meeting the Challenge of Climate Change

**Aim:** To contribute locally to the transition to a low-carbon future by reducing greenhouse gas emissions, re-using and retaining heritage assets and historic fabric and supporting renewable energy adaptations. This will increase future resilience to extreme weather and contribute to carbon reductions [Plate 3.2].

**Policy D.1** Thermal upgrading should be approached holistically and should seek to retain existing historic fabric such as windows and doors, exploring options such as secondary glazing. External wall cladding will not be supported. Where modern or more unsympathetic windows exist these may be replaced with double or triple glazing where there are overall improvements to the significance of a heritage asset through the material, pattern of glazing and method of opening.

**Policy D.2** Solar panels will only be supported in locations that will not visually impact on the appearance of a building and should be reserved for rear elevations. They should be sensitively designed to minimised impact.

**Policy D.3** Domestic roof-mounted wind turbines are unlikely to be acceptable.

**Policy D.4** Air Source Heat Pumps (ASHP) and Ground Source Heat Pumps (GSHP) should be located to the rear or side elevations (aside from those that are street facing) in locations not visible from the street. Sensitive screening should be provided. Thought should be given to the amenity of neighbours and GSHPs may require archaeological mitigation.

**Policy D.5** EV charging points, if required, should be of suitable appearance, materiality and illumination and should be located to the rear or side elevations where possible. The charging point should be mounted discreetly, away from historic features, and as close to the parking as possible.

**Policy D.6** Historic rainwater goods should only be replaced where they can be replaced and, where justified, with a wider diameter. Where existing downpipes, hoppers or rainwater goods are being replaced, pipes with a wider diameter or a more resilient design should be considered to mitigate the harmful effect of extreme weather and the potential for water ingress associated with this.

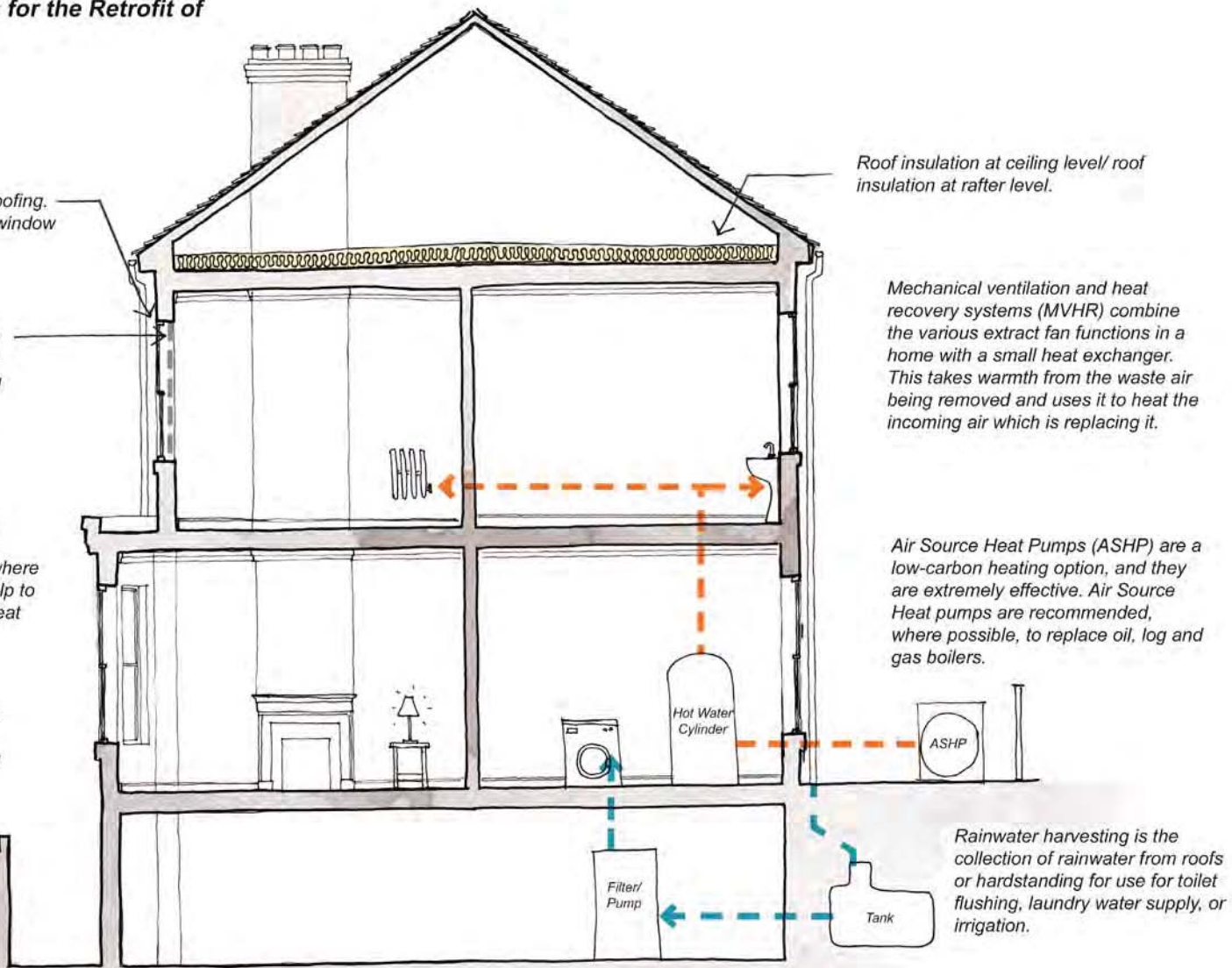
## Potential Opportunities for the Retrofit of Unlisted Buildings

Timber sash window draughtproofing. In addition to ensuring that the window is well maintained.

In addition to enhanced thermal performance secondary glazing can also eliminate draughts and improve acoustic performance.

About 30% of a home's heating energy is lost through windows. Reinstating shutters in homes where they have been removed will help to insulate buildings and reduce heat loss.

Old electric boilers are not particularly efficient; gas boilers should be replaced with renewable energy sources such as heat pumps.



Roof insulation at ceiling level/ roof insulation at rafter level.

Mechanical ventilation and heat recovery systems (MVHR) combine the various extract fan functions in a home with a small heat exchanger. This takes warmth from the waste air being removed and uses it to heat the incoming air which is replacing it.

Air Source Heat Pumps (ASHP) are a low-carbon heating option, and they are extremely effective. Air Source Heat pumps are recommended, where possible, to replace oil, log and gas boilers.

Rainwater harvesting is the collection of rainwater from roofs or hardstanding for use for toilet flushing, laundry water supply, or irrigation.

### 3.2 Typical residential retrofit actions

## E Public Realm and Green Space

**Aim:** To preserve those elements that contribute to the character and green amenity of the conservation area and to enhance others that are detracting.

**Policy E.1** Chester Road is a substantial barrier to movement across the conservation area from east to west. Any future highways alterations should improve the access across the conservation area for cyclists and pedestrians.

**Policy E.2** Side and 'back' streets should be regarded as important corridors taking pedestrians away from the heavy traffic of Chester Road. New development should introduce active frontages only where they will not impact on the appearance of rear or side elevations.

**Policy E.3** Suitable paving materials should be used in all areas, including new developments to enhance the setting of heritage assets; existing historic and sympathetic modern coverings must be preserved and efforts should be taken to rectify inconsistent, uneven and patched pavement, island and crossover surfaces.

**Policy E.4** A coherent and unified scheme of road and street furniture should be introduced when resources allow, including replacement of discordant lighting and management of telegraph poles.

**Policy E.5** The provision of green amenity spaces within new development proposals will be required where contextually appropriate. The reinstatement or introduction of green spaces, planting and trees where appropriate to existing sites and within the public realm would be welcomed and the retention of hedges actively encouraged.

**Policy E.6** Building owners and occupants should not make use of their boundaries for signage. Trafford Council should also be proactive in acting against unauthorised advertising.

**Policy E.7** Owners and occupants should ensure that frontages to public streets are well presented. Where sites detract from the visual amenity of the conservation area, Trafford Council should consider enforcement action.

### 3.4 Character Area A: Essence Factory and Commercial Heart

#### F Landmark Industrial Buildings

**Aim:** To preserve and enhance the character, appearance and landmark status of these important buildings and to prevent the further deterioration of those that are currently vacant or under re-development.

**Policy F.1** The distinctive architectural composition, material and colour palettes of the Essence Factory and the Venno and Trafford Press Buildings should be retained and enhanced, in the first instance through repair and like-for-like replacement of deteriorating features and secondly, where development is proposed, through well-considered design. The architectural design and materials of the existing buildings should not be diluted by new interventions.

**Policy F.2** Any new or replacement development in the grounds of these buildings should enhance their significance and setting. It should be positioned away from the main street-facing frontage and observe the policies for new development above.

**Policy F.3** Trafford Council should take steps to address the stalled development projects at the Venno and Trafford Press Buildings, the Essence Factory and Empress Mill. Any changes to the consented schemes should be of a high quality and respect the character of the conservation area.

## 3.5 Character Area B: Hadfield Street and Industrial Belt

### G Industrial Buildings

**Aim:** To preserve and enhance the character, appearance and setting of the industrial buildings and the contribution that this area makes to the setting of the landmark buildings adjacent.

**Policy G.1** Elements and features of heritage assets and their sites that evidence their industrial origins and character should, wherever possible, be retained and preserved.

**Policy G.2** The addition of metal cladding to historic brickwork is not supported. Walls should remain exposed wherever possible and repaired where required.

**Policy G.3** Opportunities to replace the more ad-hoc or poor-quality modern industrial buildings with new well-designed development that gives a more consistent street frontage, framing views and reinstating a tighter grain should be encouraged. Considered design should ensure that any new development is sympathetic to the historic industrial buildings and does not obscure them from view .



### 3.6 Character Areas C and D: Chester Road Residential North and South

#### H Alterations and Extensions

**Aim:** To ensure that, as a minimum, alterations to buildings preserve the character and appearance of the conservation area, with opportunities taken to bring about an enhancement wherever possible.

**Policy H.1** There is a presumption against development in front gardens, including the addition of new porches and ancillary structures such as sheds or bike stores.

**Policy H.2** Original architectural detailing and features including door surrounds should not be removed or replaced, unless replaced on a like-for-like basis and only where this is made necessary by deterioration or damage; they should not be obscured by paint, render, extensions/ additions or functional features, or used to bed attachments or fixtures.

**Policy H.3** Original or historic timber doors, windows and architraves (surrounds) make an important positive contribution and should be retained:

- If refurbishment/repair is necessary this should be delivered on a like-for-like basis, replacing only the minimum fabric necessary.
- Any replacement doors or windows should be in sympathetic design and materials – typically painted timber in a traditional colour and with traditional door furniture. All windows should have suitably slim frames that do not detract from the proportions of the original opening. Architraves should be in timber.
- Fanlight windows should be retained and they should bear historic property names or numbers.
- Doors and windows should not be relocated or existing openings altered in size or shape unless to reinstate a more appropriate opening where unsympathetic changes have been made previously.
- Juliette balconies should not be added to historic buildings.

**Policy H.4** Where mailboxes or security features are required on front- or side- facing elevations careful consideration should be given to their size, appearance and positioning to minimise their effect on the character of the building. Disproportionately large main or delivery boxes will not be permitted.

**Policy H.5** Solar panels, ventilation/ extracting, ducting, vents, service entries, satellite dishes, meter boxes and other functional features should be as unobtrusive as possible in their design, materials and positioning; these should not be installed on the primary elevations of a property. Additions to rear and ancillary street elevations should be unobtrusive to protect the visual amenity of the conservation area. Any such detracting features to the primary elevations should be removed / repositioned as part of any new works.

**Policy H.6** Elevations facing side and 'back' streets should similarly be maintained in a good state of repair and efforts should be made to minimise the visual impact of bars and railings to doors and windows.

**Policy H.7** When considering extensions to buildings the following apply:

- Extensions must be sympathetic to the appearance of the principal building, historic plan form, yard and prevailing pattern with the terrace group. They must be of an appropriate design, mass, scale, height, colour, material, layout and form to ensure that they are subservient to the original building and that it is not obscured or overdeveloped.
- Extensions to the front or street-facing facades, including porches, will typically be unacceptable. Rear extensions should not present blank elevations to any street.

- Full width extensions, or those than span more than one building will be resisted; extensions should typically terminate at the penultimate storey level.
- Not all existing extensions or alterations, which may be many years old, should be regarded as sympathetic. Inappropriate or detracting examples should not set a precedent for future changes.
- Where historic projections or extensions exist at the rears of buildings there is a presumption in favour of their retention and any further extension to the rear should follow the design guidance above and should not erode the definition of the plan form or be visually dominant; historic openings should be retained where possible.
- Where the historic projections or extensions have been lost any new or replacement extension should be sympathetic to the form and proportions traditional to the conservation area.

- A proportionate area of garden/ yard or other outside amenity space should be retained and this should remain accessible and maintainable.
- Where additional entrances exist or are created in extensions to the rear this should not be at the expense of the principal entrance and street access, which must remain in use.

**Policy H.8** Extensions that alter the form and massing of the terrace roofs will not be permitted. Loft conversions may be permitted only if conservation rooflights are used and new rooflight openings are restricted to the rear elevations of properties. Alterations at roof level should:

- Not disrupt uniform rooflines, rise above the apex (ridge) or result in the loss of historic fabric.
- New dormer windows will not be permitted and the removal of existing modern dormers,

particularly those to the street-facing frontages, should be encouraged.

- Chimneys should not be removed or truncated.
- Roof slates should be Welsh or Westmorland, or an equivalent natural state, avoiding imported slates, which have a polished appearance. Where slates have been replaced with modern materials their reinstatement will be encouraged.

**Policy H.9** Rooflights should not be installed in locations that impact on the aesthetic value of the principal elevation or streetscape and should instead be confined to the rear or side elevations (aside from those that are street facing). Where rooflights are proposed these should follow the hierarchy of openings to the storeys below and should not be disproportionately large compared to the established fenestration. Conservation rooflights should be installed, rather than standard rooflights, and should be set flush with the plane of the roof slope.

**Policy H.10** Opportunities should be taken to reinstate truncated chimneystacks and/or chimneypots.

**Policy H.11** Basement-level conversions that require the hollowing out of lightwells or access will not be permitted. Where these already exist efforts should be made to reduce their visual impact through sensitive landscaping, particularly where they front onto Chester Road.

**Policy H.12** The change of use of buildings should include careful consideration of the physical changes that will be required to meet needs, including the provision of delivery access, parking and external services (such as condensing units).

**Policy H.13** It is recommended that Trafford Council adopt the proposed Article 4(2) direction to introduce additional restrictions to the conservation area. To apply an Article

4(2) Trafford Council must define the properties to be included within the direction, consult with owners and notify them of its adoption. This would withdraw permitted development from the following classes: Schedule 2 Part 1 Classes A, B, C and D and F and Part 2 Class A. As such planning permission would need to be obtained for:

- The enlargement, improvement or other alteration of a dwelling house. This includes all extensions, changes to windows, doors, cladding and dormers.
- The erection or construction of a porch outside any external door of a dwelling house.
- The erection, construction, maintenance, improvement or alteration of a gate, fence, wall or other means of enclosure.

## I Commercial Frontages

**Aim:** To ensure that conversions of domestic buildings to commercial use are appropriate to the character of the area and to seek enhancements where the opportunity exists.

**Policy I.1** The insertion of shopfronts and commercial frontages to historic domestic buildings where they do not already exist will not be permitted. When applications for works to buildings are proposed it will be expected that the detracting elements of commercial frontages will be removed.

**Policy I.2** Signage should be sensitively designed and sited:

- Signage attached to buildings must be subordinate within the façade and should relate to the particular building's character, scale and architectural features.
- Excessive, overly large or poorly situated signage will not be acceptable.
- Projecting and box signage will not be permitted.
- Digital or LED illuminated signage will not be permitted.
- No windows, doors, historic features or detailing should be concealed or removed for the erection or attachment of signage.
- Banner signs and free-standing signage in front gardens and attached to boundary walls will not be permitted.

**Policy I.3** Security provisions should be unobtrusive in design and carefully positioned to maintain the character and appearance of the historic building. Roller shutters will not be permitted to street-facing frontages.

**Policy I.4** Commercial entrances introduced to the rear or side must be modest and sympathetic in appearance and material. They must not compete architecturally with the principal façade and signage.

**Policy I.5** Advertising hoardings/ billboards and banner posters are detrimental to the character of the conservation area and will not be permitted on buildings, in car parks or on vacant sites. Associated changes to provide parking for commercial operations are addressed below.

## J Advertisements

**Aim:** To protect visual amenity and to ensure that the character and appearance of the conservation area are at the very least preserved, with opportunities taken to bring about an enhancement wherever possible.

**Policy J.1** Trafford Council will proactively enforce advertisement control in the conservation area.

**Policy J.2** Advertisements should be of an appropriate scale, character and design so as to not detract from the aesthetic value of the conservation area. All appropriate consents should be obtained.

**Policy J.3** Proposals for the attachment of advertisements to buildings are unlikely to be approved. The proportions of advertisements proposed to be attached to a building should reflect those of the building and excessively large advertisements that dominate a building will not be supported. Side elevations of buildings are unlikely to be appropriate for advertisements due to the likelihood of visual harm. Proposals for excessive amounts of signage will not be supported.

**Policy J.4** The use of freestanding signs such as A-boards on the public highway is unlikely to be permitted as they create a cluttered and unattractive appearance and cause obstructions along the busy streets.

**Policy J.5** Small format banner advertisements are only acceptable for temporary periods, to draw attention to specific events or activities, for example. The siting of advertisement banners upon listed buildings and other positive contributors will, however, be strongly resisted.

**Policy J.6** Large advertising hoardings will not normally be permitted within the conservation area due to the significant visual impact that this would have on its surroundings.

**Policy J.7** New billboard and sheet signage will not be permitted within the conservation area.

**Policy J.8** Overly large and/or excessive amounts of signage will result in a cluttered streetscape and detract from the buildings and wider area and, therefore, will not be permitted.

**Policy J.9** Trafford Council will seek the discontinuance of long-standing advertisements that cause substantial injury to the amenity of the conservation area.

## K Gardens and Boundary Treatments

**Aim:** To prevent the cumulative harm brought about by the erosion of landscaped character to front gardens, the loss of rear service yards, the removal of historic boundary treatments and the erection of inappropriate replacements, as well as to enhance the conservation area through their reinstatement.

**Policy K.1** Where they survive, front gardens should remain as green spaces with trees retained and appropriately managed.

**Policy K.2** Historic features such as steps and surfaces should be retained; their reinstatement where previously removed is encouraged.

**Policy K.3** The conversion of front gardens or corner plots for parking is not acceptable; this includes associated alterations such as hard landscaping and the removal of boundary treatments.

**Policy K.4** The enhancement of rear yards through the replacement of hard surfacing, reinstatement of boundary walls and greening including the introduction of trees, planters, beds or areas of lawn is encouraged and should form part of any development proposals.

**Policy K.5** Any proposed hard surfacing must comprise of complementary materials which relate to their surrounding streetscape context. Surfaces must be simple, permeable or porous and where possible use natural materials and/or retain historic surfacing. The historic form of streets should also be retained by maintaining kerb lines

**Policy K.6** Dustbin stores should be kept in rear service areas and not in front gardens and should be sensitively screened.

**Policy K.7** Where properties are amalgamated or under joint operation their rear yards should not be amalgamated to form shared car parks; a visual distinction between historic plots should be reinstated as part of any development proposals.

**Policy K.8** The different historic boundary walls throughout the conservation area should be retained, and consolidated where necessary using appropriate materials and repair methods.

**Policy K.9** Original gatepiers and gateposts should be retained where possible.

**Policy K.10** Opportunities should be taken to improve any unsympathetic boundary treatments, including to the rear and side boundaries, and any existing vehicular gates.

**Policy K.11** The reinstatement of boundary treatments incorporating low brick and stone walls, hedging and cast-iron railings, and which follow the prevalent townscape character, including coping stones where contextually appropriate, is encouraged and should form part of any development proposals. The introduction of non-traditional materials should be avoided and any reinstatement should preserve the historic variation in boundary treatment visible between the different character areas.

**Policy K.12** Replacement gatepiers and gateposts should match the style already established in the immediate area (i.e. those in place along a specific stretch of road frontage).

**Policy K.13** Street front gates should be for pedestrian use and not widened to permit vehicle access.

**Policy K.14** Street front gates should be of a traditional iron railing style.

**Policy K.15** Hedging of all ages should be retained.

### 3.7 Implementation, Future Management and Recommendations

#### L Implementation and Future Management

**Aim:** This Management Plan sets out guiding policies to maintain and enhance the special character and appearance of the area and to support the positive and active management of the conservation area through the development management control process. Furthermore, they seek to foster a sense of ownership amongst property owners and occupants, to ensure long-term conservation and, where possible, to bring forward enhancements and promote the regeneration of the conservation area as a whole.

**Policy L.1** Following consultation and any necessary subsequent amendments, the suite of interactive documents of which this Appraisal and Management Plan forms a part, and which also includes the Design Code, will be adopted by Trafford Council as Supplementary Planning Documents (SPD), which will be hosted on Trafford Council's website. The Design Code and policies contained within it will be a material consideration in the determination of planning applications and appeals for proposed works within the conservation area.

**Policy L.2** Following its adoption as a SPD, Trafford Council, building owners and developers will all have responsibility for the implementation of the policies within the plan. All future development must be informed by the accompanying Design Code. It is Trafford Council's responsibility to review and determine planning permission for changes within the area, monitor the condition of the conservation area, maintain and enhance the public realm in its ownership, keep building owners informed of the conservation area

designation and potential Article 4(2) Direction, and to review and update this plan on a regular basis.

**Policy L.3** It is the responsibility of building owners, occupiers and developers to ensure that the appropriate consents are in place before undertaking any changes to their property; these include, but are not limited to, planning permission, listed building consent, advertisement consent and works to trees applications.

**Policy L.4** All proposals for change should be informed by an assessment of the building/site and its conservation area context in line with the requirements of national and local planning policy and guidance.

**Policy L.5** Trafford Council will monitor the condition of the conservation area, maintain and enhance the public realm in its ownership and keep building owners informed of the conservation area designation and potential Article 4 Directions.



**Policy L.6** Trafford Council will investigate unauthorised development and may take necessary enforcement action to ensure compliance with national and local policy, including the management policies set out above.

**Policy L.7** A regular review of the Conservation Area Appraisal and Management Plan should be undertaken updating policy as required to ensure that the plan remains a useful, relevant working document. A full review should be undertaken within four years of adoption and every four years thereafter.

## M Recommendations

**Recommendation M.1** Trafford Council should adopt the proposed boundary changes to include 1) the industrial development to the west of Hadfield Street, 2) the land to the rear of Empress Mill, 3) the southwest side of Darwen Street, and 4) the area enclosed between Northumberland Road, Stretford Road and East Union Street (as laid out in Section 8 of the accompanying Appraisal).

**Recommendation M.2** All buildings identified in the Appraisal as Positive Contributors should be identified and treated as non-designated heritage assets. Trafford Council should also consider these for inclusion in the Local Heritage List.

**Recommendation M.3** Trafford Council should work with the local community to establish a Friends of Empress Conservation Area Group to encourage community ownership of the conservation area and its management and enhancement, informed by the Conservation Area Appraisal, Management Plan and Design Code.

**Recommendation M.4** Trafford Council should establish a coherent cross-departmental strategy establishing how it will achieve the concerted improvement to the condition of the conservation area that is necessary for its removal from Historic England's Heritage At Risk Register.

**Recommendation M.5** Trafford Council's Planning and Highways Departments should work collaboratively to establish strategies for the enhancement of the pedestrian experience.

**Recommendation M.6** Trafford Council should bring forward a holistic scheme of upgrades to features in the streetscape including road and pavement surfaces, signage, road markings, protective pedestrian barriers, lighting columns, wayfinding and street furniture, with the opportunity to also add interpretation by signage or other means.

**Recommendation M.7** Trafford Council should adopt its proposed Article 4(1) and 4(2) Directions to

remove certain permitted development rights for buildings identified as positive contributors to the conservation area (see Appendix D).

**Recommendation M.8** The continued vacancy and decline of part of the group of landmark historic buildings at the heart of the conservation area is the greatest single threat to its historic and architectural interest. Trafford Council should consider enforcement action to require the immediate safeguarding of the buildings and to bring forward urgent works to prevent the further loss of heritage fabric.

**Recommendation M.9** Where sites have a serious adverse effect on the amenity of the conservation area, Trafford Council should consider serving notices under Section 215 of the Town and Country Planning Act 1990.



**Donald Insall Associates**  
Chartered Architects and Historic Building Consultants

**Conservation Area Appraisal and Management Plan**

**Volume 3**

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# Conservation Area Appraisal and Management Plan

## Appendix A: Townscape and Character Area Maps *and* List of Heritage Assets

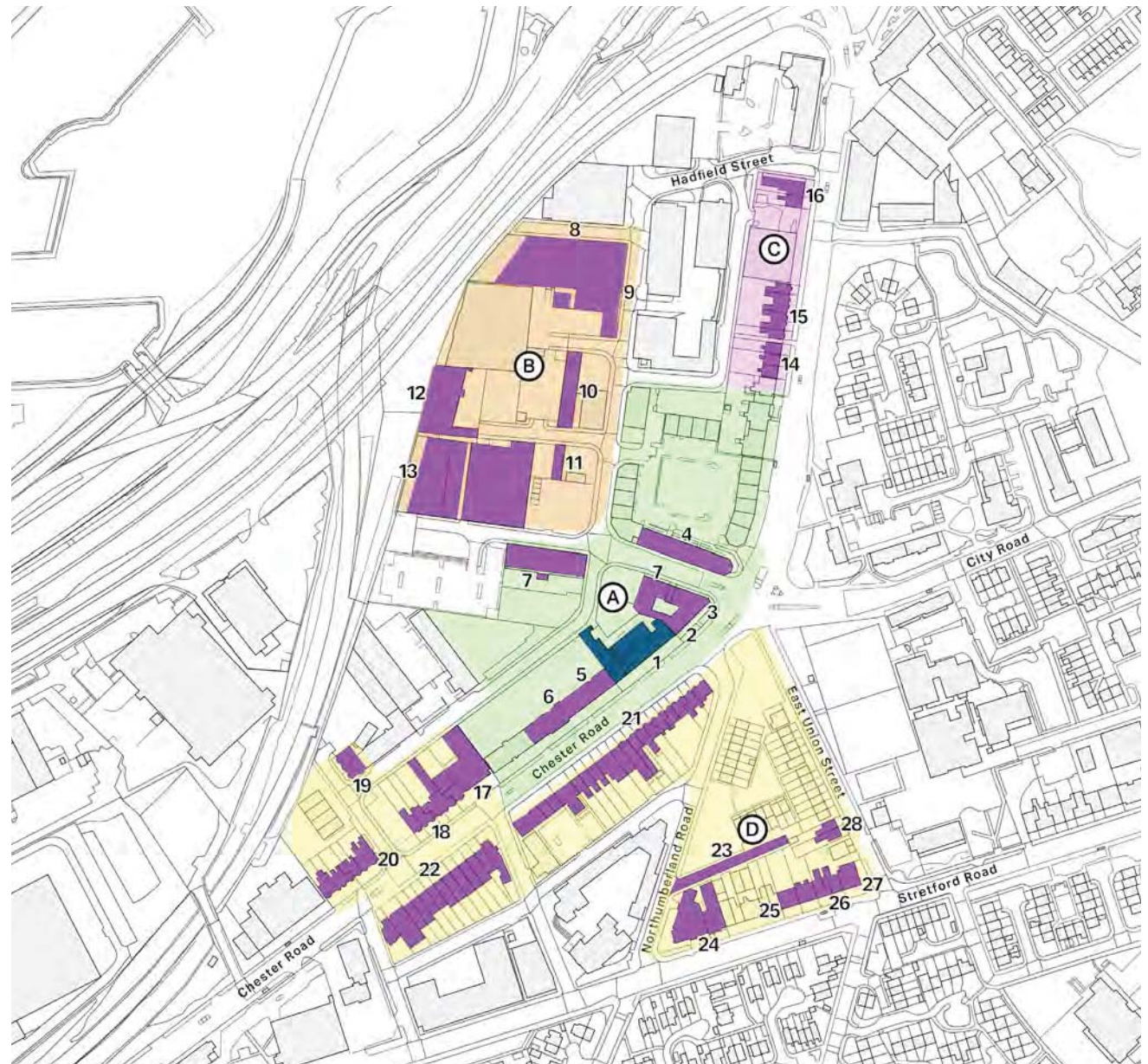


# Appendix A Townscape and Character Area Maps and List of Heritage Assets

## 1.1 Townscape Appraisal

**Key**

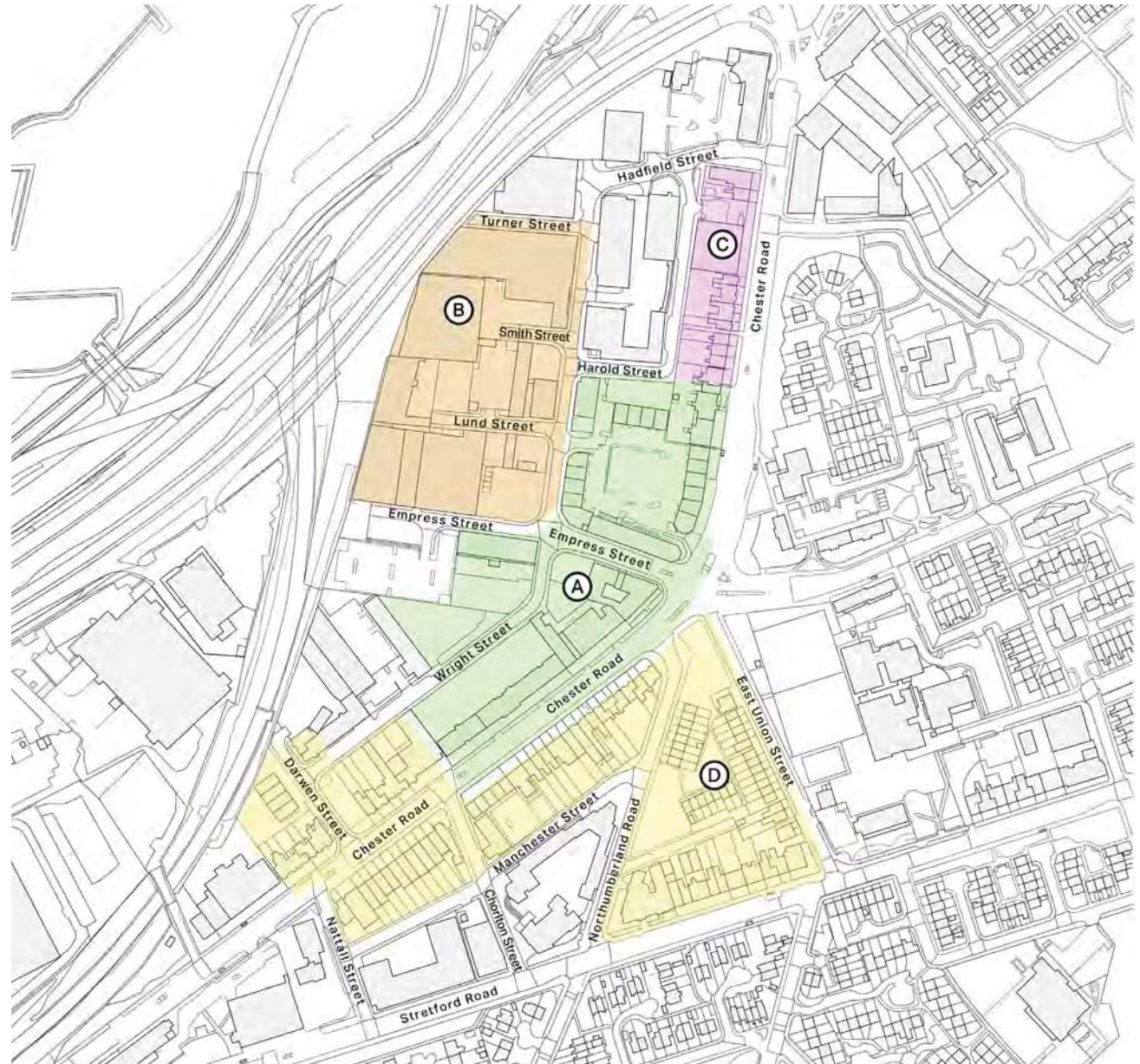
- Positive Contributors
- Listed Buildings
- 1 2 3** Building ID
- A Essence Factory and Commercial Heart
- B Hadfield Street and Industrial Belt
- C Chester Road Residential (North)
- D Chester Road Residential (South)



## 1.2 Character Areas

**Key**

- (A)** Essence Factory and Commercial Heart
- (B)** Hadfield Street and Industrial Belt
- (C)** Chester Road Residential (North)
- (D)** Chester Road Residential (South)





### 1.3 List of Heritage Assets

The conservation area and its proposed extensions contain a number of heritage assets. These include one listed building (which is a designated heritage asset) and a number of Positive Contributors. Positive Contributors are non-designated heritage assets that make a positive contribution to the conservation area and are identified by the local authority as having a degree of heritage significance meriting consideration in planning decisions.

Where a building, structure or site has not been identified in the Appraisal as a positive contributor this does not necessarily mean that the building detracts from the character or appearance of the conservation area. Its heritage significance may require further investigation or there may be the need for enhancement of the building, structure or site.

These elements have been assessed with reference to the criteria established by Historic England in *Advice Note 1: Understanding Place: Conservation Area Designation, Appraisal and Management* (revd. edn. 2019). These criteria are listed in Appendix B.

#### Character Area A:

##### Listed Buildings

Essence Factory, Empress Street (Grade II) (1)

##### Positive Contributors

384 Chester Road (2)  
National Works, Empress Street (3)  
Empress Brewery,  
Chester Road/Empress Street (4)  
(Former) Trafford Press, Chester Road (5)  
Veno Building, Chester Road (6)  
Empress Mil, Empress Street (7)

#### Character Area B:

##### Listed Buildings

N/a

##### Positive Contributors

(Former) School Furniture Works, Hadfield Street/Turner Street (8)  
Hadfield Street Works, 14  
Hadfield Street (9)  
Old Engine Works, 2 Lund Street (10)  
1 Lund Street (11)  
Former Pomona Engine Works, west end  
of Lund Street (12)  
Industrial complex to the north side of  
Empress Street (13)

#### Character Area C:

##### Listed Buildings

N/a

##### Positive Contributors

Cornbrook Terrace,  
328-330 Chester Road (even) (14)  
Birkdale Terrace,  
342-352 Chester Road (even) (15)  
Chester Terrace,  
354-358 Chester Road (even) (16)

## Character Area D:

### Listed Buildings

N/a

### Positive Contributors

Former Almondbury Place (believed),  
448-450 Chester Road (17)

Dover Terrace,

452-460 Chester Road (even) (18)

Darwen Terrace, 4 and 6 Darwen Street (19)

Trafford Mount, 464-470 Chester Road and  
1 Darwen Street (20)

Trafford Place, 429-457

Chester Road (odd) (21)

Winfield Terrace, 473-497 (odd)

Chester Road and 2a Chorlton Street (22)

Blackley Street, road setts (23)

Old Trafford Technical Institute,

540 Stretford Road (24)

522-524 Stretford Road (25)

518-520 Stretford Road (26)

Hullard Hall Terrace, 516 Stretford Road (27)

18-20 East Union Street (28)



# Conservation Area Appraisal and Management Plan

## Appendix B: Criteria for the Identification of Positive Contributors



## Appendix B Criteria for the Identification of Positive Contributors

Guidance on the identification of Positive Contributors is provided by Historic England's *Advice Note 1: Understanding Place: Conservation Area Designation, Appraisal and Management* (revd. edn. 2019).

This states that:

*Most of the buildings in a conservation area will help to shape its character. The extent to which their contribution is considered as positive depends not just on their street elevations but also on their integrity as historic structures and the impact they have in three dimensions, perhaps in an interesting roofscape or skyline. Back elevations can be important, as can side views from alleys and yards.*

*Whilst designated status (ie nationally listed) or previous identification as non-designated heritage assets (such as through local listing) will provide an indication of buildings that are recognised as contributing to the area's architectural and possibly historic interest, it will be important also to identify those unlisted buildings that make an important contribution to the character of the conservation area.*

*A positive response to one or more of the following may indicate that a particular element within a conservation area makes a positive contribution, provided that its historic form and value have not been eroded.*

'Criteria to guide the identification of buildings of heritage significance that contribute positively to the conservation area':

- Is it the work of a particular architect or designer of regional or local note?
  - Does it have landmark quality?
  - Does it reflect a substantial number of other elements in the conservation area in age, style, materials, form or other characteristics?
  - Does it relate to adjacent designated heritage assets in age, materials or in any other historically significant way?
  - Does it contribute positively to the setting of adjacent designated heritage assets?
  - Does it contribute to the quality of recognisable spaces including exteriors or open spaces within a complex of public buildings?
- Is it associated with a designed landscape, eg a significant wall, terracing or a garden building?
  - Does it individually, or as part of a group, illustrate the development of the settlement in which it stands?
  - Does it have significant historic associations with features such as the historic road layout, burgage plots, a town park or a landscape feature?
  - Does it have historic associations with local people or past events?
  - Does it reflect the traditional functional character or former uses in the area?
  - Does its use contribute to the character or appearance of the area?

# Conservation Area Appraisal and Management Plan

## Appendix C: Audit of Heritage Assets



## Appendix C Audit of Heritage Assets

### A.1 Introduction

#### Heritage Assets

A.1.1 As defined in the National Planning Policy Framework (NPPF), a heritage asset is 'a building, monument, site, place, area or landscape identified as having a degree of significance meriting consideration in planning decisions, because of its heritage interest. It includes designated heritage assets and assets identified by the local planning authority (including local listing)' (Annex 2).

A.1.2 Designated heritage assets are defined in the NPPF as 'a World Heritage Site, Scheduled Monument, Listed Building, Protected Wreck Site, Registered Park and Garden, Registered Battlefield or Conservation Area designated under the relevant legislation' (see NPPF, Annex 2).

A.1.3 Listed buildings are designated under the Planning (Listed Buildings and Conservation Areas) Act 1990 and are recognised as having special architectural or historic interest of national importance.

Through their designation they are entered on the National Heritage List for England and accorded protection through the Planning (Listed Buildings and Conservation Areas) Act 1990. Listing ranges from Grade I (the most important buildings, which are accorded the greatest degree of protection) to Grades II\* and II.

A.1.4 The Empress Conservation Area and its proposed extensions contains one designated heritage asset – a Grade II listed building of 'special interest' (the Essence Factory).

A.1.5 The conservation area and its proposed extensions also contain 27 Positive Contributors. The term Positive Contributor identifies a Non-Designated Heritage Asset that makes a positive contribution to the conservation area. This may be a building, monument, site, place, area or landscape. Positive Contributors are classed as heritage assets as they are identified by the local authority as having a degree of significance, meriting consideration in planning decisions, because of their heritage interest.

A.1.6 As per the guidance to the NPPF, Non-Designated Heritage Assets are 'buildings, monuments, sites, places, areas or landscapes identified by plan-making bodies as having a degree of heritage significance meriting consideration in planning decisions but which do not meet the criteria for designated heritage'.

A.1.7 In identifying Positive Contributors, the Appraisal focuses primarily on buildings and structures and does not necessarily take into account the positive contribution made also by landscaping or spaciousness. It also does not take into account any positive contribution made by other historic structures within the curtilage or setting of positive contributors. These characteristics, amongst others must be taken into account during the decision-making process.

A.1.8 These elements have been assessed with reference to the criteria established by Historic England in *Advice Note 1: Understanding Place: Conservation Area Designation, Appraisal and Management* (revd. edn. 2019). These criteria are listed in Appendix B.

A.1.9 Where a building, structure or site has not been identified in the Appraisal as a positive contributor this does not necessarily mean that the building detracts from the character or appearance of the conservation area. Its heritage significance may require further investigation or there may be the need for enhancement of the building, structure or site.

### Scope of the Audit

A.1.10 This high-level audit of heritage assets within the Conservation Area was carried out by Donald Insall Associates in 2024-25 as part of the production of the accompanying Appraisal and Management Plan.

A.1.11 The audit is not intended to be comprehensive or all-encompassing in its content or scope. Omission of any specific building, site, space, landscape or other feature should not be taken to imply that it has no heritage significance or does not make a positive contribution to the character and appearance of the conservation area. Underground and other archaeological remains are not considered.

A.1.12 Wider knowledge, further research, the exposure of fabric, or other circumstances, may lead to a greater or revised understanding of a structure, site, landscape or other feature, of any of its individual elements, or of its contribution to the conservation area. Consequently, this additional information should also be considered, in conjunction with

the Appraisal and Management Plan, during the course of decision making by the local planning authority. Any evidence relating to a heritage asset, some of which may also present itself subsequent to the preparation of this audit, will also be taken into account during the course of a planning or listed building consent application.

A.1.13 The elements, features and characteristics referred to in the identifying descriptions below should be the starting point for discussion with the local planning authority when considering alterations or development. Each site or building and each proposal will be judged on its own merits, paying regard to those elements that make the Empress Conservation Area significant in heritage terms. Ultimately, and as required by the Planning (Listed Buildings and Conservation Areas) Act 1990, special attention shall be paid to the desirability of preserving and enhancing the character or appearance of the conservation area.

A.1.14 The descriptions provided are intended primarily to aid identification. As the audit was carried out from the public right of way only it is not intended to be a comprehensive or exclusive record of all the features of significance [Map 2]. Further evidence relating to a heritage asset, which may also present itself subsequent to the preparation of this audit, will also be taken into account during the course of a planning or listed building consent application.





## A.2 Character Area A: Essence Factory and Commercial Heart

### Essence Factory (1)

386-416 Chester Road

**Status:** Grade II

**Date of construction:** 1896

**Historic use:** Old Trafford Essence Distillery (factory, offices and warehouse)

**Current use:** Vacant

**Condition:** Poor

Purpose-built in 1896, the Essence Factory served Duckworth & Co. for 107 years but fell vacant when the company was acquired in 2003. Built in a bright red brick, the Jacobean Revival style, towers, turrets, oriel windows, terracotta dressings and matching ornate boundary wall all combine to express the high status of this block, behind which were located the yard and ancillary buildings. With a substantial footprint, and rising high above the industrial buildings adjacent, the Essence Factory dominates views both into and through the conservation area at the junction in its north. Uniquely ornate, it serves as the principal landmark within the conservation area and the focal point to the wider group of landmark buildings. Its turreted form and unrivalled height create a distinctive roofline, further enhancing its dominating streetscape presence, whilst the setts to its forecourt are a unique survival within the conservation area. The rear is plainer but is the key townscape feature in views along Hadfield and Wright Streets; its vehicle entrance, metal stairs and loophole loading doors and the gate lodge adjacent all combine to articulate its industrial heritage.

Vacant for two decades, the exterior shows evidence of salt leeching and vegetation growth.



## 384 Chester Road (2)

**Status:** Positive Contributor  
**Date of construction:** c.1900  
**Historic use:** Industrial  
**Current use:** Residential  
**Condition:** Good

This Edwardian red brick industrial building was constructed at some date between 1897 and 1908 and may have formed the high-status office and warehouse block to the National Works (3) factory block behind. A dominant townscape feature because of its position adjacent to the Essence Factory, this long linear block has a strong townscape presence in its corner plot, particularly to Chester Road, where it presents a symmetrical façade with simple but dignified detailing and proportions and a dominant central entrance. Its visual impact is particularly strong from Northumberland Road, reading as part of a group with the Essence Factory (1) and the Empress Brewery (4), adding to the varied roofline and providing a bridge between the tall, large scale and elaborate building to its south and the smaller scale but ornamented former brewery in the north (which has certain shared decorative features). Whilst subservient to the Essence Factory in both height and detailing, there is a shared coherence in its gabled roof and arched windows and in the common frontage and boundary lines, here set behind the original gate piers and a sympathetic wall and railings.

With the National Works (3), the building was converted to apartments in the 1990s and is in good external condition.



### National Works (3)

Empress Street, rear continuation of 384 Chester Road

**Status:** Positive Contributor  
**Date of construction:** c.1900  
**Historic use:** Industrial  
**Current use:** Residential  
**Condition:** Good

Although plainer in style, this red brick block has a long linear footprint, symmetrical façade, central entrance, arched windows and simple finishes that unite it visually with 348 Chester Road (3) as a single ensemble. Possibly serving historically as the rear factory block to this building, it has a strong presence to Empress Street, enclosing the road to the south, counterposing the former Empress Brewery (4) opposite and framing views to the east and west, consolidating the townscape role of the group of landmark buildings. 'NATIONAL WORKS', in white brick, announces its industrial origins, and this building articulates and encapsulates the character of the late Victorian and Edwardian red brick warehouses and factories that once dominated the Hadfield Street/Wright Street area. Its presence to the rear is weaker, set back behind what is now the car park and with an austere frontage of rough brown brickwork (formerly concealed by extensions), yet forms a group with the similar rear of the Essence Factory (1) adjacent, the two preserving the more utilitarian back-of-house architecture that was once commonplace in the industrial character areas. The modern roof extension also becomes visible here, its visual impact on Empress Street being much more limited.

Now in apartment use (converted in the 1990s), the building is in good external condition.



## Empress Brewery (4)

380 Chester Road

**Status:** Positive Contributor

**Date of construction:** 1889

**Historic use:** Brewery

**Current use:** Business centre

**Condition:** Good

Built in 1889, the brewery operated until 1955 and was converted to business centre use in 1992, with sympathetic extensions added to the side and rear. The historic building occupies a prominent corner position within the group of landmark buildings in the north of the conservation area and forms part of a cluster of red brick industrial buildings with a consistent materiality and coherent industrial style. Separated from the central group by Empress Street (nos. 1-3 and 6-7), its presence to Chester Road is derived primarily from the tall moulded gable end (with painted name tablet) of its middle section and the higher pitched and flat roofs and industrial chimney behind. Forming almost a pair with the red brick industrial buildings opposite, it encloses Empress Street and frames views towards the industrial area in the west, whilst its terracotta and brick detailing add visual interest. The rear also remains fully exposed within what is now the car park, allowing its scale, varied roofline and industrial features to be appreciated, offset by the much smaller scale of the subservient modern buildings adjoining.

The building remains in use and is in good external condition, forming one of Manchester's best surviving examples of a late Victorian ornamental brewery.



**(Former) Trafford Press (5)**  
**418-446 (part of) Chester Road**

**Status:** Positive Contributor

**Date of construction:** Early-20<sup>th</sup> century

**Historic use:** Industrial

**Current use:** Residential (incomplete) but vacant

**Condition:** Extreme risk

Constructed at some date between the OS maps of 1908 and 1922, this Flemish-style building was built as a factory for the Veno Drug Company, replacing pre-existing 'Veno Buildings' on the same site, and originally occupied a much deeper footprint. It was later supplemented or replaced by the new Veno Building (6), the two operating under the Co-Operative Press (later Trafford Press) from the 1960s until 2004, with both truncated to the rear during their subsequent conversion, halving the depth and probably leaving little more than a façade. Mainly intact, this highly distinctive red brick façade with its central Flemish gable, orange/tan terracotta dressings, moulded architraves and pilasters is unique in the conservation area, the visual interest further increased by its contrast with both the simple classical details of the Veno Building adjacent and the highly decorative Essence Factory (1) to each side. The contrast in scale, height, material and style with the Essence Factory has a notable visual impact, the sudden fall in height at the Trafford Press building allowing full appreciation of the scale of this larger building.

The 21<sup>st</sup> century additions to the rear are now mainly concealed from view by heavy vegetation growth but the whole group is believed to be in very poor condition, with the roof to the historic building lost to fire damage. See also Veno Building (6).



## Veno Building (6)

418-446 (part of) Chester Road

**Status:** Positive Contributor

**Date of construction:** Early 20<sup>th</sup> century

**Historic use:** Industrial

**Current use:** Residential (incomplete) but vacant

**Condition:** Extreme risk

This building forms a group with the Trafford Press building (5) adjacent, having been built to replace or supplement it at some date in the 1920s or 30s. Both are important in articulating the continued industrial development and character of the area well into the mid-20<sup>th</sup> century, whilst deriving communal and historic value from their association with the Veno Drug Company, founded in Manchester in 1898 and famous for Veno's Cough Syrup and Germolene. Faced in a white render (a post-war addition), the art deco influenced classical frontage is unique within the conservation area in its style, material and colour palette, contributing to the pleasing visual contrast that continues across the single townscape group formed by the Veno, Trafford Press (5) and Essence Factory (1) buildings. There is also strong visual interest to the symmetrical frontage, particularly in its dominant central entrance pavilion, giant columns and classical proportions, which give a horizontal emphasis complementing the vertical lines of the Essence Factory adjacent.

The building and its 21<sup>st</sup> century rear extensions are in very poor condition and the rear site is heavily overgrown.



## Empress Mill (7)

### 7-11 Empress Street and 2 Wright Street

**Status:** Positive Contributor

**Date of construction:** Early-20<sup>th</sup> century

**Historic use:** Industrial

**Current use:** Residential (incomplete) but vacant

**Condition:** Extreme risk

Built at some date between 1903 and 1909, this tall red-brick factory building has a strong townscape presence at the junction of Empress, Hadfield and Wright Streets. Situated facing the rears of the Essence Factory (1) and National Works (3), its distinctively early-20<sup>th</sup> century geometrical architecture reiterates the continued industrial development and character of the area. It is also one of the few known surviving examples – and the only still intact example – of the larger red brick factories and works that once dominated the east side of Hadfield/Wright Street. As such, it has high townscape importance, further enhanced by its contribution in enclosing Wright Street to convey a sense of the traditional closed-in character of these roads, providing a sympathetic setting to the rear of the Grade II listed Essence Factory and articulating the historic age of the side streets running off to the west (where much of the visible architecture is now modern). Its functional architecture, strong vertical emphasis and tall lower floors are typical of Manchester's industrial architecture and suggest the appearance of Character Area B prior to its modern redevelopment, giving a degree of streetscape importance, which is further increased by its role as a dominant feature in views along Hadfield/Wright Street.

The west wall has been rebuilt, poor-quality modern additions have been added to the roof and the south of the building and the building has subsequently suffered fire damage. It is believed to be in poor condition internally.

The rest of its complex has been cleared. The grounds are heavily overgrown but retain a pair of gate piers surviving from this or neighbouring works (since lost).



## A.3 Character Area B: Hadfield Street and Industrial Belt

### (Former) School Furniture Works (8)

#### Part of 14 Hadfield Street (Hadfield Street/Turner Street)

**Status:** Positive Contributor

**Date of construction:** Late-19<sup>th</sup> century (with mid-20<sup>th</sup> century additions)

**Historic use:** Industrial

**Current use:** Industrial

**Condition:** Good

This two-storey workshop/warehouse of the final quarter of the 19<sup>th</sup> century is the oldest surviving building in Character Area B and the second oldest industrial building in the conservation area (second only to the Empress Brewery (4)), although its sympathetic extension is mid-20<sup>th</sup> century. To its rear there was historically a yard, enclosed by a courtyard range along its west, the whole group operated as a single unit, as a timber yard in the 1890s and subsequently as the School Furniture Works, the whole accessed from Turner Street. The replacement of the yard and courtyard range by the modern extension now obscures this historic relationship.

This building has been brought into joint operation with the former Hadfield Street Works (9) adjoining to its immediate south, the whole site today operating as the headquarters and despatch centre for J. Parker. Consequently, its former side return to Hadfield Street has become its principal frontage, although loading access continues to be situated to the north. As a result, it is now read as part of a group and townscape block with the Hadfield Street Works building (9), fronting onto Hadfield Street. Although historically separate, the two buildings combine to evoke the historic appearance of the smaller works and warehouses that were

the first of the industrial buildings to emerge in the Hadfield Street area (most since lost). Its mixture of red/brown and white brickwork with blue engineering brick banding – unique to the conservation area as it survives today – and the simple detailing add visual interest and variety, whilst its positioning hard against the pavement maintains a more robust street scene. It is, however, in its distinctive gable ends to Turner Street (also now unique) that it has the greatest townscape role; this creates a gabled roof profile behind, complementary to the sawtooth roofs surviving at the former Empress Machine Tool Works and extension (13) and elsewhere in remnant form.

The building remains in light industrial use today, with some modern alterations (including extractor units, security bars, rooflights and inserted and altered openings).

The late 20<sup>th</sup> century addition in the west extends across its former yard and part of the enclosing courtyard but is not part of the Positive Contributor Building.





## Hadfield Street Works (9)

### 14 Hadfield Street (Hadfield Street part only)

**Status:** Positive Contributor

**Date of construction:** 1890s

**Historic use:** Industrial

**Current use:** Residential

**Condition:** Good

The brickwork and the stone embellishments to the street-facing frontages of this late Victorian works are relatively intact, the changes limited to the infilling and shortening of a small number of windows in the north of the building and the closing of a doorway along its south. The building forms a distinctive townscape group with the former School Furniture Works (8) abutting, the two also sharing elements of detail although historically separate and distinct. Its shallow depth and low three-storey height (including basement) again preserve the historic character of the smaller works and warehouses that were the first industrial buildings to emerge in the Hadfield Street area (most since lost). Its mixture of red/brown and white brickwork with a stone banding and simple detailing also adds visual interest and variety. Partly built hard against the pavement, the building helps to maintain a more robust streetscape, its staggered roofline and tapered building line establishing a visual relationship with the Former School Furniture Works (8). Its lower section, abutting the School Furniture Works, is set back behind a lightwell edged with historic railings.

Whilst the masonry to the street-facing frontages shows little change, these faces are, however, heavily altered, with modern roller shutters across the ground-floor windows, boarding and security mesh applied to the outside of multiple windows, and roller

shutters to the doorways, their fanlights boarded over. The roller shutters have large boxes and are particularly dominant, as are the numerous wall-mounted extraction units. There is a general cluttering of mesh, cabling, lights, cameras and signage.

The building remains in light industrial use today in conjunction with the former School Furniture Works (8). Its rear is more heavily altered and the side range to Smith Street is a modern replacement, the whole site today operating as the headquarters and despatch centre for J. Parker.



## Old Engine Works (10)

### 2 Lund Street

**Status:** Positive Contributor (historic building only)

**Date of construction:** 1930s

**Historic use:** Industrial

**Current use:** Commercial

**Condition:** Good

This simple linear range of red/brown brick with a slate pitched roof echoes the form of 1 Lund Street (11) opposite but is built on a larger footprint. Its pared-back interwar style is reflected in its lack of detailing beyond simple brick lintels and segmented headers, some of the ground-floor openings seemingly altered and the east elevation entirely obscured at ground floor by a modern flat-roofed addition. With the resultant limited visibility from the east, its main contribution is in acting as a pair to 1 Lund Street, drawing the eye along the road towards the former Pomona Works (12) to the west end of Lund Street and the Empress Street complex (13) to its south, which together comprise the main townscape feature in views along Lund Street.

The sawtooth roofs of the Empress Street complex are visible behind from Hadfield Street, rising above this lower building. The building is in good external condition, with replacement windows and accretions including security bars, extractor units, cameras and lights. Its southern wall is modern, presumably built after the demolition of the abutting and surrounding structures, which has left this building isolated.



## 1 Lund Street (11)

**Status:** Positive Contributor  
**Date of construction:** c.1900  
**Historic use:** Industrial  
**Current use:** Commercial  
**Condition:** Good

Probably built as a chemical works, this small red/brown brick range with pitched slate roof runs into the interior of the block and was historically concealed to all sides but its north. Both of its narrow gable ends have been rebuilt and many of its window openings have been altered or blocked, particularly to the west, whilst the modern domestic-style windows obscure its age and historic industrial character (its very late Victorian/very early Edwardian origins now really articulated only by the chimney and segmental arched window heads). The building makes some contribution to the historic streetscape through its brick materiality, linear form and low two-storey height, forming a pair with the Old Engine Works building opposite (10) framing Lund Street. However, its main contribution is its role in the red brick townscape block with the former Empress Machine Tool Works and extension (13), its low height also allowing views across the building towards the higher sawtooth roof behind.

The building is believed to be in commercial (office) use and appears in generally good condition, with heavily altered windows and openings.



## Former Pomona Engine Works (12)

### 6-8 Lund Street

**Status:** Positive Contributor

**Date of construction:** Second half of the 19<sup>th</sup> century

**Historic use:** Industrial

**Current use:** Industrial

**Condition:** Unknown

This group of buildings encloses the west and southern sides of their yard, the east side being enclosed by a tall boundary wall of historic brick containing a set of modern vehicle gates. The yard, principal building, chimney and former boiler house all existed by 1894, as Pomona Engine Works, and appear to retain their original external footprint today, only their water tank having been lost. The building lining the south of the yard dates from between 1894 and 1915 (either replacing or incorporating a smaller building present by 1894). Adjoining buildings present by the early and mid 20<sup>th</sup> century, filling much of the yard, have since been removed, restoring the external footprint of the buildings as they existed at and soon after construction.

As visible from the street, the principal building is a two-storey linear structure of brick, the brickwork in recessed bays between stanchions and with simple decorative corbelling at eaves level, the windows at first floor with stone cills and segmented brick arches. The exterior shows a number of alterations including modern roller shutters, uPVC windows to the first floor and corrugated metal sheeting to the pitched roof, with several windows variously bricked up, boarded over or infilled from the interior. A single-storey bay in the south, also of brick, has a flat roof, a doorway and a large window with a stone lintel and metal bars.

In the north of the site, the boiler house and part of the chimney are also painted, the chimney rising high above the principal building and the boiler house, without openings, being much lower. A small structure built against the chimney appears to be later.

The building in the south of the site is of brick, mainly unpainted, and two storeys tall, with a single-pitched roof. There are a number of openings to the north and east frontages, those to the east mainly infilled, with a roller shutter added; the north wall has uPVC windows, whilst remnant brickwork survives from since-demolished buildings.

Modern lights and cameras have been added across the complex, the door and window openings are heavily altered, and the modern uPVC windows are unsympathetic. However, and whilst there are extensive alterations to the buildings' exteriors, the complex has an intact external footprint and is one of the few industrial sites in the conservation area, and perhaps the only site in Character Area B, to retain its historic brick boundary wall. It retains a high degree of intactness in the survival of its original buildings (with their original external footprints) – although these show evidence of alteration – and its yard (now undeveloped) and boundary wall.

The site is believed to have a continuous history of industrial use and has notable historic significance as one of the numerous late Victorian mid-size works once found across this character area, and as the most intact of the very small number surviving today. Its early use as an engine manufactory and its later occupancy by Venio Laboratories give further historic significance, highlighting the initial engineering-based

industrial activities that initially dominated this area and their later diversification across the 20<sup>th</sup> century. It is currently occupied by Empress Autos.



## Industrial complex to the north side of Empress Street (13)

**Status:** Positive Contributor

**Date of construction:** Second half of the 19<sup>th</sup> century and early 20<sup>th</sup> century, with later remodelling

**Historic use:** Industrial

**Current use:** Industrial

**Condition:** Good

This group spans the space between Empress Street and Lund Street at the western end of the conservation area, bordered in its west by the Metrolink line (formerly the railway). Whilst the southern frontage to Empress Street has been variously reclad, refronted and rebuilt, in various phases, the historic character and appearance is retained in the group's east and north elevations. Here, the two easternmost buildings retain their historic walls of brown brick (the southern wall with cladding to part of the height) and their historic roof forms, these having a sawtooth profile with "northern lights" in their east (although with a modern metal covering). The westernmost building is less visible but retains one historic pitched slate roof with continuous rooflights, the exposed northern wall below this being of a historic brown brick, raising the possibility that further historic fabric survives below the adjacent modern roof coverings.

The middle building (four bays) was erected in the early 1890s and by 1915 a number of additional structures had been added to either side, with some or all of the complex in use as an engineering works. By the 1930s the original building had been further extended to the west and divided into the Rlico Works (biscuits, west) and the Empress Works (machine tools, east). By the 1950s, Rlico Works had been divided into multiple units and Empress Works (also subdivided) had expanded into part of the pre-1915 additions to

the east (today occupied by VExtra TV World), which had been extended or rebuilt in its south (and was later remodelled again). Those eastern additions not incorporated into Empress Works have since been demolished, leaving only 2 Lund Street (10) but the surviving block retains its pre-1950s footprint almost intact, bar the loss of various 20<sup>th</sup> century lean-to structures to the rear (Lund Street).

As one of the first industrial buildings to be built following the Empress Brewery, the original early 1890s (middle) structure has particular historic and architectural interest. This interest also extends to the group as a whole, however, for its early date (1890s-1910s), representing the first industrialisation of the area and serving as now-rare examples of the area's numerous small- and mid-size manufactories, many initially used for engineering. Further interest comes from the almost entirely intact footprint and for the two "northern light" sawtooth roofs – the only examples to survive in the conservation area and rare at a regional level for their application to buildings of two storeys.

The cohesive alignment, its densely developed plot, and the boundaries provided by Empress and Lund Streets give the group a coherency reinforced by the shared brown brick materiality and matching metal-framed windows that unite the two exposed historic structures. Variations in the form, external footprints and roof profiles of these two latter structures articulate the staged development and multiple phases of remodelling that created the group as it exists today, reflecting not only the initial phase of rapid industrialisation that placed great development pressure on the area but also the later decline of heavy engineering. Post-1950s remodelling and modern refronting and recladding continue this history into the present-day.

The group has undergone substantial change to its Empress Street, removing its historic principal frontages, with the modern frontages reducing its legibility. At Lund Street, however, the original building has simple corbelling, a deep eaves cornice with corbels, various redundant attachments, and mainly altered ground-floor openings, some with metal joist lintels, whilst the east building has recessed bays, windows to both floors (some altered and all boarded over), concrete cills and lintels (some repaired/replaced) and an inserted doorway. To Hadfield Street, the east building is clearly of two forms and phases, that in the north double-storey, with first-floor windows and a sawtooth roof, and that in the south with a wall of brick and modern cladding above, set beneath a two-phase roof (also with modern cladding). Buttress-like remnants along its edge survive from associated buildings, since demolished.



Demolition to the building's east has increased its prominence and better exposed its "northern lights" roofs, whilst to Lund Street the functional rear preserves a unique remnant of "side street" industry, allows views of the unique roof forms above and create a townscape group with the buildings of the former Pomona Engine Works (12) adjacent, the two being contemporary. Although altered, the building remains distinctive, its materiality, mass, scale and siting giving townscape presence.



The group is now occupied by SBT Engineering Services, The Churchill Machine Tool Company and VExtra TV World.

## A.4 Character Area C: Chester Road Residential (North)

### Cornbrook Terrace (14)

#### 328-330 Chester Road (even)

**Status:** Positive Contributor Building

**Date of construction:** c.1830s

**Historic use:** Residential

**Current use:** Unknown, presumed vacant

**Condition:** Poor

These two brick townhouses form one of the oldest surviving buildings in the conservation area and articulate its first phase of development as a middle-class suburb. Their classical proportions, Georgian simple detailing and symmetry, half-basement and recessed entrance bays, and features including doorsteps, narrow chimneys, stone string courses and internal shutters, all convey a quiet and refined dignity that contrasts pleasantly with the more exuberant architecture of some of the later Victorian terraces further south. Their more generous width, articulating their higher status, reflects their earlier age, these buildings having an important role as evidence of the historically residential character of the land to the east of Hadfield Street, now dominated by industry.

The terrace of four properties was truncated in the later 20<sup>th</sup> century, the crow stepped gable in modern brick being out of keeping (the original roof hipped), whilst the building shows evidence of neglect and the seemingly replaced architraves and windows appear out of keeping. The modern boundary wall to the front, hard landscaped gardens and replacement doors, windows and architraves are all of a low quality and a post-war workshop runs along its north and wraps along the rear, a billboard above. To

the rear, the elevation is plain and the ground floor concealed by ad-hoc structures and heavy vegetation, which have subsumed the former gardens.





## Birkdale Terrace (15)

342-352 Chester Road (even)

**Status:** Positive Contributor Building

**Date of construction:** Late-19<sup>th</sup> century

**Historic use:** Residential

**Current use:** Commercial and some residential

**Condition:** Good

Externally, this is the architecturally highest quality, best preserved and most intact terrace in the conservation area, in a good state of external repair. Dating from the second half of the 19<sup>th</sup> century, its more ornate Palladian-style red brick frontage creates an interesting visual contrast with the simpler, Georgian-style, townhouses to either side, articulating its later date. The gable fronts and recessed entry bays, polychromatic brick bands, segmented headers, slate porches, and pierced barge boards at porch and eaves level are also visually and architecturally superior to the simpler detailing of the much less decorative terraces to the south, indicating superior residences for the lower middle classes, whilst the paired front and rear chimneys and some sash windows are retained. Gardens and the original walls and gate piers, with a mixture of original and sympathetic railings, give a strong townscape presence whilst emphasizing the residential character. To the rear, the building is one of the few to retain its full set of historic outriggers (closet wings) intact and legible, without modern additions, although security bars, paint and modern windows reduce their definition and uniformity.

The building is occupied for residential and commercial uses and is in a very good state of repair, maintaining its original character despite the presence of concrete roof tiles, uPVC casement

windows and modern rooflights. Only one rear garden remains (behind a rebuilt wall), the boundary wall to the others partially dismantled, exposing to view the amalgamated rear space now hard landscaped for parking.



## Chester Terrace (16)

### 354-358 Chester Road (even)

**Status:** Positive Contributor Building

**Date of construction:** Mid-19<sup>th</sup> century

**Historic use:** Residential

**Current use:** Residential

**Condition:** Good and poor

Present by the 1870s, this truncated brick terrace references the older Cornbrook Terrace (14) in its Georgian style and detailing, the two bookending the more decorative Birkdale Terrace (15) in between. The classical proportions (although narrower), recessed entrance bays, string courses and other stone dressings, entrance steps and half-basement are here accompanied by two sets of timber panelled doors and classical architraves, whilst a dentilled eaves cornice reflects its later age, articulating the continued middle-class residential development of the area as industry emerged. With the southernmost property demolished in the mid-20<sup>th</sup> century, the terrace has lost its original symmetry, giving an unbalanced appearance, although the retained gardens, original gate piers (one pair missing its caps) and sympathetic replacement wall retain its presence to the street, whilst the plain façade and sympathetic sash windows create the impression of a building much older than the roughly contemporary Birkdale Terrace adjacent.

No. 354 is in a better state of repair but has undergone a number of unsympathetic alterations, most notably the replacement of the stone architrave to the front door. The timber architrave has also been lost and the fanlight replaced in a contemporary design, whilst a non-original window opening has been added at basement level. The properties to its south are in a poorer state of repair and

show signs of neglect. Part of the roof has been resurfaced in concrete tiles and all chimneys have been lost, the rebuilt modern example in dark brick out of character. The rear is less intact, only no. 354 retaining its outrigger (closet wing) unaltered and sympathetic sash windows, the other properties seemingly refaced and now dominated by oversized additions, with modern casement windows throughout, their gardens combined for parking behind a tall modern wall.



## A.5 Character Area D: Chester Road Residential (South)

### Former Almondbury Place (believed) (17)

#### 448-450 Chester Road

**Status:** Positive Contributor

**Date of construction:** Pre-1849 (potentially)

**Historic use:** Residential

**Current use:** Commercial

**Condition:** Good and poor

To Chester Road, the principal frontage shows many similarities to Chester Terrace (16) adjacent, being a two-storey building of five bays, fronted in red brick and with brown brick to the sides, and matching its proportions, scale and height. These similarities continue at first floor, where there is a stone band, five windows with stone cills, and a slate pitched roof above.

The remodelled ground floor is much more ostentatiously classical and has no domestic character, its two outer bays treated as three-light bay windows divided and framed by stone columns, the stone cornice above continuing across the intervening bays, where the windows are similarly framed by wide stone columns, the whole surmounting a heavy stone plinth. A modern infill building of metal and glazing runs along the south elevation, has a prominent triangular portico, and is incongruous in character and materials. The north wall is rendered and painted, having previously abutted a set of terraced housing (now lost).

The uPVC windows to both floors and uPVC panelling to the ground floor are unsympathetic, as is the signage to the cornice. There are also flagpoles attached to the frontage.

The former front gardens have been combined and paved in modern bricks to create a parking area, enclosed behind modern railings

set to either side of three pairs of concrete gate piers (these being modern introductions, although their positions might reference earlier side entrances). Modern railings have been added inside the gateways but the garden wall has been removed for vehicular access. Modern railings also run along the north side above a modern wall that frames the steps leading to a side or rear entrance. There is inappropriate standing signage in the parking area.

To the rear the building is taller and deeper than Chester Terrace and is in a plain, dark, brown brick. Its ground floor is obscured by two over-sized modern structures, one continuing across the glazed infill extension, whilst the first-floor windows appear to be later alterations, their segmented flat arches plain and the surrounding brickwork rebuilt. The second floor is the least altered, its windows having splayed flat segmented arches and stone cills.

The building has been united with no. 452 (part of Dover Terrace, 21) adjacent (linked by the glazed infill addition), undermining the integrity of the historic plots. This combined space is occupied by a full-length brick storage building abutting no. 448 and two shorter glazed buildings against the rear frontages of nos. 450 and 452, all single storey. The northern building (no. 448) fills the entire former garden to this plot, runs to the pavement edge and has a brick skin, concrete lintels and brick cills, metal sheeting to its pitched roof, 20<sup>th</sup> century metal framed windows and unsympathetic cameras and roller shutter. The add-on to no. 450 is later, being metal framed and clad with glazing to the gable beneath its pitched roof, and abuts a similar structure to no. 452 (with a shared yard). The yard itself is hard landscaped and used for storage and loading, an unsympathetic palisade fence and gate fronting onto Wright Street, with barbed wire above.



## Dover Terrace (18)

### 452-460 Chester Road (even)

**Status:** Positive Contributor Building

**Date of construction:** Mid/late-19<sup>th</sup> century

**Historic use:** Residential

**Current use:** Residential and some commercial

**Condition:** Good

These alternating double- and single-fronted properties are grander and of far greater visual interest than the terrace opposite, their canted bays reminiscent of the north end of Trafford Place (21). The northernmost property has been lost, its site now occupied by the modern infill extension. Otherwise, however, the brick frontage survives intact, having classical proportions and a canted ground floor with central entrances, its detailing including stone string courses, dentilled eaves cornicing and segmented headers, fanlight windows and timber architraves, all in good condition, the chimneys all retained above (alternating paired and single and complete with chimneypots). The symmetry is also retained despite that loss of two southernmost properties most evident in the modern gable end (extended to the rear). Mainly sympathetic sash windows further enhance the historic character, as do the low stone walls (partially rebuilt), sympathetic railings and soft landscaping. The insertion of basement windows and the addition of rooflights, alarms and similar features are comparatively unobtrusive, the glazing bars to the less sympathetic of the sash windows having greater visual impact.

The terrace historically continued further to the south, where its side return to Darwen Street appears to have mirrored that to Trafford Mount facing. Its loss, and replacement with an austere

parking area, has undermined this framing to Darwen Street and compromised the otherwise continuous enclosure of the street to Chester Road.

Otherwise, the terrace rear shows a good survival of the historic fenestration pattern (although with unsympathetic replacement windows in uPVC) and a small number of historic additions/projections of various sizes. The gardens, although retaining their historic integrity, are hard landscaped for parking, with poor boundary treatments and only one property retaining any soft landscaping to the border. To no. 454 a long modern extension fills the entire former garden and fronts onto the pavement, its former garage door now infilled with blockwork. Rendered and with a roof of metal sheeting, this dominates the rear of the building and erodes its historic domestic character.

No. 452 has now been united with nos. 448-450 (20) and a pitched glazed bolt-on building abuts its rear elevation at ground floor, the combined yard shared with no. 450 used for storage and loading.



## Darwen Terrace (19)

### 4 and 6 Darwen Street

**Status:** Positive Contributor Building

**Date of construction:** Late-19<sup>th</sup> century

**Historic use:** Residential

**Current use:** Industrial and/or vacant

**Condition:** Poor

This intact pair of substantial brick buildings is the last remnant of historic Darwen Street and of the housing historically situated to the west of Wright/Hadfield Street. It is much finer architecturally than the houses once situated to its north, having canted ground-floor bays, dentilled eaves cornicing and a stone string course echoing the terraces to Chester Road, plus brick dentillation and moulded stone keystones - fine features now seemingly unique in the conservation area. The brick boundary wall also survives intact across the frontage, partially rendered, with both sets of stone gate piers, whilst the chimney breasts are exposed to either side, both with break-forward ground-floor bays adjacent.

As the frontage to the SAI site, which continues to its rear, the building shows evidence of modern alterations, with an arched window lintel bearing the date 1991, stone mullions below probably added at the same time, plus uPVC windows, rooflights, security grilles, cameras and roller shutters. Both properties appear underused. Their large rear gardens have been lost and the domestic character is here entirely eroded, the rear to no. 6 extended and now abutted by an industrial building and no. 4 (south) opened up into the modern warehouse behind.



## Trafford Mount (20)

464-470 Chester Road and 1 Darwen Street

**Status:** Positive Contributor Building

**Date of construction:** Late 19<sup>th</sup> century

**Historic use:** Residential

**Current use:** Residential

**Condition:** Good

This truncated brick terrace makes its principal contribution to townscape character in bookending the southern boundary of the conservation area and providing historic context to Darwen Terrace behind. To Chester Road it appears very similar in design and detailing to Dover Terrace (18), differing primarily in its shorter first-floor windows and the render at plinth level, with all fanlight windows, two sets of entrance steps and all but one of the corbelled timber architraves retained. On the more assuming frontage to Darwen Street the original entrance steps lead to a classical brick porch with dentilled eaves corning to its timber roof, matching details on the window at first floor above, and a Diocletian window with stone keystones and corbels at attic level, being almost identical to the north frontage of Winfield Terrace (22).

Externally, the building retains what appear to be the original stone wall and gate piers to the street, although pierced by wide vehicular entrances in the south. All windows are modern, rooflights, basement windows and replacement fascias and entrance steps have been added, the modern south gable end is entirely blind and the gardens now form two hard landscaped forecourts, one in car park use. Some historic outriggers (closet wings) survive to the rear, primarily in the north, but their fenestration is altered and they are heavily obscured by later additions and the wholesale use of render, with the roofing materials modern.





## Trafford Place (21)

### 429-469 Chester Road (odd)

**Status:** Positive Contributor Building

**Date of construction:** Mid/late-19<sup>th</sup> century

**Historic use:** Residential

**Current use:** Residential and commercial

**Condition:** Good

Occupying almost the entirety of the east side of this section of Chester Road, this brick terrace is original in the north, where it has a prominent townscape presence. Built in phases, the classical proportions and simple detailing are consistent, the timber corbelled architraves with fanlights above, and the stone string course and cills, segmented headers and dentilled eaves cornice uniting the terrace. The northernmost properties are of the highest architectural quality and status and the best preserved, being double-fronted with two double-height canted bays, those to their south mainly single-fronted and the canted bays single-storey before ending altogether. The boundary wall (in many places rebuilt) further contributes to the townscape character with railings above and areas of soft landscaping behind but is weakened where the replacement wall is poor, whilst gate piers survive only at no. 453. The southern section of the terrace is a rebuilding, the windows smaller, the architraves missing and the roof lower (and the rear modern in character).

Although in good condition to its front, the terrace has lost many of its chimneys, with others truncated, whilst sympathetic sash windows are concentrated in the north, others being uPVC casements. Wholesale hard landscaping to many of the gardens has had a detrimental impact on the character of the area and

there are rooflights and isolated dormer windows, basement conversions and painting. To the rear it is again the northernmost properties that retain most historic character, although there are modern additions even here, the remainder of the terrace dominated by poor-quality modern additions and all gardens now converted for parking (and some amalgamated), set behind generally poor boundary treatments.



## Winfield Terrace (22)

### 473-497 (odd) Chester Road and 2a Chorlton Street

**Status:** Positive Contributor

**Date of construction:** Mid/late 19<sup>th</sup> century

**Historic use:** Residential

**Current use:** Residential and commercial

**Condition:** Good

Comprised of five or more separate elements, this long terrace is more ad-hoc than Trafford Place (21), with changing brickwork, rooflines and string courses, varied brickwork, differences in chimney design (and inconsistent survival), and canted bays to one property only. This variation reduces its coherence and its contribution to townscape character, although the terrace shares the simple classical proportions and detailing visible in other groups. Features of note include corbelled timber architraves (which survive to most doors), fanlights (some with historic coloured glass), segmented headers, a stone string course and dentilled eaves cornices. The southernmost property has a stone keystone and corbels to the fanlight and the north gable almost identical to Trafford Mount (20) in its entrance steps, classical brick porch with dentilled eaves cornicing, matching details on the window at the first floor, and attic-level Diocletian window with stone keystones and corbels.

There are uPVC windows throughout, commercial signage, and areas of modern fascias, whilst the stone boundary wall (which is without gate piers) has been removed at several properties and gardens hard landscaped for parking. It is not clear to what extent the end properties have been rebuilt/refaced. To the rear, whilst many properties retain the historic outriggers (closet wings) these

have been enveloped and infilled with modern additions, whilst others have been replaced by new commercial structures on a large footprint. All gardens have been converted to parking (and some amalgamated), with austere security fencing and an evident reorientation to create commercial frontages.



## Blackley Street, road setts (23)

### Off Northumberland Road

**Status:** Positive Contributor

**Date of construction:** By the 1910s

**Historic use:** Service access

**Current use:** Access to modern houses and rear access to other buildings

**Condition:** Good

The southern part of this block between Stretford Road and East Union Street was developed after the 1840s, and by 1889 Old Trafford Police Station existed in its east and there were houses to East Union Street and most of Stretford Road, plus a single dwelling to Northumberland Road. Access to the rear of the houses to Stretford Road and East Union Street was probably formalised when the Old Trafford Technical Institute was built in 1899-1900, with Blackley Street recorded by name in the OS map surveyed in 1915.

The original Yorkshire stone setts still survive, as do the profiles of the original drainage channels (now with a cement surface). The original pavements (if any) are likely to have been flagstones, with matching stone kerbs, and could potentially survive beneath the present-day tarmac and concrete (which are generally poor). Boundary treatments are also mainly poor and often missing (most notably in some of the newer housing, where the pavement has been absorbed informally as parking). There remains, however, a strong sense of an enclosed roadway running into the interior of the block, no example of which survives elsewhere in the conservation area.

Blackley Street is important as the sole remnant and evidence of the historic sett surfaces that were once omnipresent across the conservation area's roads, as its most intact rear service road, and for the views that it offers of the rear elevations of the former Technical Institute and the dwellings to Stretford Road, which are far less altered than those in the conservation area as designated.

The road now provides access to modern houses built on the police station site and to the rears of the former Technical Institute and the historic dwellings.



## Old Trafford Technical Institute (24)

540 Stretford Road

**Status:** Positive Contributor

**Date of construction:** 1899-1900 (extended 1913)

**Historic use:** Public free library with technical school classrooms and halls

**Current use:** Temple

**Condition:** Good

The former Old Trafford Free Library and Technical Institute has substantial streetscape presence and frames and closes views along Northumberland Road from the south. Architecturally impressive and with a notable presence within its corner plot, it shares much of its form, style and detailing with the more ornate Essence Factory to its west, the two being contemporary (both dating from the very last years of the Victorian period). Its red (Ruabon) brick and terracotta match the materiality of the conservation area as a whole and the Essence Factory in particular, its octagonal turret (here with a clock), engaged piers, dentillation, terracotta dressings and other detailing all showing clear similarities to the Essence Factory, the similarities continuing in its enclosed forecourt with hedge and brick boundary wall.

As a library, vocational educational provider and lecture room this building played a substantial role in the lives of the people living and working in the Empress Conservation Area, giving it status not only as an architectural and spatial landmark but also as a social landmark.

The building closed as a library in the 1970s.



## 522-524 Stretford Road (25)

*and*

## 518-520 Stretford Road (26)

**Status:** Positive Contributors

**Date of construction:** Second half of the 19<sup>th</sup> century

**Historic use:** Residential

**Current use:** Residential

**Condition:** Good

These are two pairs of high-status red brick terraced houses of two principal storeys, built at some date between 1849 and 1889. The two pairs are of a similar date, form and style, although of a different design (with nos. 518-520 more ornate) and, therefore, form an attractive and cohesive townscape group with Hullard Hall Terrace (27) adjacent.

This pair continues the materiality of the conservation area, combining red brick fabric with stone dressings, also echoing its terraces in detailing that includes overhanging eaves with timber corbelling, brick chimneys, eaves detailing, single-storey bay windows and two-storey canted bays, and brick chimneys. It is, however, a particularly fine example of these Victorian terraces, with its tall chimneys (some with caps) and elaborate architraves of brick and stone unrivalled within the conservation area. They also retain their red brick boundary walls with stone copings (one wall rebuilt) and the original stone gate piers, nos. 522 and 524 also having the original steps. All of the forecourts are still in garden use (although hard landscaped).

Retaining its domestic character, this group has high architectural and historic interest despite some alterations. To the front these

include the addition of rooflights and both modern and historic dormer windows, uPVC windows, a general buildup of clutter associated with subdivision, a basement conversion and an attic conversion to nos. 518-520. To the rear, no. 520 appears rebuilt, whilst nos. 522 and 524 have small modern extensions, 522 also being rendered. All, however, retain their rear garden spaces (hard landscaped for parking at no. 524), no. 524 with its historic boundary wall (with a wide vehicle entrance) but that to no. 522 replaced using concrete panels.



## Hullard Hall Terrace (27)

### 516 Stretford Road

**Status:** Positive Contributor

**Date of construction:** Second half of the 19<sup>th</sup> century

**Historic use:** Residential

**Current use:** Residential

**Condition:** Good

A substantial three-storey villa built present by 1889, this is an important example of the fine early villas once found across this area (all others in the conservation area now demolished), evidencing its varied historic character and social composition. Its name, recorded in an engraved tablet, evokes the large Hullard Hall and Hullard Hall Gardens historically facing the building across Stretford Road (part surviving today as Hullard Park). In material and style there are marked similarities to nos. 518-524, combining red brick with stone dressings and with detailing including three-storey canted bays, brick tile dentillation and an arched architrave of brick, stone and granite, in addition to crow stepping in brick.

The red brick boundary wall with its coping stone and gate piers is retained across the frontage, a hedge rising behind and above and the forecourt still in garden use, the gable end rising prominently above East Union Street and the whole having a degree of townscape presence. There are uPVC windows (although some sashes survive to the side), a modern roofing material, and satellite dishes, plus modern rooflights to the rear pitches, but the historic character remains.

The plainer rear elevation, visible from both East Union Street and Blackley Street, is comparatively unaltered and retains its historic domestic character. However, the loss of the garden for commercial garage use (probably in the 1950s) and the accumulation of bins to the alley entrance is detrimental.



## 18-20 East Union Street (27)

**Status:** Positive Contributor

**Date of construction:** Second half of the 19<sup>th</sup> century

**Historic use:** Residential

**Current use:** Residential

**Condition:** Good

These two dwellings survive from a short terrace of three and form a symmetrical pair. Two storeys high to the frontage, they are each single-fronted, with a face of red/brown brick, set back from the road behind gardens. The principal frontage is defined by paired front doors with a canted bay to the outside, vertical windows above, set below a slate pitched roof. Closet wing (outrigger) extensions into the long rear gardens give a T-shaped profile.

Although showing some change, particularly to no. 20, this pair provides the sole surviving historic character to East Union Street beyond the return of Hullard Hall Terrace (27) adjacent. This gives historic and architectural interest. Features of interest include the repeated banding in blue engineering brick, matched by the stone cills to the first floor, and the original timber doorcases with incised detailing, set below segmented brick segmental arches with brick dentillation. The canted bays have slate roofs, that to no. 18 retaining its iron balustrade (although not intact), whilst no. 20 has an eaves barge board with a simple incised circular pattern, that to no. 18 plain. The front doors and transom windows are modern replacements whilst no. 18 has replacement sash windows and no. 20 modern casements. No. 18 retains its chimneys to either side of the apex, the chimneybreasts visible on the west wall, set in concrete, where the poorer brickwork of the former party wall is now exposed (the abutting building now demolished). The east wall has a single ground-floor window and is otherwise plain. All doors,

windows and downpipes are modern, whilst no. 20 has an inserted basement window and has lost its chimneys. There are also prominent television aerials and, to the canted bay of no. 20, lead flashing.

Front gardens have been retained (although overgrown). The brick boundary walls survive to the front, at no. 18 with the original stone copings and to no. 20 with replacement brick copings. No. 18 lacks the original sandstone gate piers present at no. 20, its own gateway being a later addition, but has a name stone and shows signs of earlier rendering, whilst both gates are modern. The tall side wall and fence to no. 20 are modern, whilst to no. 18 there is only a hedge, beyond which a modern timber gate provides vehicular access.

To the rear the houses are more changed and make less of a contribution to the streetscape. The original brown brick is mainly concealed behind substantial closet wing extensions in a lighter brick, no. 20 having a small lean-to also. No. 18's windows match those to the front but those of no. 20 are conspicuous for their unsympathetic modern shape and white uPVC. Both also have skylights to the closet wing, whilst no. 18 has numerous solar panels to the rear pitch of the principal roof. The houses themselves are set back from Blackley Street within their gardens but No. 18 has vehicular access, small sections of its historic boundary wall surviving to either side of a modern garage.

Whilst it retains features of interest, there is a marked contrast between the embellishments and scale of 18-20 East Union Street and the grand properties adjacent to Stretford Road, its simpler detailing and smaller scale casting light on their very different historic built and social contexts.





# Conservation Area Appraisal and Management Plan

## Appendix D: Control Measures Brought About By Designation and New Article 4 Direction



## Appendix D Control Measures Brought About By Designation *and* New Article 4 Direction

### Article 4 direction – Empress Conservation Area Control Measures Brought About By Designation

1.1.1 In determining applications for development in conservation areas, local planning authorities must pay special attention 'to the desirability of preserving or enhancing the character or appearance of that area'. This requirement, as set out in legislation, is also reflected in national and local policy.

1.1.2 In order to protect and enhance conservation areas any changes that take place must do so in a way that encourages positive conservation and management. Statutory control measures are designed to prevent development that may have a negative or cumulative effect on the character and appearance of an area.

1.1.3 The statutory control measures include the following:

- Planning permission is usually required to totally or substantially demolish buildings or structures including walls, gate piers, gates, chimneys, fence or railings within a conservation area.

- The extent of 'permitted' development is reduced for commercial and residential properties restricting such things as cladding, extensions to the side of the original dwelling or the installation of satellite dishes. Further control measures such as Article 4 Directions may be placed upon an area. These may be served to further restrict permitted development rights, for example, elements or alterations such as windows, doors, chimneys, boundary walls and gate posts and restrict certain types of extensions.

- Trees with a stem diameter of 75mm or greater, measured at 1.5 metres from soil level, enjoy a measure of protection if they stand in a designated conservation area. Trafford Borough Council requires six weeks written notice of any proposed felling or pruning of such trees, other than the removal of dead wood and the felling of dead and/or dangerous trees, which do not require notification. In the case of the removal of undesirable trees to allow superior trees to flourish, known as 'selective thinning', the requirement is relaxed to allow the removal of trees

of stem diameter up to 100mm to be removed without giving Trafford Borough Council prior notice.

- Should the notified tree work be unacceptable to the Council, the latter will make a Tree Preservation Order during the six week notification period, thus ensuring continuity of protection. Local authorities cannot insist upon a replacement for a tree lawfully felled within a conservation area, unless the tree is also protected by a Tree Preservation Order.
- Certain categories of advertisement which have deemed consent under the Advertisement Regulations are restricted in areas of special control.

## Article 4 Direction – Empress Conservation Area

There are two types of Article 4 Directions proposed for the Conservation Area to restrict development:

1. Directions under Article 4(1) affecting other buildings:
  - 1) Dwellinghouses in the conservation area not fronting onto highways, waterways and open spaces.
  - 2) Other properties in the conservation area (e.g. commercial buildings).
  - 3) Properties not in the conservation area.

***These directions require approval from Secretary of the State***

1. Directions under Article 4(2) affecting dwellinghouses in conservation areas.

***These directions do not require approval from the Secretary of the State***

## Permitted Development rights proposed to be removed:

### The Town and Country Planning (General Permitted Development) (England) Order 2015

#### PART 1 - Development within the curtilage of a dwellinghouse

- A. The enlargement, improvement or other alteration of a dwellinghouse.
- C. Any other alteration to the roof of a dwellinghouse.
- D. The erection or construction of a porch outside any external door of a dwellinghouse.
- F. The formation, replacement or alteration of a hard surface.
- G. The installation, alteration or replacement of a chimney, flue, or soil and vent pipe.
- H. The installation, alteration or replacement of a microwave antenna.

#### PART 2 Minor operations – (All Classes)

- I. The erection, improvement, or alteration of any gates, fences, walls or other means of enclosure.
- J. The formation, laying out and construction of a means of access to a highway.
- K. The painting of the exterior of any building or work.

- L. The installation, alteration or replacement on a building of a closed circuit television camera to be used for security purposes.

#### PART 7 Non-domestic extensions, alterations

- E. Hard surfaces for shops, catering or financial or business premises.
- F. The extension or alteration of an office building.

#### PART 11 Heritage and demolition

- E. Any building operation consisting of the demolition of a building.
- F. Demolition of gates, fences, walls etc.

#### PART 14 Renewable energy

- E. The installation, alteration or replacement of microgeneration solar PV or solar thermal equipment.
- F. Installation or alteration etc of solar equipment on non-domestic premises.

# Conservation Area Appraisal and Management Plan

## Appendix E: Glossary and Further Guidance



## Appendix E Glossary and Further Guidance

### Glossary:

**Active Frontage:** 'Making frontages 'active' adds interest, life and vitality to the public realm', by frequent doors and windows, narrow frontage buildings, use of projecting bays and porches and, where appropriate, 'lively internal uses visible from the outside' (Trafford Design Code).

**Amenity:** 'the desirable or useful features of a building or place which support its ongoing use and enjoyment by building occupants, residents, visitors, workers etc. It is usually understood to mean visual and aural amenity (Trafford Design Code).

**Appearance:** 'the aspects of a building or space within the development which determine the visual impression the building or space makes, including the external built form of the development, its architecture, materials, decoration, lighting, colour and texture. In the case of a space, its landscape also influences its appearance' (Trafford Design Code).

**Built form:** buildings, their shape in 3D, alignment, configuration and function and their spatial relationship with the plots, streets and open spaces surrounding.

**Character:** 'Character includes all of the elements that go to make a place, how it looks and feels, its geography and landscape, its noises and smells, activity, people and businesses. This character should be understood as a starting point for all development. Character can be understood at three levels; the area type in which the site sits, its surroundings and the features of the site' (Trafford Design Code).

**Character Area:** a discrete geographical area that has a broadly consistent character with definable characteristics, in the instance of Empress forming a specific zone or sub-area inside the conservation area as a whole.

**Conservation:** 'the process of maintaining and managing change to a heritage asset in a way that sustains and, where appropriate, enhances its significance (NPPF, Annex 2).

**Conservation Area:** 'an area of special architectural or historic interest, the character and appearance of which it is desirable to preserve or enhance' (Planning (Listed Buildings and Conservation Areas) Act 1990).

**Design code:** a set of illustrated design requirements that provide specific, detailed parameters for the physical development of a site or area.

**Design guide:** 'a document providing guidance on how development can be carried out in accordance with good design practice, often produced by a local authority' (Trafford Design Code).

**Designated Heritage Asset:** 'a World Heritage Site, Scheduled Monument, Listed Building, Protected Wreck Site, Registered Park and Garden, Registered Battlefield or Conservation Area designated under the relevant legislation (NPPF, Annex 2).

**Details or detailing:** 'the individual components and how they are put together. Some are a deliberate part of the appearance of a building, including doors, windows and their surrounds, porches, decorative features and ironmongery. Others are functional, although they can also contribute to the appearance of a building. These include lighting, flues and ventilation, gutters, pipes and other rainwater details. Detailing affects the appearance of a building or space and how it is experienced. It also affects how well it weathers and lasts over time' (Trafford Design Code).

**Harm:** something that reduces or is detrimental to the heritage significance of a heritage asset.

**Heritage Asset:** 'a building, monument, site, place, area or landscape identified as having a degree of significance meriting consideration in planning decisions, because of its heritage interest. It includes designated heritage assets and assets identified by the local planning authority (including local listing)' (NPPF, Annex 2).

**Identity:** 'the identity or character of a place comes from the way that buildings, streets and spaces, landscape and infrastructure combine together and how people experience them' (Trafford Design Code).

**Landmark building:** a building that plays an important role in a streetscape or townscape and views within or across this.

**Nodal points:** urban spaces formed or emerging at the junctions of routes, often emphasized by key buildings or groups of buildings.

**Non-Designated Heritage Assets:** 'buildings, monuments, sites, places, areas or landscapes identified by plan-making bodies as having a degree

of heritage significance meriting consideration in planning decisions but which do not meet the criteria for designated heritage' (<https://www.gov.uk/guidance/conserving-and-enhancing-the-historic-environment#non-designated>, Paragraph 039)

**Positive Contributor:** 'buildings, monuments, sites, places, areas or landscapes identified by plan-making bodies as having a degree of heritage significance meriting consideration in planning decisions but which do not meet the criteria for designated heritage assets' (see <https://www.gov.uk/guidance/conserving-and-enhancing-the-historic-environment#non-designated>).

**Public realm:** 'the space between and within buildings that is publicly accessible, including streets, squares, forecourts, parks and open spaces' (Trafford Design Code).

**Scale:** the size (width, length and depth) of a building relative to the other buildings and architectural and landscape elements in its surroundings.

**Significance:** 'The value of a heritage asset to this and future generations because of its heritage interest.

The interest may be archaeological, architectural, artistic or historic. Significance derives not only from a heritage asset's physical presence, but also from its setting...' (NPPF, Annex 2).

**Streetscape (or street scene):** the natural and built elements that combine to create the overall character, design quality and visual environment of a street or road. These typically include the street furniture and signage, public realm, trees and open spaces, plus the buildings that line this mainly public open space.

**Townscape:** the visual elements that combine to create the distinctive character of a town. This urban landscape includes buildings, open spaces, and the relationships between and within these categories.

**Urban grain:** the pattern of plots within an urban block, which may be coarse (comprised of a small number of large plots) or fine (where plots are smaller and present in greater numbers).

**View:** 'What is visible from a particular point' (Trafford Design Code).

**Vista:** 'An enclosed view, usually a long and narrow one' (Trafford Design Code).

**Visual clutter:** 'The uncoordinated arrangement of street furniture, signs and other features' (Trafford Design Code).

**Wayfinding:** tools to avoid navigation through a place or area.

### **Further Guidance Links:**

Trafford Design Code (2023)  
<https://trafforddesigncode.uk/>

Trafford Design Guide (2022)  
<https://www.trafford.gov.uk/planning/development-control/Inquiries/World-of-Pets/CD-D-Statutory-Development-Plan-and-Supplementary-Planning-DocumentsGuidance/CD-D11-Draft-Trafford-Design-Guide.pdf>

National Planning Policy Framework (updated 2021)  
[https://assets.publishing.service.gov.uk/government/uploads/system/uploads/attachment\\_data/file/740441/National\\_Planning\\_Policy\\_Framework\\_web\\_accessible\\_version.pdf](https://assets.publishing.service.gov.uk/government/uploads/system/uploads/attachment_data/file/740441/National_Planning_Policy_Framework_web_accessible_version.pdf)

National Planning Practice Guidance (updated 2019)  
<https://www.gov.uk/guidance/conserving-and-enhancing-the-historic-environment>

Historic England online guidance: Looking after Historic Buildings  
<https://historicengland.org.uk/advice/technical-advice/buildings/>

Vacant Historic Buildings: Guidelines of Managing Risks (Historic England, updated 2018)  
<https://historicengland.org.uk/images-books/publications/vacanthistoricbuildings/heag183-vacant-historic-buildings/>

Stopping the Rot: A Guide to Enforcement Action to Save Historic Buildings (Historic England, updated 2016)  
<https://content.historicengland.org.uk/images-books/publications/stoppingtherot/heag046b-stopping-the-rot.pdf/>

Heritage Counts (Historic England, updated 2018)  
<https://historicengland.org.uk/research/heritage-counts/>

The Setting of Heritage Assets: Historic Environment Good Practice Advice in Planning Note 3 (Historic England, updated 2017)  
<https://content.historicengland.org.uk/images-books/publications/gpa3-setting-of-heritage-assets/heag180-gpa3-setting-heritage-assets.pdf/>

Partnership Schemes in Conservation Areas (Historic England)

<https://historicengland.org.uk/services-skills/grants/our-grant-schemes/partnership-schemes-in-conservation-areas/>

Breathe New Life into an Old Place - Make It a Heritage Action Zone (Historic England)

<https://historicengland.org.uk/services-skills/heritage-action-zones/breathe-new-life-into-old-places-through-heritage-action-zones/>

National Lottery Grants for Heritage

<https://www.heritagefund.org.uk/funding/national-lottery-grant-heritage>

## **Contacts:**

### **Trafford Borough Council**

General development enquiries concerning the Empress Conservation Area should be referred to Development Management. Telephone: 0161 912 3149. Email: [development.management@trafford.gov.uk](mailto:development.management@trafford.gov.uk)

Enquiries relating to trees within the Conservation Area should be addressed to the Local Planning Authority's Arboricultural Officer. Telephone: 0161 912 3199







Donald Insall Associates  
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Empress Design Code



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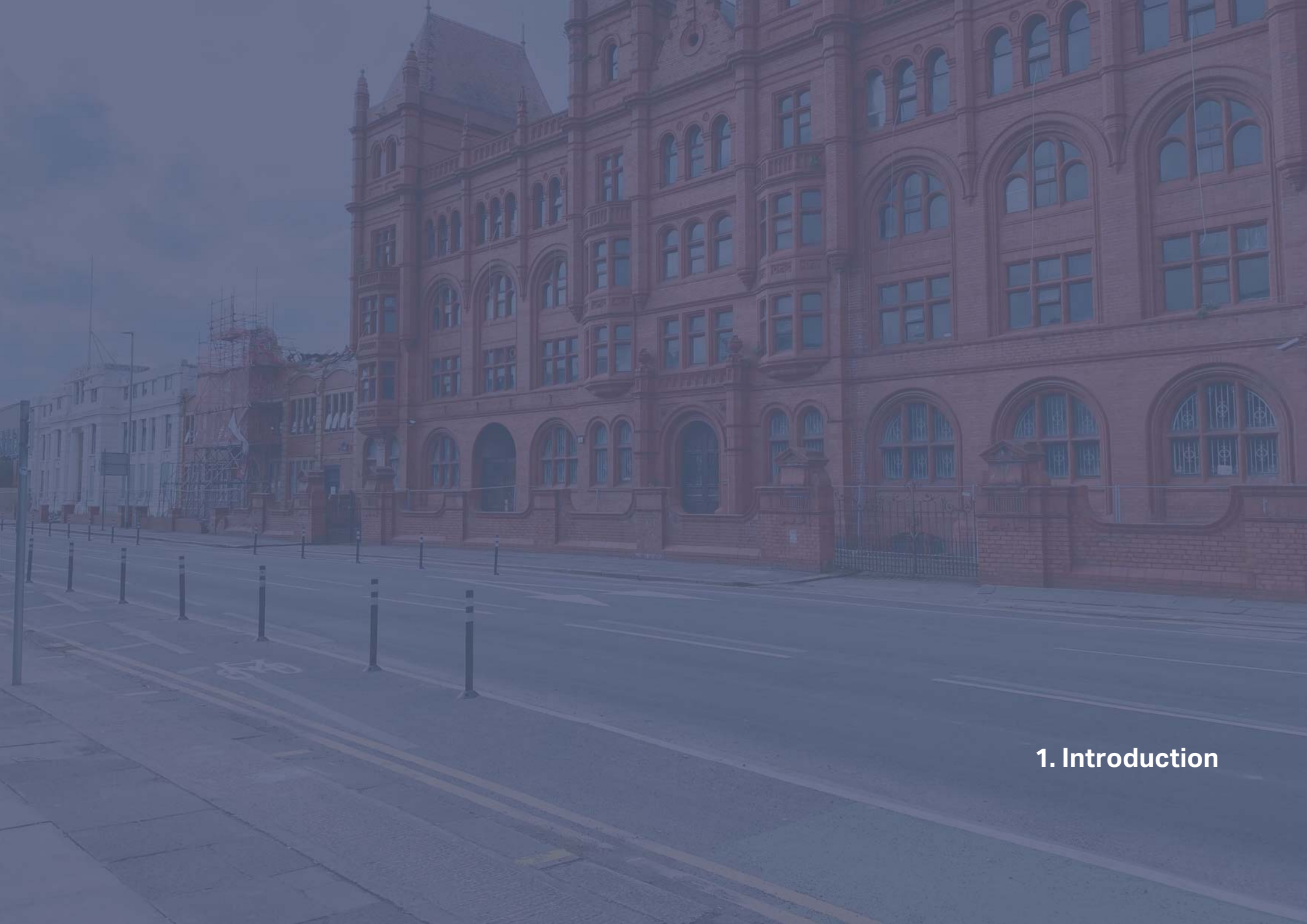
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## 1. Introduction

# 1. Introduction

## 1.1 Trafford Design Code

The Trafford Design Code seeks to take the best of Trafford and use that to underpin a set of design principles, codes and guidelines that will ensure that future development continues to offer distinctive, innovative and high quality placemaking that can be enjoyed for generations to come.

Written as a Supplementary Planning Document, it sits alongside the National Design Guide and National Model Design Code as a key material consideration in planning decision making.

Trafford is made up of a diverse collection of towns neighbourhoods and landscapes and the borough has a rich tapestry of streets and open spaces, with fine examples of architecture. The Trafford Design Code is therefore intentionally broad in its scope.

This design code focuses on the Empress Conservation Area and is intended to be much more site specific. It is based on a thorough analysis of the area, its history and what makes it special.

As such, the Empress Design Code deals with local materials, character and details and does not provide guidance on those broader issues, such as space standards or the principles of good design, that are covered in great detail in the Trafford Design Code. However, it follows the same broad structure as the wider Trafford Design Code for clarity and easy reference.

As the National Model Design Code explains, quality design takes different forms according to its purpose and the area of the country. It is the purpose of this design code to ensure local distinctiveness, protect character and streetscape, and encourage appropriate and high quality development.

This design code forms part of a suite of documents for to the Empress Conservation Area and should be read in conjunction with the Conservation Area Appraisal and the Conservation Area Regeneration Plan.

Applicants should also refer to the Trafford Design Code for further guidance in conjunction with this design code.

## 1.2 Policy Background

The National Planning Policy Framework (NPPF) makes clear that all local authorities should prepare design guides or codes and that these should be consistent with the principles set out in the National Design Code and National Model Design Code and should reflect local character and design preferences. Design codes and guides should provide a framework for the creation of high quality places.

Conservation areas are 'areas of special architectural or historic interest, the character and appearance of which it is desirable to preserve or enhance' (section 69 of the Planning (Listed Buildings and Conservation Areas) Act 1990) and are recognised for their distinctive townscape, including their buildings, streets, and public realm. The National Planning Policy Framework (NPPF) states that 'when considering the designation of conservation areas, local planning authorities should ensure that an area justifies such status because of its special architectural or historic interest and that the concept of conservation is

not devalued through the designation of areas that lack special interest'. In addition, Section 69 of The Act requires local authorities to, from time to time, review the character and boundaries of its conservation areas.

Once designated, conservation areas are considered to be 'designated heritage assets'; any proposals for change or development must assess the effect that the proposals might have on the character and appearance of the area. This is reinforced by local policy, which in Trafford includes The Core Strategy (2012) and the Revised Unitary Development Plan (2006), which are currently being replaced by the Trafford Local Plan, and the recently adopted Greater Manchester Places For Everyone Joint Development Plan Document (2024).



## 2. Context and Vision



## 2. Context and Vision

### 2.1 Site Character/ Local Identity

The Empress Conservation Area is of special interest as a rare survivor in Trafford of the mixed residential and industrial communities that sprang up in the second half of the 19<sup>th</sup> century as Trafford's new transport connections transformed its earlier, mainly rural, middle-class suburbs into dense urban areas. This distinctive duality to Trafford's historic development is expressed within the conservation area and its immediate surroundings, where manufactories, industrial offices/warehouses and rows of terraced housing are all found within a comparatively small geographical space. These buildings exhibit a range of architectural styles representative of this essentially Victorian character.

This is an inherited character that reflects the area's origins in the industrial booms of the late-18<sup>th</sup> and 19<sup>th</sup> centuries, supported by the rail and canal infrastructure that remains today, and has shaped its evolution through to the modern day. It is articulated by the red brick terraced houses focussed on Chester Road and the area's industrial buildings, many of which remain in operation. This includes landmarks such as the former Empress Brewery (now an active business centre) and the Essence Factory, the Veno Building and the former Trafford Press offices, and Empress Mill, clustered around Chester Road and Empress Street. These buildings and uses both contrast and complement one another to tell the story of the area's growth and development over time, and its continued importance to Trafford as a whole.



#### Key

- Current boundaries
- - - Proposed Inclusion
- ① Industrial development west of Hadfield Street
- ② Land to the south of Empress Mill
- ③ Land bound by Northumberland Road and East Union Street
- ④ West side of Darwen Street

## 2.2 Neighbourhood Placemaking Strategy

### **Future Vision for Empress:**

#### **A place to live, work, create and engage**

Building on the area's history as a place where traditional worker homes and industry sit side by side, the future of Empress will be a dynamic live-work environment where creative businesses, communities and industry come together to create a connected and liveable community.

As the area continues to be a place of innovation and making, the place-making strategy aims to build on its rich history as a live-work environment. Our vision is for the neighbourhood to become a place for communities to thrive in new, high-quality sustainable homes. New shops, cafés, and bars will enliven the place and embrace the independent and creative spirit that has existed at Empress for decades. Rather than turning its back on the surrounding community, it will be a welcoming place that opens its arms to the people who live and visit the area. Historic assets will no longer be unknown relics, instead becoming landmarks that define an exciting, characterful, and engaged community for the future.

The plan is to create green connections to surrounding parks and play spaces, along with community links to Ayres Road, Trafford Bar, Castlefield and Hulme. The aim is to unlock connections to the canal network and Manchester Waters, including a link to the new National Trust Castlefield Viaduct and Ordsall. There will be enhanced connections to surrounding public transport links and emerging development areas at Civic Quarter and Cornbrook. All these steps will help in making the area more accessible and well-connected, providing a more convenient and pleasant experience for the residents and visitors alike.

## Strategic Objectives

The following key strategic objectives will guide the change and growth of Empress:



### **Celebrate History and Character**

Celebrating the history, distinctive character and identity of Empress, drawing on the “Live-Work” history and creating a new lease of life for key buildings and landmarks in the area. Celebrate the emerging and existing community by reflecting the diverse, multi-generational and multi-cultural communities surrounding Empress.



### **Unlock Positive Growth**

Create opportunities for new development and use which preserves and enhances the existing historic and industrial fabric of the place. Support the sustainable and successful growth of the area through a dynamic mixture of uses - including new homes and local centre uses, whilst retaining the industrial core of the place.



### **A New Trafford Landmark**

Open a gateway within Trafford and between the wider city, which is inviting and unique, that celebrates the different cultures, people and the history of Trafford, through the independent identity of community, work, life and creativity.



### **A New Heart to the Community**

A celebratory sense of arrival, drawing people into the heart of Empress, unlocking active uses and bringing together people from different walks of life.



### **Connected into the Network**

Enhancing connections into and towards the area, creating a place that is easy to understand and creating better links to Trafford, Manchester and Salford.



### **Active and Engaged Community**

Creatively activate spaces and streets through a mix of uses and high quality public realm, encouraging community activity, engagement and pride. Supporting and integrating existing enterprise, creativity and independence.



### **Outward Facing and Active**

Knitting together the surrounding community, creating an welcoming edge which connects people into and through Empress and creates a new community destination for the area.

## Illustrative Masterplan

The design code focusses within the regeneration plan area. The plan on the right illustrates the vision for Empress and identifies sites for immediate development opportunities, retrofit and enhancement, potential future development opportunities and key opportunities.

The adjacent key moves underpin the structure of the masterplan and will foster a vibrant and inclusive new community. These moves will not only connect the community to the broader neighbourhood but also enhance its liveability and activity.

### Key

-  Regeneration plan boundary
-  Indicative layouts for development areas
-  Existing buildings to be retrofitted
-  Private, communal, and public green spaces
-  Key hard open spaces
-  Existing site plots and buildings
-  Immediate development opportunities
-  Sites for retrofit / enhancement
-  Future development opportunity for change



Illustrative masterplan

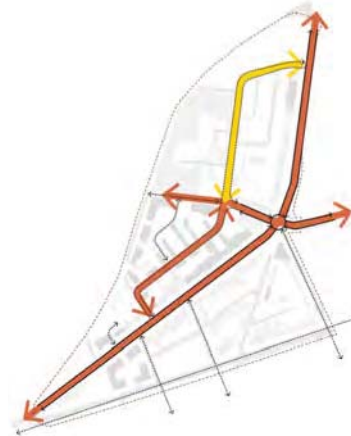
## 2.3 Design Vision

Our vision is to create an outward-looking community that not only engages the existing and emerging communities but also fosters a sense of belonging and inclusivity by offering a place to live, work, create and engage at the gateway to Old Trafford.

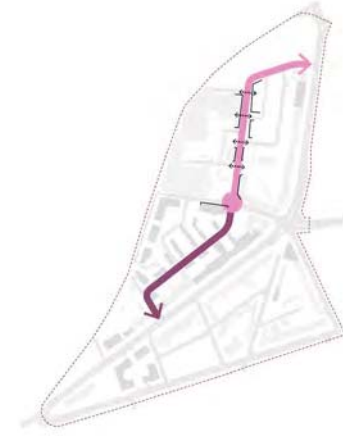
Empress Street marks the gateway into the heart of the area, a primary arrival point, offering the potential for enhanced public space and increased activity. Darwen Street and Hadfield Street will provide secondary arrival points and enable access to a new active central spine and a central heart at Empress Street, which will draw the new neighbourhood together. The northern section of the site will retain heavy goods vehicle access for the industrial and commercial units, while the southern part will form a residential focus.

A string of open spaces enables permeability to pass through the site whilst responding to the various uses, including pocket parks for meeting, spaces with informal play, community parks, routes for dog walkers, social spaces adjacent to meanwhile uses, and spill-out areas for breweries or other commercial activities. The open spaces align with the urban ecosystem strategy to form a network of green and blue infrastructure to access the site to connect people with nature and improve local biodiversity.

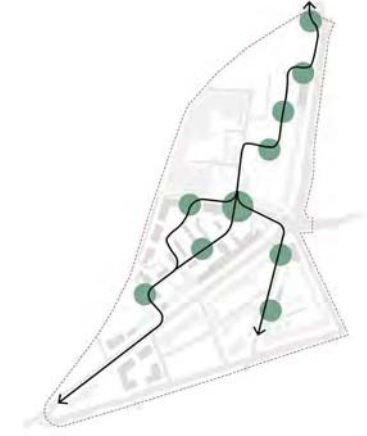
### Six Key Moves



1. Enabling connectivity



2. Active and creative spine



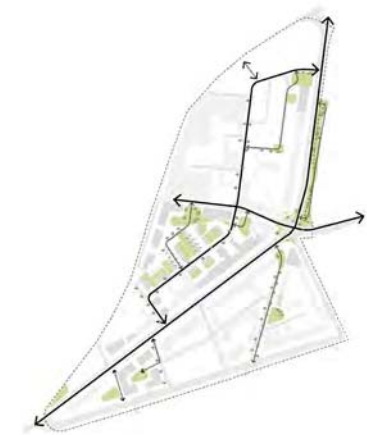
3. String of active public yards (pocket parks)



4. Unlock development opportunities



5. Rejuvenate, activate and celebrate historic assets



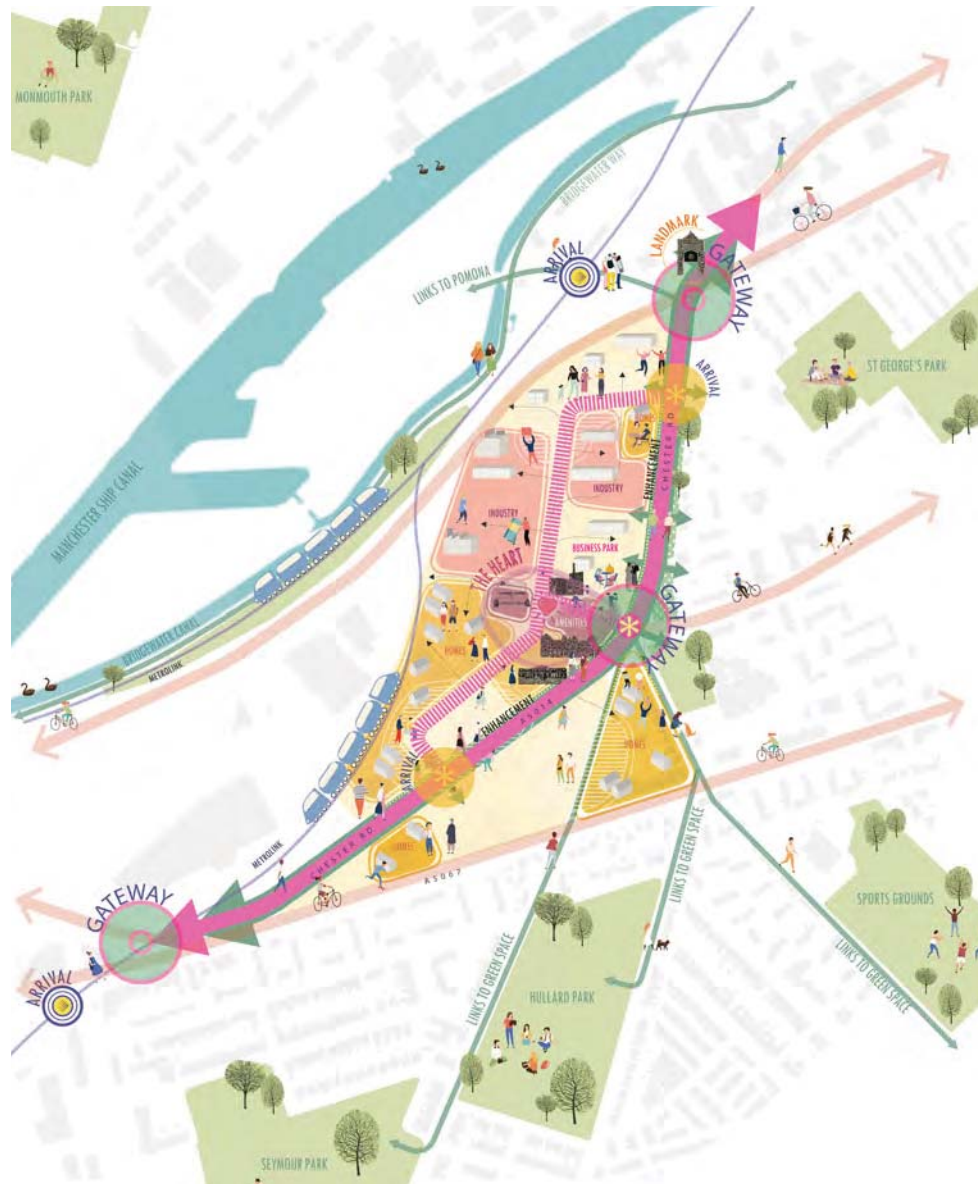
6. Define a new urban ecosystem

## Vision for Placemaking

The diagram on the right illustrates the vision for Empress to create a place to live, work, create and engage.

### Key

- Amenities and community uses
- Industry
- Business park
- Residential
- Local parks
- Regeneration plan area
- Important gateways
- Key routes to parks
- Key routes
- Existing routes



The vision for Empress to create a place to live, work, create and engage.



**3. Area Code**

## 3. Area Code

### 3.1 Area Code

#### Introduction

The Empress Design Code aims to support owners, developers, tenants and decision-makers in the future development of Trafford, particularly within the Empress Conservation Area. By providing clear guidance on appropriate change, as well as presenting exemplar developments elsewhere, this Design Code will inspire stakeholders to make the most of their assets, and ensure that future development continues to offer distinctive, high quality placemaking that can be enjoyed for generations to come.

#### Methodology

The Empress Design Code consists of the following sections:

- Houses - Codes relating to new housing including:
  - Type form and profile
  - Elevation and proportion
  - Materials and detail
  - Thresholds and boundaries
- Apartments - Codes relating to new apartment buildings including:
  - Form and profile
  - Elevation and proportion
  - Materials and details
  - Boundaries, servicing and plant
- Streets and public realm – Codes relating to new streets and public realm
- Landscape and nature – Codes relating to new and existing green infrastructure
- Plot codes - Plot codes are provided for the sites identified as immediate development opportunities on the masterplan and these are to guide the development of individual sites in relation to:
  - Proposed new green space
  - Access and mobility
  - Height
  - Building line and active frontages



## 3.2 Houses

### Introduction

The area has a rich mixture of historic architectural styles. Most obvious is the contrast between the small, two-story terraced houses, the small historic industrial units and the landmark buildings. All, however, show a variety of practical features and decorative detailing, articulating their varied origins and historic functions. There is, however, an overall sense of cohesion despite this variety, largely derived from their shared palette of materials.

Residential buildings in the Empress Conservation Area (some now in commercial use) are primarily Victorian and Edwardian terraced housing, with some examples of semi-detached and Georgian-style housing. Whilst united by their shared classical influences and generally cohesive materiality, there is an interesting (sometimes subtle) variety between the typically short terraces, which adds further visual interest and character.



Birkdale Terrace.

## HOUSES 1 – Housing Type, Form and Scale Relevant to Context

Developments within existing places will be required to respect the established house type form and profile in the immediate context.

The type, form and scale of housing must mirror those already present in the nearby area.

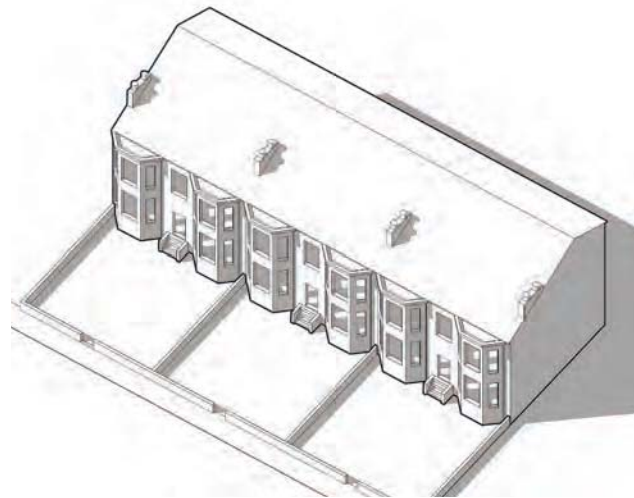
Infill housing projects on streets where there is an established housing type must be clearly influenced by these styles.

### Examples

The houses within the Empress Conservation Area are primarily Victorian and Edwardian terraced housing, with some examples of semi-detached and Georgian-style housing.

### **Type Form and Profile**

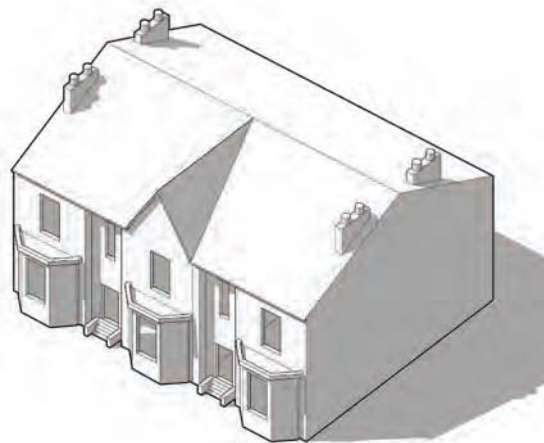
The type form and profile of a building has an effect on how it sits within the setting. It must seek to be complimentary to the surroundings, particularly in historic environments.



Terrace.



Trafford Place.



Detached.



Darwen Terrace.

## HOUSES 2 – Building Line

Houses must follow building lines to create visual order to streets. Any variance of setback or projections from an established building line must be influenced by the existing context.

Strong building lines contribute to the character of Empress, particularly along Chester Road, by creating a rhythm and order to streets where no single building stands out (projects).

The variance and extent of set backs or projections to a building line will be influenced by the existing context as this will strengthen the character which may be made up of a strong building line.

For projects on established streets, the existing building line must be identified and new proposals must demonstrate how they align with this, or justify the approach if it deviates. Designs must reflect the typical high-density, set-back, form of residential streets such as Chester Road.

There is a presumption against development in front gardens, including the addition of new porches and ancillary structures such as sheds or bike and dustbin stores.

### Examples

The building line to Chester Road, where most residential buildings are located, is strong. This creates cohesive and generally enclosed sweeps of domestic and commercial forms, reinforced by low brick and stone boundary walls and their railings.

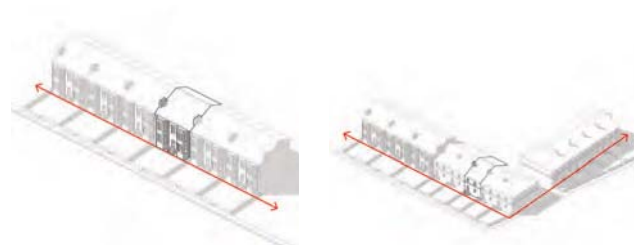
The surviving terraces (some now truncated) demonstrate a good degree of architectural detailing and historically incorporated gardens to the front and rear.



Trafford Place.



Winfield Terrace.



Strong building line.

Corner building line.



Precedent- Calais Street Lambeth by PRP Architects.

### HOUSES 3 – Roof Types Informed by Local Context

The roof types and profile must reflect the immediate context.

The roof type must be predominantly influenced by the immediate context. Elements of interest such as gables, gable ends and chimneys to the roofscape are encouraged.

Infill projects must demonstrate how the roof design will replicate or complement the established strong rhythm of roof types.

Alterations at roof level must:

- Not disrupt uniform rooflines or rise above the apex (ridge).
- Where rooflights are proposed these must be confined to the rear or side elevations (aside from those that are street facing), follow the hierarchy of openings to the storeys below and be set no more than 15cm from the plane of the roof slope – the use of conservation rooflights will be actively encouraged.

- New dormer windows will not be permitted and the removal of existing modern dormers, particularly those to the street-facing frontages, should be encouraged.
- Chimneys must not be removed or truncated.
- Roof slates must be Welsh or Westmorland, avoiding imported slates, which have a polished appearance. Where slates have been replaced with modern materials their reinstatement will be encouraged.

#### Examples

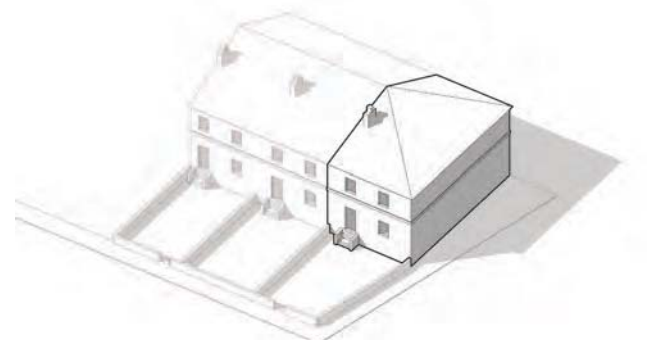
Existing houses within the area primarily have pitched or hipped roofs, some with gables, as at Birkdale Terrace. There are also some examples of gable ends, whilst hipped roofs are found on some corner sites, as at Trafford Place.



Typical terrace, gable ends shown in bold.



Gable ends at Birkdale Terrace.



Typical hipped roof.

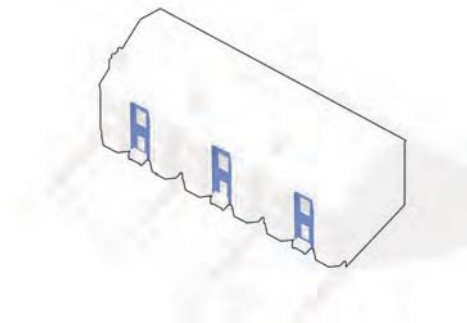
## HOUSES 4 – Depth and Articulation of Façades

New houses must create depth and articulation, and must generally not be flat or featureless. Much of the housing within the area has an undulating profile comprised of features such as set-back bays and recessed doors and windows. This is important in maintaining the building form of the area, particularly in the residential area of Chester Road. The extent to which features are recessed in new development must be informed by the local context and must reflect the internal layouts of nearby houses.

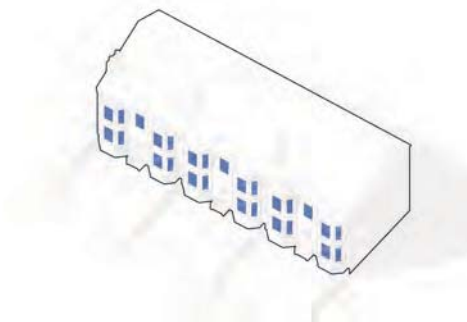
Developments that deliver an exceptionally high quality design through the use of an alternative design approach will be considered.

### Examples of Recessed Elements

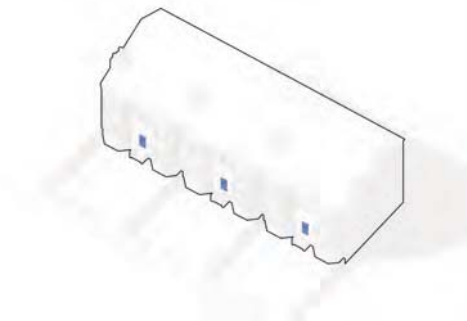
Entrance bays are the most common recessed element across the area. There is also some recessing to windows and doors within their reveals.



Recessed bays.



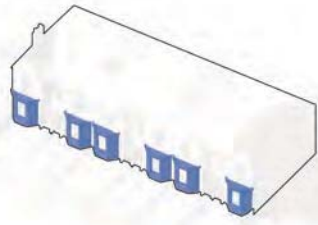
Recessed windows.



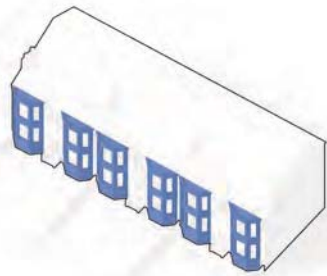
Recessed doors.



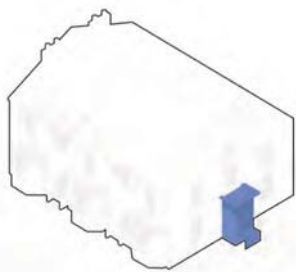
Recessed bay at Birkdale Terrace.



Projecting single-storey bay.



Projecting double-storey bay.



Porch.



Single-storey bays at Birkdale Terrace,



Double-storey bays at Trafford Place.

Examples of Projecting Elements  
Projecting elements found across the area include:

- Single-storey bay windows
- Double-storey bay windows (canted bays)
- Other bays
- Wooden doorcases (architraves)
- Porches
- Eaves and their detailing



Porch at Trafford Mount.



Precedent- Keelson Yard by 31/44 Architects

## HOUSES 5 – Entrance and Porch Articulation

Entrances must be clearly articulated and expressed. Designs must reflect the variety across the area.

There is a variety of attractive entrance designs within the area, which helps to define the distinctive character of houses and streets.

Designs must clearly articulate entrances and integrate them into the design of the elevation. Simple design solutions such as recessed porches can add character, depth and expression.

### Examples

Entrances usually have wooden doorcases, or architraves, and are typically of a portico-style or decorated with corbels. Fanlight windows sometimes feature numbering or house names, some historic. Doors are timber panelled and are often accompanied by stone or brick entrance steps with metal railings.

Original or historic timber doors, windows and architraves (surrounds) make an important positive contribution and must be retained:

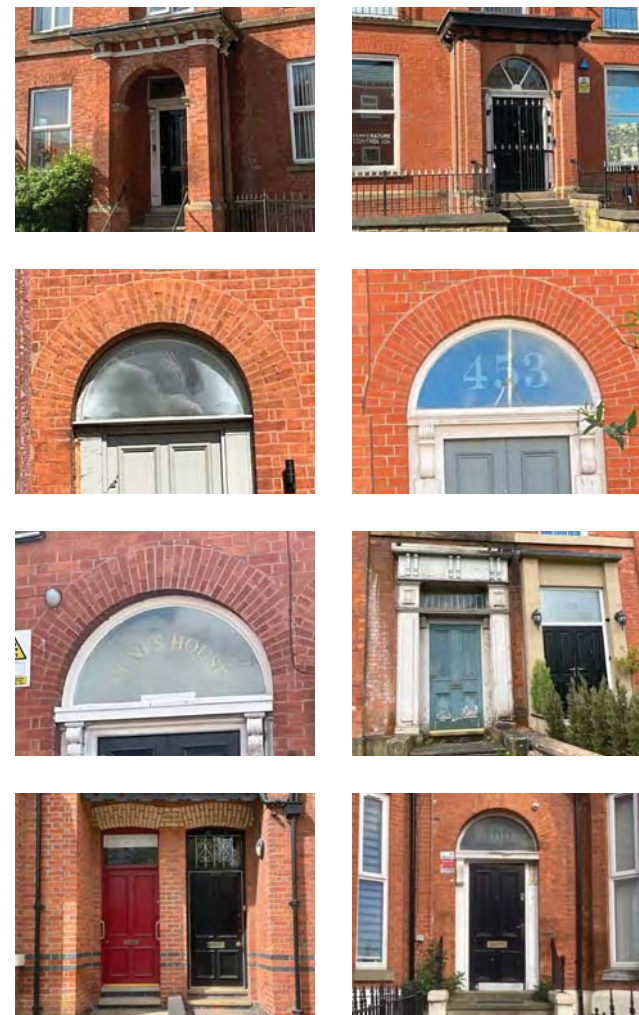
If refurbishment/repair is necessary this must be delivered on a like-for-like basis, replacing only the minimum fabric necessary.

Any replacement doors or windows must be sympathetic in their design and use of materials – typically timber. All windows must have suitably slim frames that do not detract from the proportions of the original opening. Architraves must be in timber.

Fanlight windows must be retained and any historic property names or numbers kept.

Doors and windows must not be relocated or existing openings altered in size or shape unless to reinstate a more appropriate opening where unsympathetic changes have been made previously.

Juliette balconies must not be added to historic buildings.



Entrance and porch articulation across the area.

## HOUSES 6 – Surrounding Context and Rhythm

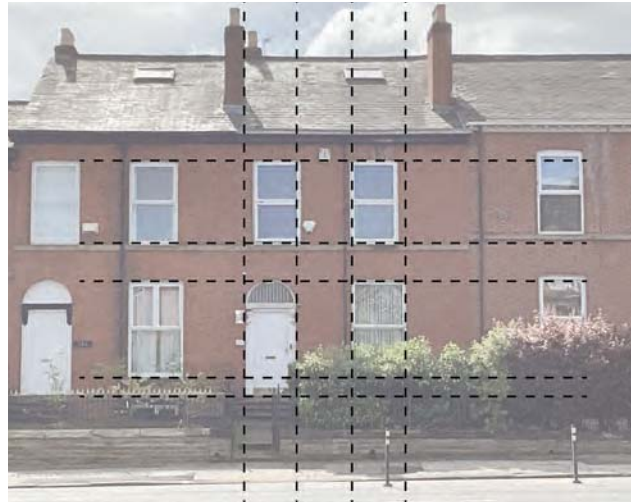
Designs for new houses must respect the context of their surroundings, maintaining suitable proportions in width and height, and ensuring coherence for visual appeal and easy comprehension. Designs must follow the rhythm established by neighbouring buildings and observe the same proportions and composition along the street.

The context of each site is unique and must be continued and referenced in the design. It is important that the context of the area is analysed in order to understand how the elevation of the project will replicate or reference that of its neighbours.

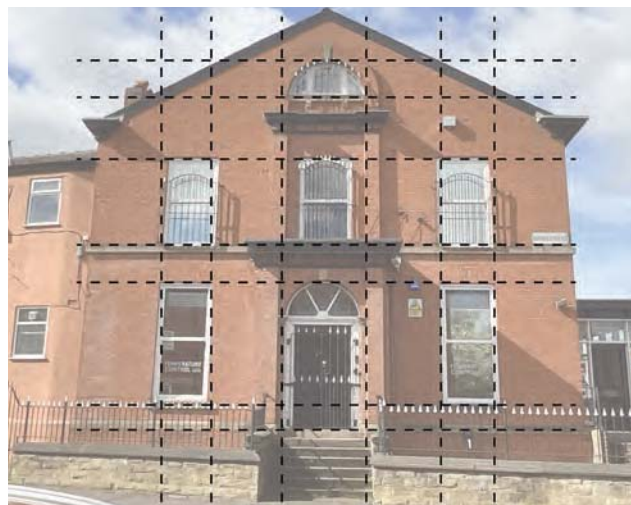
### Examples

Existing houses within the area have vertical openings, and follow patterns of symmetry in a formal arrangement.

Terracing brings a sense of order to the street, particularly on Chester Road.



Elevation rhythm of Winfield Terrace.



Elevation rhythm of Winfield Terrace gable end.

## Elevation and Proportion

Building elevations help to express the character and style of a development and must be designed as a response to the context. A well-proportioned elevation will be aesthetically pleasing, bring legibility and harmony to the building or series of buildings, and animate the street.



## HOUSES 7 – Shape and Proportion of Openings

Window and door openings must be in proportion with the shape of the principal facade.

Symmetry must be used to create order within a facade or between adjacent properties.

Neighbouring symmetry is formed when two adjoining buildings mirror each other, contributing to the overall structure of the street. Within individual detached houses, internal symmetry can enhance the visual appeal of a single facade.

Designs must follow the formal setting out of existing elevations, in which a set of shapes, proportions, and alignments follow a strict order to add structure and coherence.

### Examples

Windows are typically vertically sliding white painted timber sashes or side hung casements, often set back into reveals. Creating depth

and articulation to façades is a simple and cost-effective method for achieving visual identity and interest.



Precedent- Westbrook Villas by Calderpeel.



Precedent- The Acres by B2 Architecture.

## Materials and Detail

The materials and detailing of new building must be informed by and take cues from the historic surroundings wherever possible. Opportunities to enrich the design of the building through articulation and detailing must be considered; these too must take cues from the surrounding vernacular.



Examples of doors and windows across the area.

## HOUSES 8 – Recessed Doors and Windows

Doors, windows and their frames must have a set-back of at least half a brick. Brickwork or another material sympathetic to the local context must be used to articulate the space around windows and give depth and visual interest.

Windows and doors must be positioned slightly back from the main facade to show the exposed brickwork within this reveal. By recessing windows and doors, shadow lines will be created and small setbacks can introduce depth and texture variations to the façades.

### Examples

Typically, windows are painted timber sash windows on small-scale residential buildings, and metal-framed, factory-style, to industrial buildings. Typical features include decorative arched and half-moon windows, fanlights, cills and lintels of brick or stone.

## HOUSES 9 – Reference Local Building Materials

Reference common materials from the surrounding context and incorporate these into elevations as the primary material.

Paint or render must not be applied to external walls where this does not already exist or has not existed historically; where render is being replaced this must have a slight textured finish to maintain character and visual interest.

Look for design cues in the immediate area to influence your choice of materials. Whilst the materials used for traditional houses may not be appropriate for modern construction methods, materials must still make reference to the traditional colours, texture, bonding and brickwork used within the context of the area.

Trafford's places are characterised by the use of common building materials.

A study of the most appropriate type and use of local materials will result in a project that complements its local area.

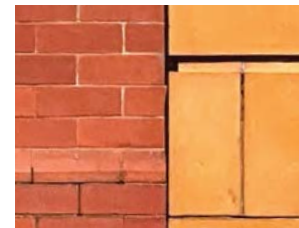
### Examples

Designs must reference common materials from the surrounding context and incorporate these into elevations as the primary material.

The Empress Conservation Area is characterised by a material palette of red brick and terracotta that provides a cohesive hue across the area and its varied uses.

Brick is laid in traditional bonds such as English, Flemish or English Garden Wall.

High quality materials such as terracotta and industrial metals can contribute to the material palette, connecting with the area's industrial activity both past and present and enriching its visual texture.



Examples of materials and detailing across the area.

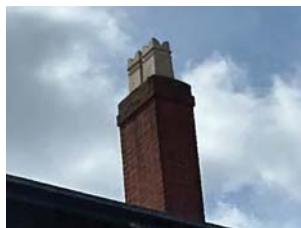
## HOUSES 10 – Roof Materials and Details

Roof materials must be high quality and reference the surrounding context. Roofs must incorporate well detailed eaves, verges and ridges.

The roof is often a dominant feature of a building and the shape, pitch, cladding and ornament - including chimneys - are important. Look to the surrounding context for design cues when considering roof materials and details.

### Examples

Natural slate tiles (Westmorland/ Welsh) are used throughout the area. Concrete tiles and artificial slates must not be used as they will erode the historic character. Brick chimney stacks in a variety of forms and styles are present, both domestic and industrial.



Examples of roof materials and detailing across the area.

## Thresholds and Boundaries

Boundary treatments must be well designed with the aim to create pleasant streets and improved security, distinguish between the public and private realm and increase biodiversity.

## HOUSES 11 – Boundaries

Boundary treatments must be in keeping with the surrounding traditional context and informed by high quality traditional examples in the surrounding area.

Where they survive, front gardens must remain as green spaces with trees retained and appropriately managed. Any new surfaces must be permeable and retain a landscaped character; non-permeable surfaces require planning permission and will be resisted.

Historic features such as steps and surfaces must be retained; their reinstatement where previously removed is encouraged.

The conversion of front gardens or corner plots for parking is not acceptable; this includes associated

alterations such as hard landscaping and the loss of boundary treatments. The enhancement of rear yards through the replacement of hard surfacing, reinstatement of lost boundary walls and greening including the introduction of trees, planters, beds or areas of lawn is encouraged and must form part of any development proposals.

Bin stores must be kept in rear service areas and not in front gardens.

Where properties are amalgamated or under joint operation their rear yards must not be amalgamated to form shared car parks; a visual distinction between historic plots must be reinstated as part of any development proposals.

Historic boundary treatments and existing landscaping must be retained. The reinstatement of boundary treatments incorporating low brick and stone walls, hedging and iron railings, and which follow the prevalent townscape character, is encouraged and must form part of any development proposals. The introduction of non-traditional materials must be avoided and any reinstatement must preserve the historic variation in boundary treatment visible between the different character areas and the individual terraces and buildings within these.

#### Examples

The building line to Chester Road is reinforced by low brick and stone boundary walls and their railings. Boundary treatments range from relatively simple and low-rise solid stone, brick or brick stone-faced walls with iron railings, to highly decorative mid-height solid brick and terracotta boundary walls.



Examples of boundaries across the area.

### 3.3 Apartments

Apartment buildings can bring greater density. When well designed, apartments can bring scale and definition to a site. The design, however, must be carefully considered in order to ensure that they will not compromise the quality of life of their inhabitants and have a positive impact on the streetscene.

#### Form and Profile

Apartment buildings can often be incongruous with the surrounding context. Therefore, careful consideration is required when thinking about their elevation, form and profile.

#### APARTMENTS 1 – Scale Form and Context

A context character appraisal must be carried out at the outset to establish the suitability of the site for an apartment building. Scale and form must respect that of the surrounding site context.

The context of each site is unique and it is important that the design and siting of any new development is

informed by an analysis of the existing site context, history and character.

Developments within existing places will be required to reflect the established scale and form of the surrounding context in order to strengthen the visual character of existing places.

Usually, apartment buildings will require greater height and mass than conventional housing, which must first be justified through an analysis of the surrounding buildings, urban context and topography.

In certain situations it will be necessary to replicate the scale, width and height of the surrounding context.

There may be circumstances where gentle densification will be acceptable, increasing the height and width of apartments compared to the surrounding context.

#### Examples

Take design cues from the conservation area when considering the composition of your elevation and roof form. The context of each site is

unique and must be continued and referenced in your design.

The residential buildings within the Empress Conservation Area demonstrate simple but effective methods for establishing correct elevation and roof proportions, window size, and rhythm along the street. This should be continued in new and infill housing projects.



Precedent- Warehouse by OMI.

## APARTMENTS 2 – Roof Form

Roof forms must respect the surrounding area context and be informed by a response to the varied roof types and rooflines of individual larger industrial buildings, which include flat, pitched, hipped and sawtooth elements and the pitched, gable end and hipped roofs of residential terraces.

Varied roof forms which complement and enhance surrounding roofscape are encouraged.

Pitched roofs are preferred and add interest and variety to the character of the area.

### Examples

There are a variety of roof types across the area including:

The combination of the Empress Brewery chimney and Essence Factory turrets, which puncture the roofline, rising high above.

The sawtooth (“north lights”) roof of the Empress Machine Tool Works and other buildings is a key townscape element.



Strong parapet line interrupted by a gable with pitched roof behind.



Sawtooth roof at Former Empress Machine Tool Works.



Empress Brewery.

### APARTMENTS 3 – Setbacks at Roof Level

Roof forms must reflect the surrounding area context and be informed by an analysis of the varied roof types and rooflines in the area, in particular larger buildings such as Trafford Press and Veno.

Where setbacks at roof level are used, the setback element must be designed as an integral part of the building using matching materials.

Setbacks must not simply be used as a method of achieving additional height. Setbacks must generally be applied on all sides of the building use materials matching those used on the rest of the building. Cladding solutions must be avoided.

### Examples

All new development must be of high quality. It must respect the character and appearance of the conservation area.

All new development must respect the skyline of the conservation area and any identified key views.



Precedent- Kensington High Street by Stiff and Trevillion.



Precedent- Trafford Town Hall by 5 Plus Architects.



## Elevation and Proportion

Apartment buildings must reference the proportions of the surrounding buildings. This includes features such as symmetrical elevation and vertical portrait proportions.

### APARTMENTS 4 – Coherent Appearance

Elevations must respect the surrounding context, be coherent so they are aesthetically pleasing and can be easily understood when viewed. There must be a rhythm to the composition of windows, balconies, entrances and other details within the elevation.

The context of Empress is unique and must be referenced in the design.

Apartments with well-balanced proportions and window sizes are generally visually appealing and filled with natural daylight. Traditional buildings throughout the Conservation Area exhibit best practice elevation and proportion and this should be continued in the development of apartment blocks.

Look to the site context to understand how the facade of your project will need to reference that of its neighbours to create a rhythm. Pick up on the spacing between window openings, projecting elements or roof details to see how that rhythm can be continued. Consider how stair and lift cores are detailed on elevations.

#### Examples

The execution of architectural styles and design varies across the area, as does scale, reflecting its historic variety of use. However, these buildings are drawn together by the street network and the material palette, which is largely of red brick and terracotta. The Essence Factory and adjacent landmark buildings are of greater scale and mass than the surrounding residential terraces and introduces significant architectural flair.



Converted warehouse into apartments, Chester Road.



National works.

## APARTMENTS 5 – Articulation

Façades must incorporate depth and articulation to add interest and relief to buildings.

Applicants must avoid flat, featureless and monotonous buildings. The articulation of façades is essential to animate larger elevations. Apartment blocks must introduce a combination of recessed and projecting elements in their design to create depth and articulation of façades, using the local context as reference.

This is essential in maintaining the building form of the area. The extent to which features are recessed must be influenced by the local context.

Elevations can be made more visually interesting by the use of recessed and projecting elements to its façades.

Further visual depth can be achieved by articulating façades with brick details, window surrounds and doorways.

### Examples

Canted bay windows are the most common projecting element across the area.

Often highlighted with decorations such as terracotta, gauged brickwork and projecting cills, other projecting elements found across the area include:

- Single storey or double storey bay windows including canted or squares
- Wooden doorcases (architraves)
- Porches
- Canopies



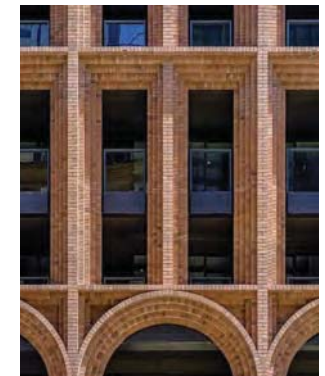
Precedent-  
Bayford Mews by pH+



Precedent-  
Oskar Residence  
by Stefan Forster



Precedent- The Caxton by  
Fletcher Priest Architects



Precedent- Arc by Koichi  
Takada Architects

## Materials and Details

The materials used for a building affect how well it functions and lasts over time. They also influence how it relates to what is around and how it is experienced.

### APARTMENTS 6 – Materials

Proposed primary materials must reference the dominant material palette from the surrounding historic context.

Cues in the immediate area must influence the choice of materials. The tonal palette should be informed by the local, predominantly red brick tones.

Whilst some materials used for traditional buildings may not be appropriate for modern construction methods, materials must still make reference to traditional colours, texture, bonding and brickwork used within the context of the area.

#### Examples

The Empress Conservation Area is characterised by a material palette of

red brick and terracotta that provides a cohesive hue across the area and its varied uses.

Brick is laid in traditional bonds such as English, Flemish or English Garden Wall.

Materials such as terracotta and industrial metals contribute to the material palette, connecting with the area's industrial activity both past and present and enriching its visual texture.

Many industrial buildings have polychromatic or geometric detailing on their façades.



Examples of materials and details across the area.

## APARTMENTS 7 - Roof Materials and Details

Roof materials must be high quality and reference the surrounding context. All roofs, including flat roofs (where appropriate), must incorporate detailed parapets, soffits, eaves, verges and ridges.

The roof is often a dominant feature of a building and the shape, pitch, cladding and ornament are important. Look to the surrounding context for design cues when considering roof materials and details. Material colours, textures and patterns must be considered.

Overhanging eaves must be interpreted and integrated in contemporary designs.

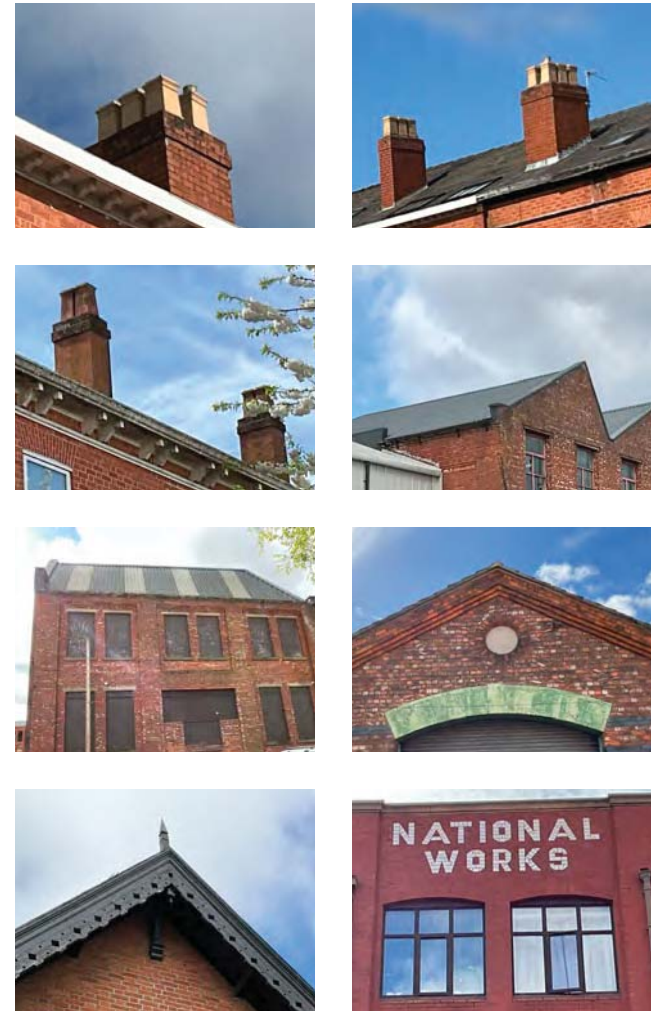
Traditional roofing materials primarily natural slate, are encouraged. Large format tiles, concrete tiles and artificial slate, and tiles with thick leading edges will not be permitted.

Where metal sheeting is considered appropriate this must be profiled or standing seam and of a high quality.

Flat roofs must include parapet detailing and add interest to the roofscape and overall appearance of the building.

### Examples

Roof slates should be Welsh or Westmorland, avoiding imported slates, which have a polished appearance. Where slates have been replaced with modern materials their reinstatement will be encouraged. Concrete tiles and artificial slates will not be permitted as they will erode the historic character. Brick chimney stacks in a variety of forms and styles are present, both domestic and industrial.



Examples of roof materials and details across the area.

## Boundaries Servicing and Plant

The impact of a site's boundaries on the immediate surroundings, and the way in which the building or buildings interact with the edges and ground around the site, must be an integral part of the design and considered at the outset.

### APARTMENTS 8 – Boundaries

Existing traditional boundary treatments must always be retained and new openings kept to a minimum. Consistent and attractive boundary treatments help to tie an area together and help to clearly define the public and private realm. The inclusion of soft landscaping increases biodiversity and adds to the beauty of the street and garden scene, often screening vehicle parking and adding value and a sense of stewardship to a development.

Boundary treatments should be informed by high quality traditional examples in the surrounding area. In Trafford this will typically be a low brick or stone walls with a hedge behind.

Public-facing boundaries must be constructed from either brick or stone walls and must incorporate soft landscaping.

Boundary treatments must be used to clearly define the public and private domain.

Strong building lines contribute to the character of the residential areas, particularly along Chester Road, by creating a rhythm and order to streets where no single building stands out (projects).

#### Examples

The building line to Chester Road, where most residential buildings are located, is strong. This creates cohesive and generally enclosed sweeps of domestic and commercial forms, reinforced by low brick and stone boundary walls and their railings.

The surviving terraces demonstrate a good degree of architectural detailing and historically incorporated gardens to the front and rear.

Historic boundary treatments must be retained and new openings kept to a minimum.

Walls and associated planting must be retained and enhanced where required. The retention of historic boundary treatments is important to ensure local distinctiveness and protect the character of the street scene.

#### Examples

Whilst some boundary walls have been removed or shortened to allow for off-road parking these generally remain a prominent feature of the conservation area, with a mixture of brick and stone topped with railings. Some walls show evidence of renewal or rebuilding and railings are generally modern.

Gate piers were not a feature of all residential properties, and have been lost at some, but are, present in full at Darwen, Birkdale and Cornbrook Terraces.

## APARTMENTS 9 – Gates

Gate piers and gates must complement the boundary treatment and, reflect the surrounding context in both design and height.

Gates must be side-hung with apertures in the top half to allow visibility in order to enhance natural surveillance. Sliding gates must be avoided as they reduce the ability to adequately landscape a site.

### Examples

The Essence Factory retains its historic boundary wall with iron gates.

The residential terraces have a variety of domestic-style gates to their frontages, although no original examples survive. These gates are most appropriate where they are good quality and domestic in character, match the adjacent railings, are of an appropriate height for the opening, complement the gate piers (if existing) and allow visibility into the plot.



Essence Factory iron gates.



Boundary wall of Birkdale Terrace.



Boundary wall of Essence Factory.



Precedent- Pocket Living by Gort Scott, soft landscaping.

## 3.4 Commercial

### Context

All developments need to draw inspiration from the context in which the site sits, whether that is an industrial park or a town centre. New commercial and industrial developments should enhance the townscape and protect the setting of existing heritage assets.

### COMMERCIAL/INDUSTRIAL 1 – Context

A context character appraisal must be carried out at the outset to establish how the proposed development reflects and enhances the character of the area.

Understanding the setting of the area enables an appreciation of its key built and urban forms and reinforces the legibility of its historic growth as an industrial area and links it to the wider pattern of growth in the area. Proposals on industrial sites, depending on the context can provide the opportunity for more innovative forms and elevation treatment- in historic areas the proposals must respect the established historic character of the area.

### Examples

Key landmark industrial buildings within the area include: the former Empress Brewery, now an active business centre, as well as the Essence Factory, Veno Building, Former Trafford Press offices and Empress Mill.



*Empress Building*



*Veno Building*

## Form and Profile

*The scale and form of new buildings must respect that of the surrounding site context. Some large-scale commercial and industrial buildings tend to appear non-descript and therefore careful thought is needed in relation to the form, profile and external appearance to create interesting buildings and improve place making.*

### COMMERCIAL/INDUSTRIAL 2 – Scale, Form and Profile

A context character appraisal must be carried out at the outset to establish the suitability of the site for commercial and industrial developments. Scale and form must respect that of the surrounding site context. The context of each site is unique, and it is important that the design and siting of any new development is informed by an analysis of the existing site context, history and character. Developments within existing places will be required to reflect the established scale and form of the surrounding context to strengthen the visual character of existing places.

Opportunities to replace the more ad-hoc or poor quality modern industrial buildings with new interventions which give a more consistent street frontage, framing views and reinstating a tighter grain are encouraged. Considered design should ensure that any new development is sympathetic to the historic industrial buildings and does not obscure them from view. Elements and features of historic industrial buildings and their sites that evidence their industrial origins and character should, wherever possible, be retained and preserved.

#### Examples

Scale and form vary in the historic context of industrial and commercial buildings. This ranges from the historic character of the smaller two-storey works and warehouses that were the first industrial buildings to emerge in the area to larger warehouses and landmark industrial buildings. Typical roof forms include:

- Staggered rooflines
- Gable ends
- Sawtooth or 'northlight' roofs



*Sawtooth roof at Former Empress Machine Tool Works*



### COMMERCIAL/INDUSTRIAL 3 – Façade Design

Proposals for commercial and industrial developments must provide elevation designs which incorporate architectural articulation in a way which respects the surrounding context of the area. Principal elevations must provide active frontages on the ground floor. Elements and features of historic industrial buildings and their sites that evidence their industrial origins and character should, wherever possible, be retained and preserved. The addition of metal cladding to historic brickwork is not encouraged. Textured rendering may be more suitable where the condition or appearance of the fabric is very poor, but walls should remain exposed wherever possible. Main characteristics of the area which must be considered include:

- symmetrical façade
- central entrance
- vertical fenestration



*Symmetrical facade at Trafford Press*

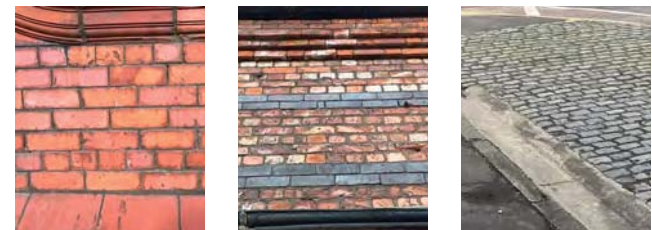
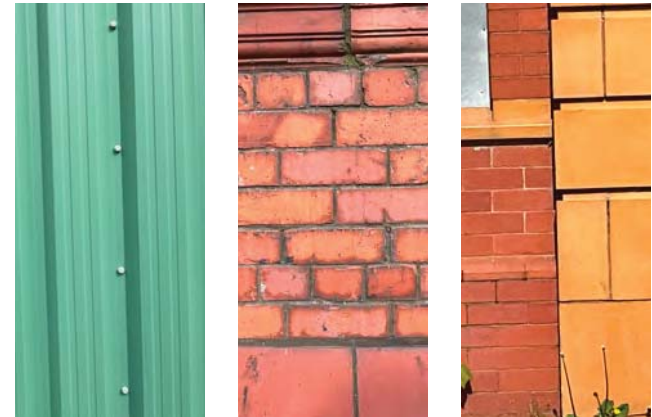
## Materials and details

*The materials used for a building affect how well it functions and lasts over time. They also influence how it relates to what is around and how it is experienced.*

### COMMERCIAL/INDUSTRIAL 4 – Materials

The proposed materials for developments must reference common materials from the surrounding context and incorporate these into elevations as the primary material. Whilst some materials used for traditional buildings may not be appropriate for modern construction methods, materials must still refer to traditional colours, texture, bonding and brickwork used within the context of the area. Materials such as terracotta and industrial metals contribute to the material palette, connecting with the area's industrial activity both past and present and enriching its visual texture. Many industrial buildings have polychromatic or geometric detailing on their façades.

Roof materials must be high quality and reference the surrounding context. All roofs, including flat roofs (where appropriate), must incorporate detailed parapets, soffits, eaves, verges and ridges. The roof is often a dominant feature of a building, and the shape, pitch, cladding and ornament are important. Look to the surrounding context for design cues when considering roof materials and details. Traditional roofing materials primarily natural slate are encouraged. Large format tiles, concrete tiles and artificial slate, and tiles with thick leading edges will not be permitted. Where metal sheeting is considered, appropriate this must be profiled or standing seam and of a high quality. Flat roofs must include parapet detailing and add interest to the roofscape and overall appearance of the building. Examples Roof slates should be Welsh or Westmorland, avoiding imported slates, which have a polished appearance. Where slates have been replaced with modern materials their reinstatement will be encouraged. Concrete tiles and artificial slates will not be permitted as they will erode the historic character.



Examples of materials and details across the area

## Boundary Treatments

*Well-designed places clearly define the boundaries for private, shared and public spaces, making it more likely that occupants will use, value and take ownership of them.*

### COMMERCIAL/INDUSTRIAL 5 – Boundary Treatments

Development proposals should maximise every opportunity to incorporate planting into streets and spaces to ensure the development overall is nature-rich. Consistent and attractive boundary treatments help to tie an area together and help to clearly define the public and private realm. The inclusion of soft landscaping increases biodiversity and adds to the beauty of the street and garden scene, often screening vehicle parking and adding value and a sense of stewardship to a development. Boundary treatments should be informed by high quality traditional examples in the surrounding area, typically low brick or stone walls with a hedge behind. Public-facing boundaries must be constructed from either brick or stone walls and must incorporate soft

landscaping. Historic features such as steps and surfaces must be retained; their reinstatement where previously removed is encouraged. Gate piers and gates must complement the boundary treatment and reflect the surrounding context in both design and height. Gates must be side hung with apertures in the top half to allow visibility to enhance natural surveillance. Sliding gates must be avoided as they reduce the ability to adequately landscape a site.



*Examples of boundary treatments in the area*

## COMMERCIAL/INDUSTRIAL 6 – Historic Boundary Treatments

Existing traditional boundary treatments must always be retained and new openings kept to a minimum. The retention of historic boundary treatments is important to ensure local distinctiveness and protect the character of the street scene.

### Examples

Whilst some boundary walls have been removed or shortened to allow for off-road parking these generally remain a prominent feature of the conservation area, with a mixture of brick and stone topped with railings. Some walls show evidence of renewal or rebuilding, and railings are generally modern. The Essence Factory retains its historic boundary wall with iron gates. The residential terraces have a variety of domestic-style gates to their frontages, although no original examples survive. These gates are

most appropriate where they are good quality and domestic in character, match the adjacent railings, are of an appropriate height for the opening, complement the gate piers (if existing) and allow visibility into the plot.



*Examples of boundary treatments in the area*

## 3.5 Streets and Public Realm

### Future Vision for Empress: A low car and healthy neighbourhood

Empress is unique in its location and area's character and offers opportunities to become a low-car neighbourhood, where vehicular traffic has been significantly reduced. This would form a catalyst for future development in Trafford to fundamentally reimagine how people and goods move around. The focus of streets is on active mobility, placemaking, and linking into the well-connected public transport network.

The key objectives for the mobility network are:

- Shifting from private vehicles to cleaner modes that take up less space can reduce air pollution and carbon emissions.
- Repurpose road space away from parked cars and vehicle traffic for more planting and trees, creating space for socialising, play, walking, and cycling.
- Enable people to live more actively by making active travel the natural choice for most journeys.

The overall vision is for a healthy and active neighbourhood where walking, cycling and the use of public transport must be the first choice for movement. It is anticipated that all development proposals will promote and support this.

The illustrative masterplan follows the 'SPSD 1: Active travel and street hierarchy' in the Trafford Design Code, and Greater Manchester's Streets for All Design Guide to provide an attractive public realm and seek to maximise active travel and opportunities for communities to meet and children to play.

### Mobility Strategy

The proposed movement strategy reimagines the roads, streets and paths as a mobility network which prioritises walking, cycling, public transport and shared mobility.

To deliver this and building on the Trafford Design Code, the movement strategy for Empress includes:

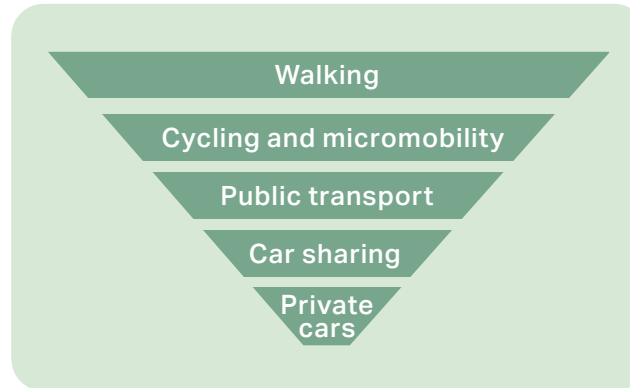
1. A connected network of streets to maintain pedestrian and cycle permeability and easy of movement across the area.
2. Separation between larger goods vehicles to service the industrial uses on the area and the proposed residential area which emphasises pedestrian and cycle priority.
3. Raised tables and other traffic measures in residential streets to slow down the speed of vehicles and improve crossing areas.
4. Improvements to Chester Road junction to create a protected intersection prioritising cyclists over vehicles.
5. Improved connectivity and way-finding to existing public transport stops.
6. Well-considered parking, servicing, and cycle parking provision.

### 3.5.1 – Mobility Strategy *(Read in conjunction with SPSD1 Active travel and street hierarchy)*

Development proposals should use a 'Vision and Validate' approach to the assessment of traffic impact and to inform their mobility strategy. This approach should be used to rebalance developer investment in highways infrastructure towards social, green and sustainable infrastructure supporting well designed places.

Developments should adopt the following key principles:













- Consider the street typologies they adjoin and provide evidence of how their development should address them to support the area's placemaking and movement strategy.
- Improve connections to Cornbrook and Trafford Bar tram stops by making pavements accessible and improve wayfinding.
- Consider the balance between industrial and residential movement when designing streets and junctions whilst following the priority movement hierarchy.

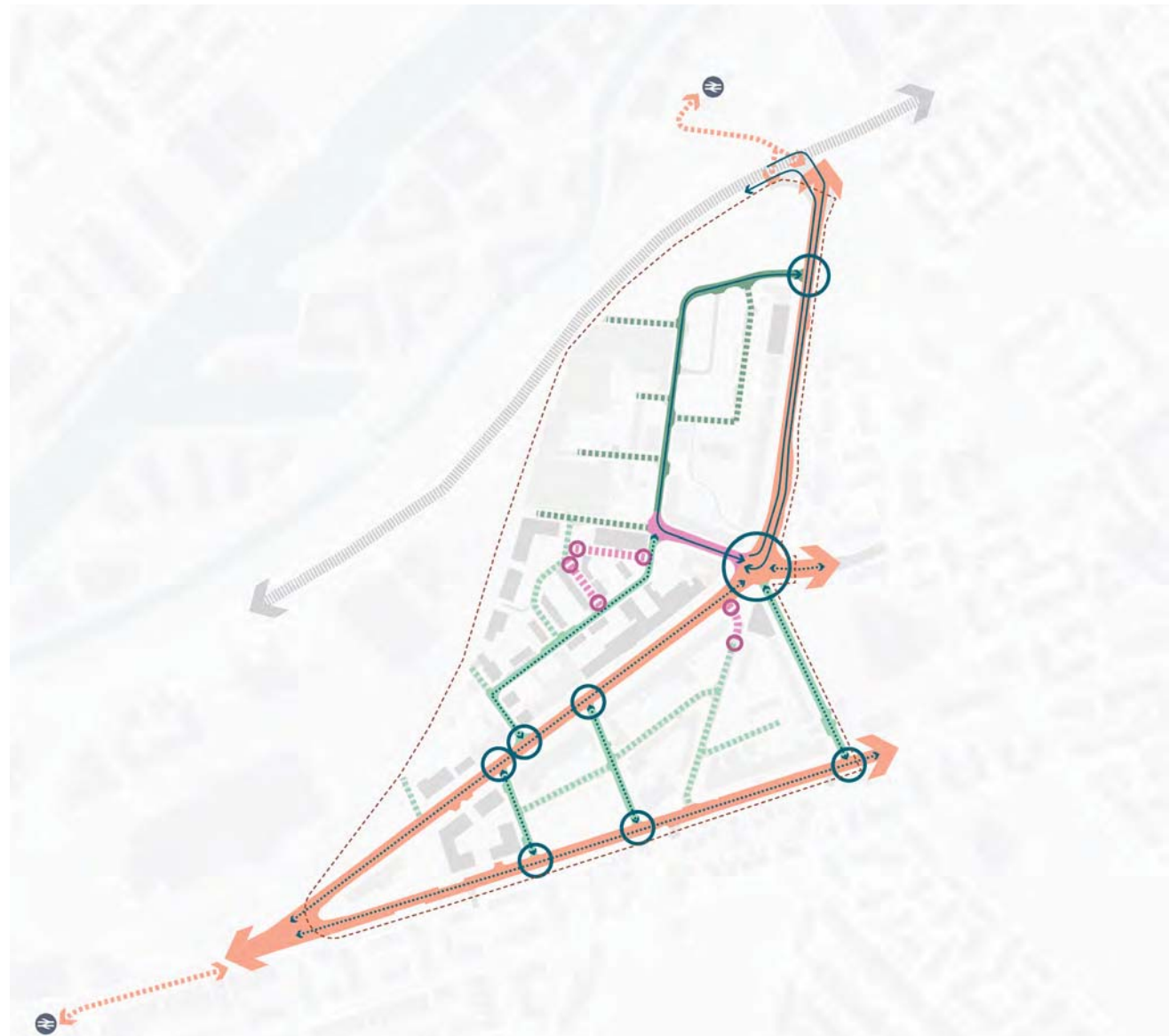


### 3.5.2 – Mobility Strategy Plan *(Read in conjunction with SPSD2 Safe Streets and attractive public realm, and SPPR6 Desire lines)*

The mobility strategy plan sets out the street hierarchy and movement principles for the area. Proposals should follow the principles of the plan and improve connections to Trafford Bar and Cornbrook tram stops. Details of functions and design of streets are set out within the street typologies section. The street framework should create a legible network to maintain pedestrian and cycle permeability, ease of movement across the site, and promote safe streets.

#### Key

-  Site boundary
-  Connector Streets (Primary) (3.1.7)
-  Arrival Street (Secondary) (3.1.8)
-  Industrial Streets (Secondary) (3.1.9)
-  Industrial Access Streets (Tertiary) (3.1.10)
-  Local Residential Streets (Secondary) (3.1.11)
-  Residential Streets (Tertiary) (3.1.12)
-  Green Lanes (3.1.13)
-  Heavy goods vehicle movement
-  Other key vehicle movement routes
-  Key vehicle, cycle and pedestrian access
-  Cycle and pedestrian access



3.4.3 Mobility strategy diagram - defines the key connections through the site. The design principles of these streets are set out in the Trafford Design Code and Greater Manchester's Streets for All Design Guide.

**3.5.3 – General requirements and future proofing** *(Read in conjunction with SP3D3 Futureproofing)*

All streets and paths must be designed with priority for active travel and to be pleasant and safe for people walking, wheeling and cycling. This includes clear sightlines, clean air, space for planting and seating and being well-overlooked by surrounding buildings with frequent windows and entrances. Secondary and Tertiary streets should be designed to accommodate speeds no greater than 20mph.

Proposals must adopt an inclusive approach which considers the needs of vulnerable users from the outset, ensuring that everyone regardless of age and ability can easily get around. This involves providing protected road space where required and step free access where possible.

Cycling must be taken into account in the design of all new and improved streets and junctions. Junctions and crossings should enable cyclists to negotiate them in comfort without undue delay or deviation.

Views should be carefully considered to ensure street fixtures and signage does not detract from the appearance of important buildings, limit active frontages or disrupt access to the ground floor.

All streets must facilitate movement towards Chester Road and onto nearby transport hubs (Cornbrook and Trafford Bar) and nearby local parks (particularly Seymour Park and Hullard Park).



### Chester Road Junction

Improvements to Chester Road junction to improve crossings for pedestrians, give priority for cyclists, and slow vehicle speeds through the use of planting. See illustrative diagrams on the right.

#### 3.5.4 – Chester Road Junction

Junctions should be designed in accordance with Manual for Streets, TfGM's Streets for All strategy and the Trafford Design Code to be safe, convenient, and attractive for all users, aligned with desire lines, and should promote active travel. Junction upgrades must be used to unlock public realm and landscape potential for pedestrians and cyclists.

#### 3.5.5 – Street trees and SuDS *(Read in conjunction with SPSS4 Street trees, SuDS and landscaping).*

Street trees, planting, and bioswales should be considered throughout the site as part of the Sustainable Urban Drainage System (SuDS). Chester Road must include increased bioswales. The central spine (secondary streets) through the site should consist of linear rows of tree planting to frame the street scene and aid wayfinding. Tertiary streets must include increased bioswales and a range of street tree planting. Green lanes must contain a higher density of tree planting and consist of a diverse range of tree species.

Street tree planting should be part of a comprehensive landscape approach and not visually obstruct important views of historical buildings or linear views across the site, not obstruct footways and access for disabled users, and not obstruct the function or heavy goods vehicle access requirements for existing businesses and industrial uses.



Vision of a protected intersection to improve the safety of pedestrians and cyclists. Note: the diagram is for illustrative purposes only.









Existing Chester Road junction.

## Car Parking

Parking can have a significant impact on the function and appearance in street scenes, especially within a conservation area. The aspiration for the area is to reduce the visual dominance of parked cars and other vehicles on the public realm and design out inconsiderate parking which affects streets in and around developments, as set out in the Trafford Design Code.



Indicative car parking strategy, excluding existing on-plot parking areas

-  No parallel on-street parking
-  Limited on-street parking, focus on parallel parking for disabled users
-  Defined on-street parking in residential streets
-  Unallocated on-street parking in commercial / industrial streets
-  On-plot parking for residential plots
-  Commercial / industrial on-plot parking

The use of shared car-clubs is encouraged to reduce parking provision related to development. Disabled and car-club parking spaces should be prioritised and co-located with entrances.

### 3.5.6 – Car parking *(Read in conjunction with SPSP5 On-street car parking)*

Provision for on-street car parking should comply with the Trafford Design Code, TfGM's Streets for All strategy and Greater Manchester's Streets for All Design Guide. Allocated parking provision must be minimised and consolidated to keep primary and secondary streets predominantly free from parked cars and ensure sustainable modes are the most attractive choice. Suitable parking quantum will vary across the area. Accessible parking should be provided in accordance with the Trafford Design Code and SPD3.



Example of reduced on street parking and screened by planting in Great Kneighton, Cambridge



Existing site condition with cars parked on pavements

### **3.5.6 – Car parking - continued**

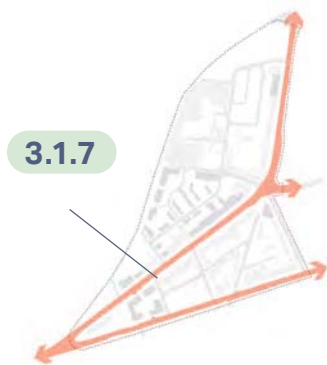
The location of car parking should be carefully considered to ensure it does not detract from the appearance of buildings of historical value and limit active frontages or disrupt access to the ground floor. To improve the visual appearance of parking areas, developers should incorporate landscape proposals that are appropriate to the site and parking layout.

Electrical Vehicle (EV) charging points should be prominently located across the site in accessible locations to promote the use of electric cars but not obstruct people's movement or contribute to cluttering of streets. The design of individual units should be selected to have minimal impact on the townscape character of the street.

## Street Types

The following street typologies proposed are adapted from the Trafford Design Code, Greater Manchester's Streets for All Design Guide, and the street approaches set out in the CIHT 'Creating Better Streets: Inclusive and Accessible Places' guidance. The proposed street typologies include:

1. Connector Streets (Primary)
2. Industrial Streets (Secondary)
3. Industrial Access Streets (Tertiary)
4. Local Residential Streets (Secondary)
5. Residential Streets (Tertiary)
6. Green Lanes (Pedestrian Prioritised Streets)



Location diagram for Connector Streets

The following pages outline the key design considerations for each street typology and applicants are advised to carefully consider the types of street they are addressing and/or creating within their development plots.

### 3.5.7 – Connector Street (Primary)

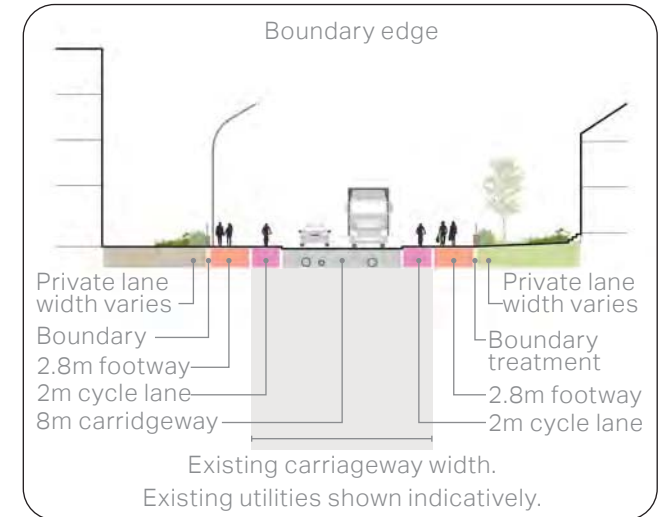
The focus of this street should be on accommodating a good balance between pedestrians, cyclists and vehicles, and comply with the Trafford Design Code and Greater Manchester's Streets for All Design Guide.

Separation between pedestrians and cyclists using low splayed kerb and guidance from TfGM's Streets for All Design Guide should be used for cycle ways.

Adequate lighting is required to allow safe vehicular, pedestrian and cycle movement.

Bus stands, bins and seating to be accommodated carefully to avoid clutter.

Strong boundary edge with low brick or stone wall and steel fence should be maintained to follow the local character of the street scene.



Typical cross section: A06 – Connector Streets (Primary)



Example of a segregated cycle lane along Oxford Road in Manchester

### 3.5.8 – Empress Street (Secondary)

Empress Street forms the arrival into the site from Chester Road. The street should be welcoming in character and retain clear visual views towards historic assets.

The design of the street should comply and be read in conjunction with the Trafford Design Code and Greater Manchester’s Streets for All Design Guide.

### 3.5.9 – Industrial Streets (Secondary) and 3.5.10 – Industrial Access Streets (Tertiary)

These streets will accommodate the movement of Larger Goods Vehicles (LGV) and moderate levels of traffic at slow speeds to access existing industrial uses within the site. The balance between servicing of businesses and residents on local streets should be carefully considered.

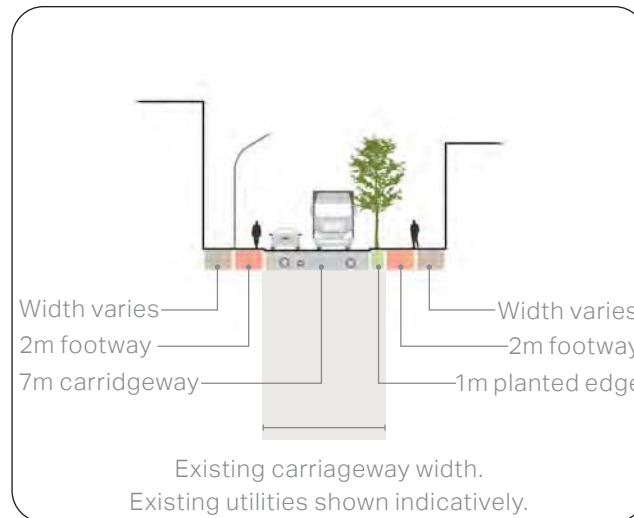
Pedestrian and cycle movement should be prioritised and promoted where possible.

On-street parking should be controlled with dedicated parking spaces and include parking for disabled use where street dimensions allow for it. Street lighting should be designed to reflect the local character with appropriate lighting levels for the proposed residential plots and neighbouring uses.

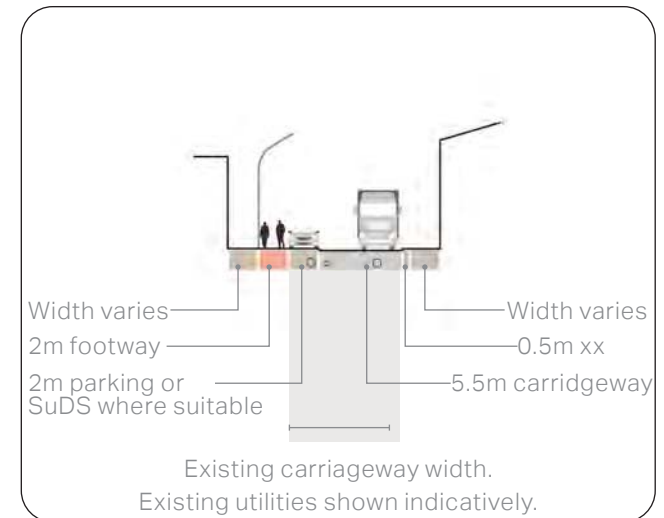
The design of the street should comply and be read in conjunction with the Trafford Design Code and Greater Manchester’s Streets for All Design Guide.



Location diagram for industrial secondary and tertiary streets



Typical cross section: A07 – Industrial Streets (Secondary)



Typical cross section: A08 – Industrial Access Streets (Tertiary)

### 3.5.11 – Local Residential Streets (Secondary)

These streets allow for low levels of traffic at slow speeds to access residential uses, and access for refuse, delivery and emergency vehicles. They do not allow for Large Goods Vehicle (LGV) for industrial use.

Raised tables are incorporated at junctions to prioritise high-quality pedestrian and cycling movement over cars, whilst helping to reduce vehicle speeds.

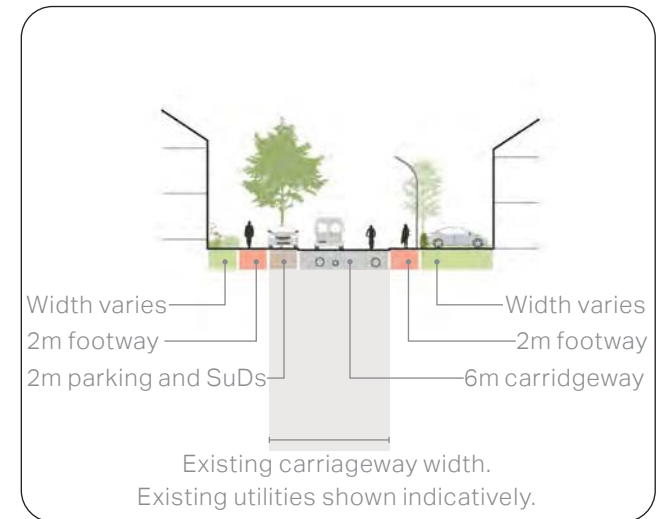
Streets should consider on street parking for disabled use only with the majority of parking provision on-plot.

Street lighting should be designed to reflect local character and be appropriate for residential uses.

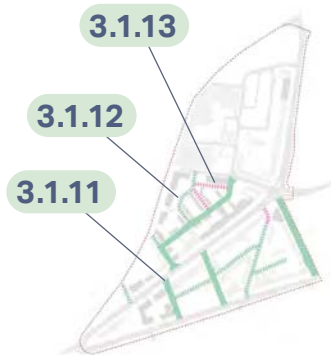
Where possible SuDS should be incorporated within the street scene.

The use of appropriate species of street trees should be considered to enhance the landscape where space and other constraints allow.

The design of the street should comply and be read in conjunction with the Trafford Design Code and Greater Manchester's Streets for All Design Guide.



Typical cross section: A09 – Local Residential Streets (Secondary)



Location diagram for Residential Streets



Example of large tarmac surface area and unallocated parking spaces, at Empress, Trafford



Example of local residential street in Accordia, Cambridge

### 3.5.12 – Residential Streets (Tertiary)

These streets are designed primarily for the pedestrian to deliver a pleasant, safe and legible experience for all users.

Vehicle access is limited to accessing driveways and parking. Streets can be made available for service and other priority access vehicles but only for limited periods during the day.

Sufficient protection at entrances to the street should be considered through careful landscape considerations, however the use of large areas of bollards should be avoided.

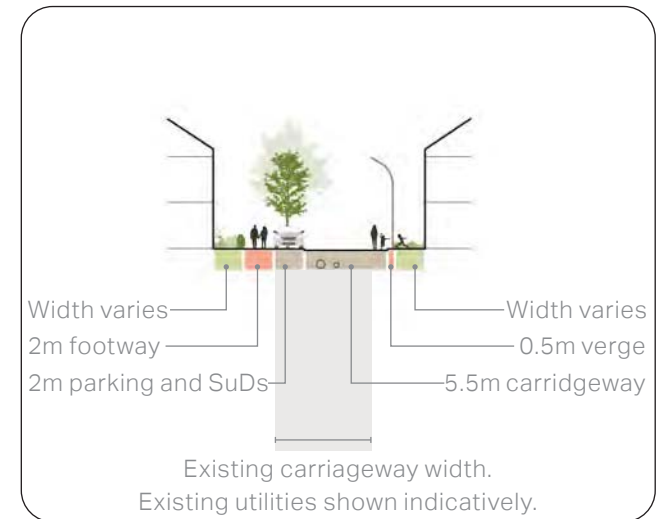
Fixed seating should be incorporated into the street, combined within planters is preferred to reduce clutter.

Wayfinding and interpretation should be carefully incorporated at key junctions and entrances, being careful to avoid clutter.

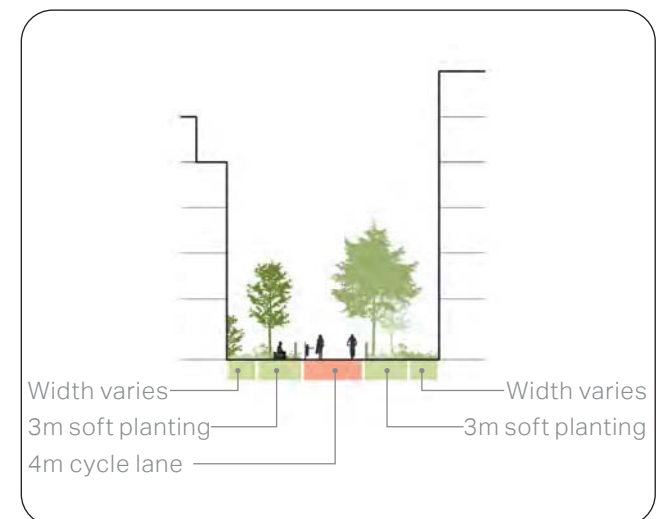
Where possible SuDS should be incorporated within the street scene.

The use of appropriate species of street trees should be considered to enhance the landscape where space and other constraints allow.

The design of the street should comply and be read in conjunction with the Trafford Design Code and Greater Manchester's Streets for All Design Guide.



Typical cross section: A10 – Residential Streets (Tertiary)



Typical cross section: A11 – Green Lanes

### 3.5.13 – Green Lanes

The focus of these multifunctional lanes should be on the pedestrian and cyclist experience.

Where necessary for servicing or access, vehicular movement should be controlled and limited at to promote pedestrian safety.

SuDS and landscaping should be incorporated throughout the street, including the use of appropriate species of trees where constraints allow.

Opportunities should be maximised for places to sit and dwell and accommodate spill out space from commercially active frontages.

Low level accent lighting and task lighting should be prioritised over highways lighting.



Example of pedestrian links at Battersea, London



### **Future Vision for Empress: A place to live, work, create and engage**

Empress is a mixed-use area with a rich history as a live-work environment. The place-making strategy aims to embrace its independent and creative spirit with high-quality sustainable homes, new shops, cafés, and bars. The goal is to create an engaged community that welcomes people who live and visit the area, with historic assets becoming landmarks that define an exciting and characterful place.

The public open spaces will align with this vision by creating a string of interlinked public open spaces through the area to improve way-finding and enable a diverse range of activities to happen in pocket parks, communal spaces, play areas, a meanwhile use space, and spill-out spaces from commercial uses.

### **Sense of place**

'A sense of place is the quality that makes a place special and lodges it in the memory so that people want to stay or return' - National Model Design Code.

Whilst all elements of this code work together to create a strong sense of place, this section sets out specific requirements to create a coherent identity for the area through the public space strategy. Empress has experienced a loss of greenery and deterioration in its landmark buildings due to the conversion of residential buildings for commercial uses, extension and subdivision into apartments, as well as stalled development resulting in a legacy of decline.

The Code describes how the area should be planned to feel vibrant again and streets and public spaces designed to add to the identity of the area.

### **3.5.14 – Character and cultural identity**

Applicants must demonstrate a comprehensive understanding of the historic and cultural identity of the local area including (but not limited to) the area's mixed use heritage and its current role and function as a place to live, work, create and engage.



Example of street art in Fish Island, London

### 3.5.15 – Open Space

Open space should reflect the area's industrial past, forming workers' yards, residential yards, and communal garden spaces in accordance with 3.1.16 Public Open Space Plan, the Fields in Trust benchmark guidelines and Trafford Design Code.

All new open space should:

- Respond to the area's industrial past.
- Be integrated into the wider pedestrian/cycle network and green/blue infrastructure network.
- Be well overlooked with frequent windows and building entrances onto the spaces.
- Be high-quality, using planting, materials and furniture that will age and weather well.
- Connect into the green and blue infrastructure network set out in section 3.2. Nature.

The material palette, furniture and planting should be coordinated across the whole area to ensure the public realm brings the development together and forms part of the identity of the area by celebrating it's history.

A strategy for stewardship and ongoing management of spaces should be developed and agreed with the Council at an early stage. Development proposals will make provision for the ongoing management and maintenance of open space to standards that have been agreed with the Council.









Stewardship models should ensure that residents have a say in how and who manages the spaces they pay a service charge for.

### 3.5.16 – Public Open Space Plan

The plan sets out the public open space typologies and a network of essential pedestrian routes through the site. It builds on the placemaking strategy to encourage permeability through the site, create legible public open spaces that respond to the various uses, and create a place for communities to thrive. It aligns with the green and blue infrastructure strategy to connect people with nature and improve local biodiversity.

Proposals should follow the principles of the plan. Adjacent plots and development proposals should seek to respond to and connect with the principles of the plan.

#### Key

-  Site boundary
-  Key junctions (P05)
-  Residential street raised tables (P06)
-  Public open space enhancement
-  Public open space within private plots
-  Key streets for pedestrian improvement
-  Key pedestrian movement
-  Pedestrian movement on private land



The public open space strategy defines the key typologies and network of open spaces across the site.

### 3.5.17 – Safety and Security *(Read in conjunction with SPPR1 Safety and Security)*

All new development must provide active frontages such as doors and windows and/or lively internal uses visible from the outside, or spilling onto the street and public realm to provide natural surveillance and mitigate any antisocial behaviour.

Sufficient lighting levels should be provided across the site. Pocket parks, communal parks and pedestrian green lanes should primarily use low lumen bollard down lighters or strip lighting supported by pedestrian light poles and overhead lights, whilst highway lighting should only be provided along adopted streets and near the industrial units. Street columns must be set back from the carriageway but not obstruct accessibility or be positioned in front of dwellings.



Example of existing street lighting columns.



Example of raised table crossing from Accordia Cambridge

### 3.5.18 – Wayfinding and legibility

*(Read in conjunction with SPPR3 Wayfinding and legibility; SPPR6 Desire lines; and SPPR8 Accessibility)*

Public open spaces should encourage permeability through the site by retaining clear visual views and providing consistent materials for accessibility (see figure 00). Railings on pavements must be avoided. Other street fixtures should be carefully considered to avoid pedestrian obstructions and allow the full width of the pavement to be used for accessibility.



Example of low level lighting for pocket parks, from Whitfield Gardens



Example of obstructive railings on pavements, from Empress, Trafford

### 3.5.19 – Key Junctions

Junctions should be designed in accordance with Manual for Streets and Streets for All to be safe, convenient, and attractive for all users, aligned with desire lines, and should promote active travel.

The junctions along Chester Road should be coordinated, ensure the public realm brings the development together through the use of a consistent design, and form part of the identity of the site by celebrating it's history. Residential streets should include raised tables at junctions or intersections to reduce vehicle speeds and promote pedestrian priority. The change in materials should consider the context of the conservation area and compliment important buildings of historical value.



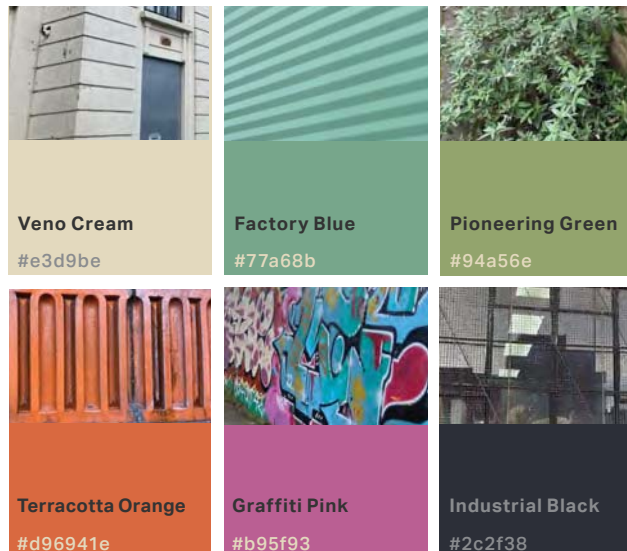
Example of local residential street from Accordia, Cambridge

Examples of local material colours from the Empress Conservation Area include oranges, creams, blues, and blacks from listed buildings marking the colours of the past industry, and from the evolving use and activity, bright pinks from graffiti and greens from pioneering plants reclaiming spaces.

### 3.5.20 – Materiality *(Read in conjunction with SPPR7 Materials)*

Materials used in areas of public realm must be high quality, durable, complement the local context, and consistent throughout the site to enhance the visual appeal and to effectively guide people, aiding in legibility and wayfinding.

New developments should consider prominent colours and textures of the Empress Conservation Area when choosing their materials.



**3.5.21 – Street Furniture** *(Read in conjunction with SPPR4 Street furniture and SPPR5 Public art)*

Seating and resting spaces must be provided along key pedestrian routes at intervals of no more than 50 metres apart within the public realm of private development plots and adopted streets. Seating should include benches with back support and arm rests for elderly or mobility impaired users, but it can include leaning rails where space is limited.

Street furniture design and finishes should consider the local character, conservation area, and any existing street fixtures such as lighting columns and bollards painted in a black gloss coat (BS4800), with two reflective gold bands and gold characters.

Public art and signage relating to the areas history, use and character should be incorporated into the public realm design to support wayfinding and celebrating the history of the area.



Example of bespoke signage from Empress Mill



Example of seating with back support and arm rest

**3.5.22 – Play and recreation** *(Read in conjunction with SPPR4 Street furniture and SPPR9 Play and recreation)*

Where local areas for play (LAPs) are required they should be integrated carefully to avoid impacting or detracting from local buildings of historic value.

Incidental play areas should also be provided within public open spaces within or adjacent to residential development plots. It can include: pocket parks; communal gardens; and car-free streets. Proposed play equipment should respect and respond to the conservation area.



Example of signage in pavement



Example of incidental play integrated with seating in a pocket park



### 3.5.23 – Chester Road Linear Space

The linear public open space should be designed to encourage active use by making it more accessible and by providing suitable street furniture.

The park must include informal play with areas for seating and socialising, to create an active use of the space.

Existing trees and vegetation of value should be retained to actively manage, maintain and enhance biodiversity.

New trees and planting should be delivered where possible to improve biodiversity, encourage use, and support traffic calming measures. Proposed planting should be robust and resilient suitable for road side conditions and for ease of maintenance. To allow for natural surveillance, planting should be maintained and not obstruct views.

Low level accent lighting and task lighting should be prioritised over highways lighting.

### 3.5.24 – Northumberland Road Pocket Park

The new pocket park within the existing housing site should be designed with the local community and form an active space for the community.

The space must include informal play spaces, areas for food growing, robust planting with seasonal variety, and a pedestrian through route to provide better access from Northumberland Road to the terraced houses along Blackley Street.



Example of a linear park space with nature play at Maidenhead



Example of community food growing spaces in a residential area, Great Kneighton, Cambridge



Example of seating and informal play, in Great Kneighton, Cambridge

## 3.6 Landscape and Nature

### Future Vision for Empress: A sustainable neighbourhood

Empress offers opportunities to create a rich ecosystem by bringing nature into the heart of the area and improving the biodiversity value in an urban area. The public and communal open spaces, interwoven with green and blue infrastructure, form a network across the area that extends into the existing habitats around the perimeter. These spaces allow residents to enjoy the view of lush planting and trees from their homes and provide direct access to outdoor spaces that promote community well-being and encourage children to play with and learn from nature.

The industrial and commercial parts of the area allow for street planting and sustainable urban drainage systems, extending the network of green spaces. Street trees will be carefully positioned to retain important views to buildings of historical value and interest, and allow the movement of heavy goods vehicles through the area.

### 3.6.1 – Green and Blue Infrastructure

Proposals should provide a nature-rich environment with plenty of opportunities for people to have daily contact with nature, from climbers on buildings and small doorstep places, to improved connections to surrounding parks. Hard surfaces should be kept to a minimum and have a clear function such as for movement or space for events.

All development proposals should submit a plot based Green and Blue Infrastructure Plan setting out how the development will link to existing green and blue infrastructure (including the proposed open spaces on the Empress site, existing green assets, promoting connectivity to Bridgewater Way Canal) and demonstrating how the development will contribute to the delivery of Biodiversity Net Gain enhancements. Where necessary, contributions to enhance green infrastructure, sustainable transport links and gateway access points will be sought.

A clear maintenance regime for planting should be put in place for green and blue infrastructure.



Example of planted bioswales as part of SuDS

### 3.6.2 – Biodiversity and ecology

*(Read in conjunction with national and local biodiversity net gain guidance and LNL5 Biodiversity)*

Existing habitats on site must be protected and enhanced. New development proposals should create and restore habitats that are suitable for the area and incorporate a range of native plant species, insect hotels, bird boxes and other suitable nest boxes and nesters.



Example of an insect hotel to attract butterflies, bees, and other important pollinators

### 3.6.3 – Green and Blue Infrastructure Plan

Proposals for green and blue infrastructure must seek to enhance connections between existing green infrastructure assets and proposed open green spaces. The adjacent strategy diagram illustrates the key typologies and connections that should be considered.

#### Key

-  Site boundary
-  Key public green spaces
-  Community green space
-  Existing enhanced public green spaces
-  Existing lineside vegetation
-  Existing trees
-  Streets with important green infrastructure role



3.5.3 Green and Blue Infrastructure Plan - defines the key typologies and connections that should be considered.

### 3.6.4 – Trees *(Read in conjunction with the Landscape and Nature chapter of the Trafford Design Code)*

Existing trees should generally be retained, and the removal of trees must be clearly justified and compensated for by planting new trees. A tree survey must be undertaken during early concept stage to ensure existing trees shape the design proposal from the outset and are well integrated into the design.

Development proposals are encouraged to increase the tree canopy cover by at least 10% when compared with the pre-development baseline. Trees should be planted in public open spaces and within residential streets. However, any trees planted within streets should be carefully located to not obstruct linear views or views towards historic assets and buildings of significant importance.

The size of trees and tree pits should be of a suitable size to ensure that they establish well and have future healthy growth.

See Trafford Design Code for further specification.

### 3.6.5 – Planting

Development proposals should maximise every opportunity to incorporate planting into streets and spaces to ensure the development overall is nature-rich.

Planting must be resilient to an urban environment and future climate change.

Planting should predominantly utilise species that directly benefit wildlife e.g. through nectar or berry production, or providing shelter and materials. Planting schemes should form ecological networks through the built environment and include native species which can be used by pollinators and native fauna.

Planting beds must be designed and sized to ensure an adequate growing medium for healthy and robust planting.

### 3.6.6 – Tramline embankment *(Read in conjunction with LNL2 Boundaries and Edges and LNL3 Protecting existing landscape features)*

Development proposals must protect and enhance the tramline embankment which is an important wildlife corridor.

This should include features such as planting along the boundaries to improve the visual appearance of the embankment and create links for wildlife.



Example of existing tramline embankment boundary fence



Example of mixed boundary planting and fencing

### 3.6.7 – Boundaries and Edges *(Read in conjunction with LNL2 Boundaries and Edges)*

Chester Road should retain a clearly defined and continuous front garden boundary wall at the back of the pavement, consisting of brick or stone with black-painted metal railings. Terraced residential units within the Empress Conservation Area should form a strong building line at the back of the pavement and limit front garden boundary walls.

Mill buildings and associated external boundary walls and fences of historical value should be retained and incorporated into the public realm design.

Tramline embankment boundary fencing should be replaced with suitable high quality fencing to screen the view and planted with vegetation to provide an appealing edge.



Example of continuous front garden boundary wall along Chester Road



Example of mill building wall with fence along Wright Street

### 3.6.8 – Sustainable Urban Drainage System *(Read in conjunction with LNL4 Drainage and SuDS)*

Natural drainage solutions should be incorporated to help reduce the speed of surface water runoff and discharge from site.

Bioswales must be provided along Chester Road, incorporated along the central spine, Green Links, and included in Tertiary streets.

Urban street tree pit systems must contribute to the underground sustainable urban drainage system.

Permeable paving should be provided for parking bays, footways, and hard surfaces in public open spaces.

### 3.6.9 – Gardens and small spaces *(Read in conjunction with LNL5 Biodiversity)*

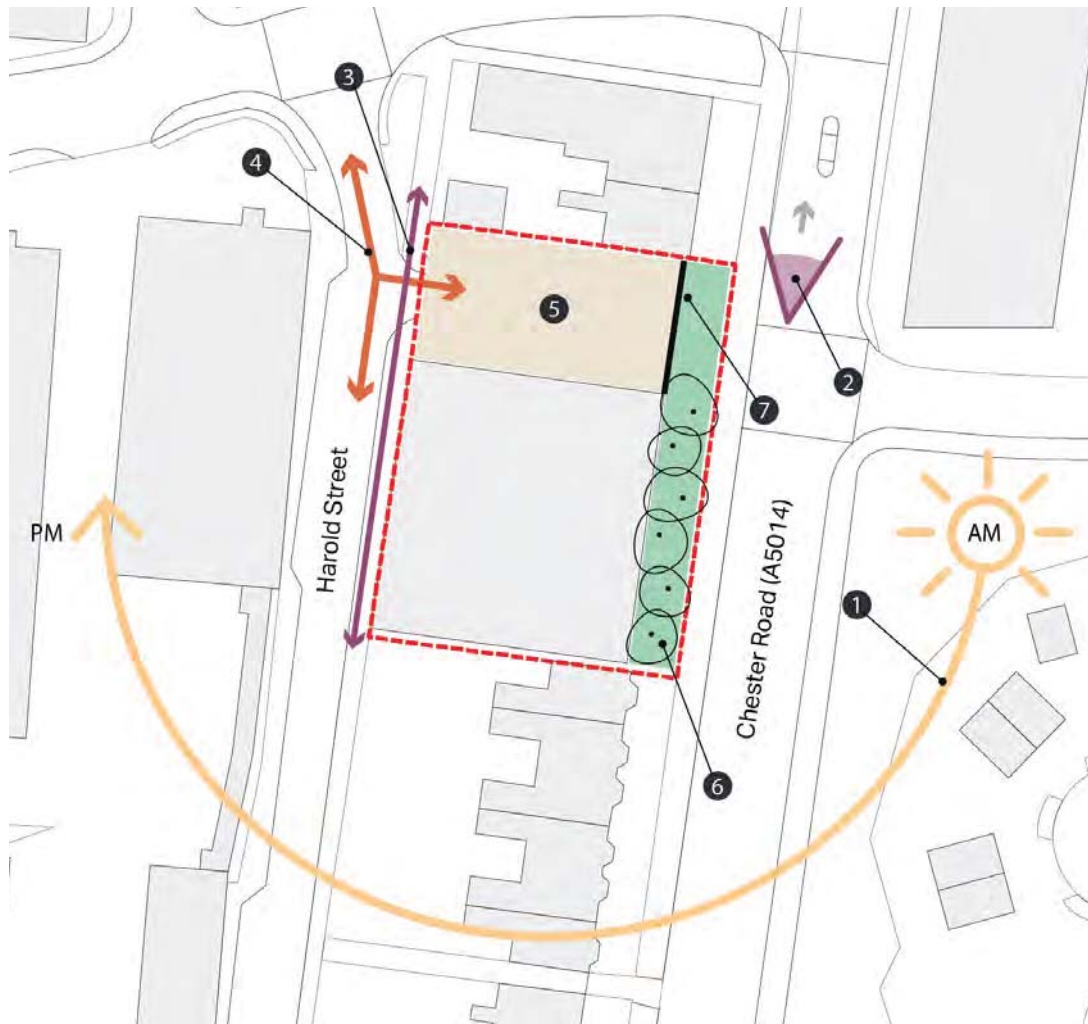
Courtyards, working yards, communal areas, play streets, and residential yards should seek to increase soft planting and include a diverse range of plant and tree species that improve ecological and biodiversity value and can contribute to local habitat creation.



#### 4. Plot Codes

## 4. Plot Codes

### 4.1.1 Plot Codes - Site 1 - Site Analysis



Key:

1. Sunpath.
2. Views towards city-centre high rise modern development.
3. Main pedestrian access.
4. Main vehicular access.
5. Open yard space.
6. Green space, blocking views and access.
7. Advertisement board blocking views and access.



Chester Road, towards the south.



Chester Road, towards the north.



Harold Street.

Site 1 is situated to the west of Chester Road, in between the Cornbrook and Birkdale Terraces which are locally listed. The site is used for commercial purposes and is occupied by a warehouse which is accessed via Harold Street.



#### 4.1.1 Plot Codes - Site 1 - Site Analysis



Existing Building Heights.



Aerial View.

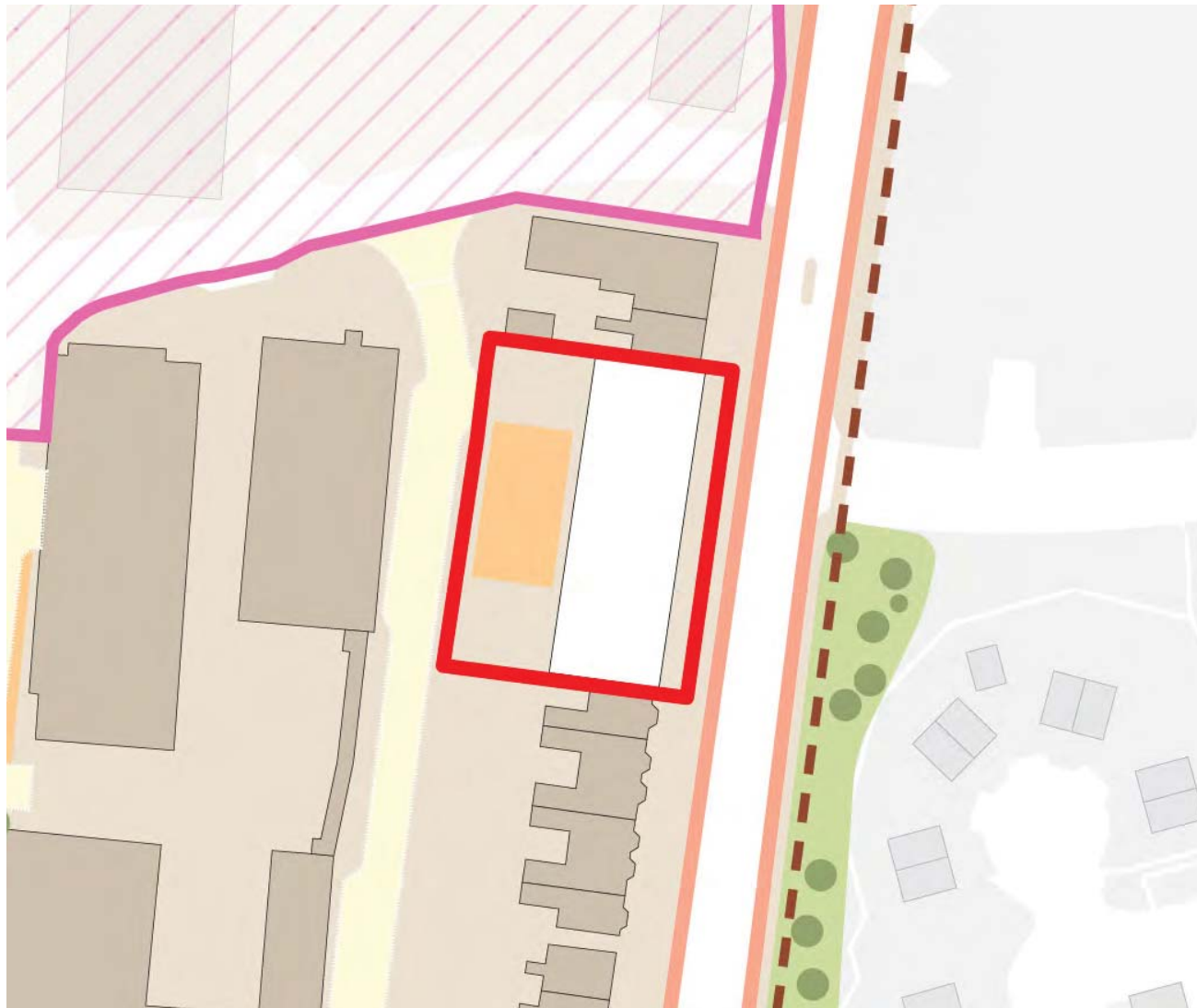
Key:  
— Site boundary



Key:  
■ Locally Listed buildings

Existing Heritage.

#### 4.1.1 Plot Codes - Site 1 - Proposals



- Key
- Site boundary
  - Indicative layouts for development areas
  - Existing buildings to be retrofitted
  - Private, communal, and public green spaces
  - Key hard open spaces
  - Existing site plots and buildings
  - Immediate development opportunities
  - Sites for retrofit / enhancement
  - Future development opportunities

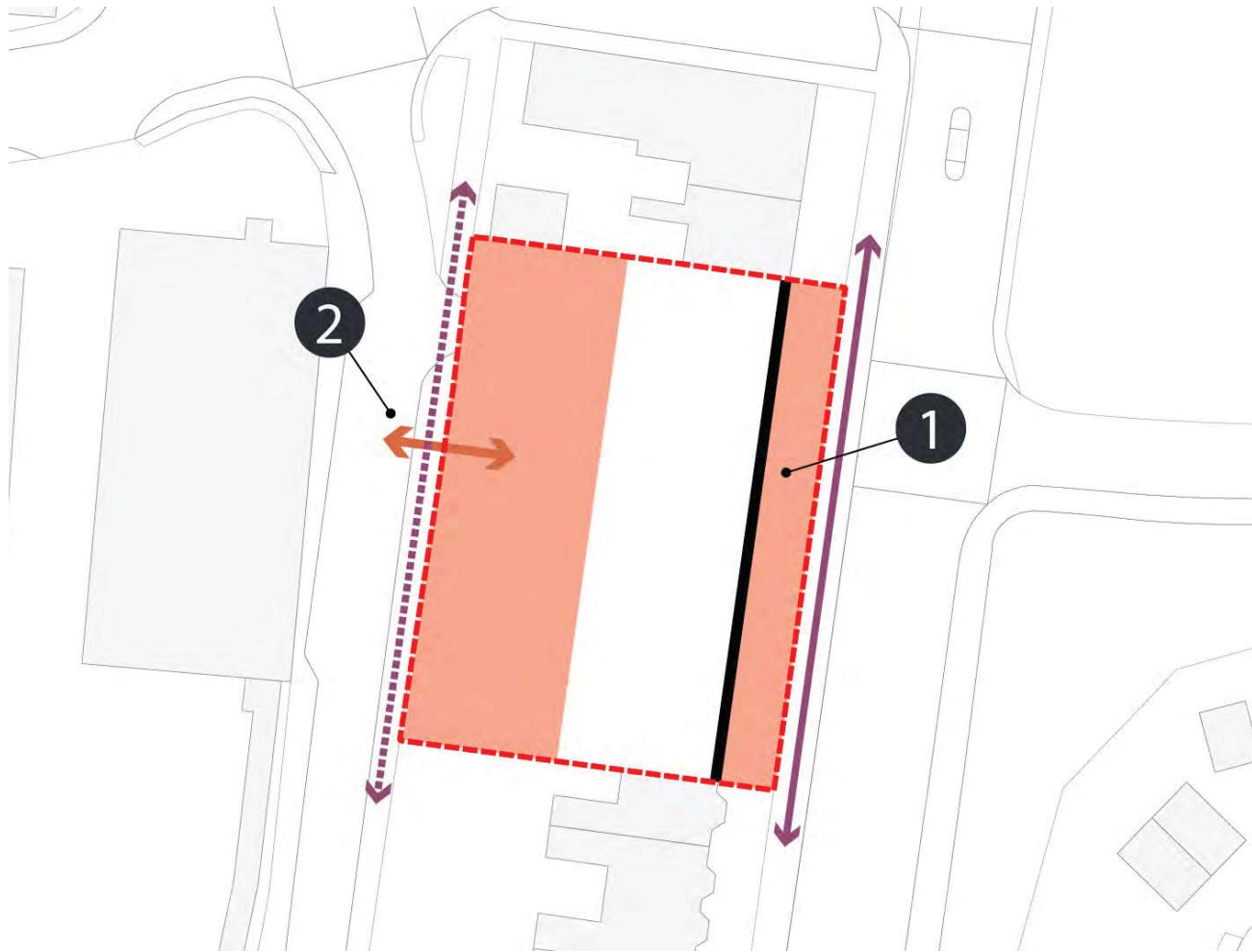


Key Plan.

Key:

1. Site boundary

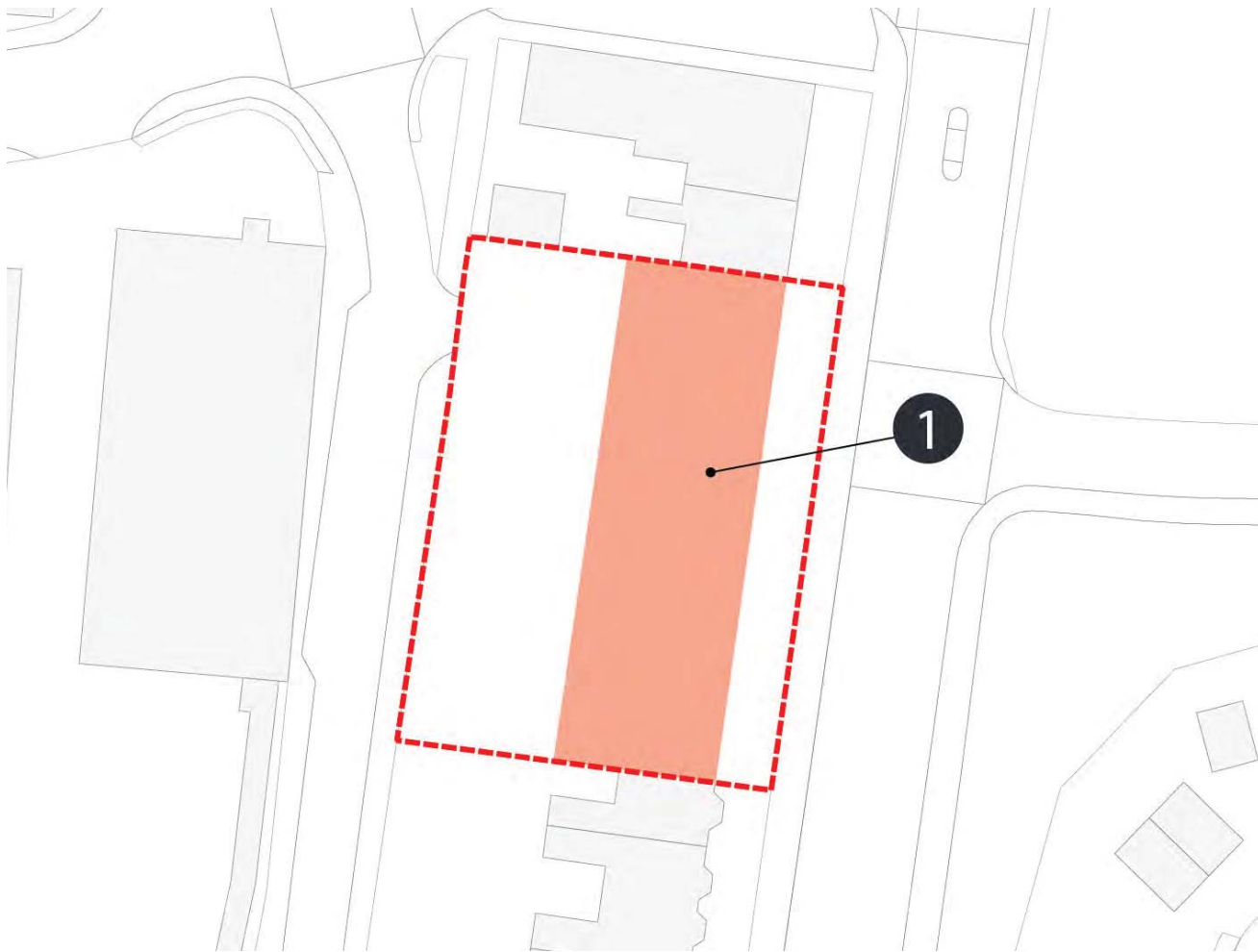
#### 4.1.1 Plot Codes - Site 1 - Proposals



Key:

1. A defined front garden as defensible landscape space at street level shall be retained and provided including a red brick boundary wall, with hedgerow or appropriate railings
2. Vehicular access to development plot

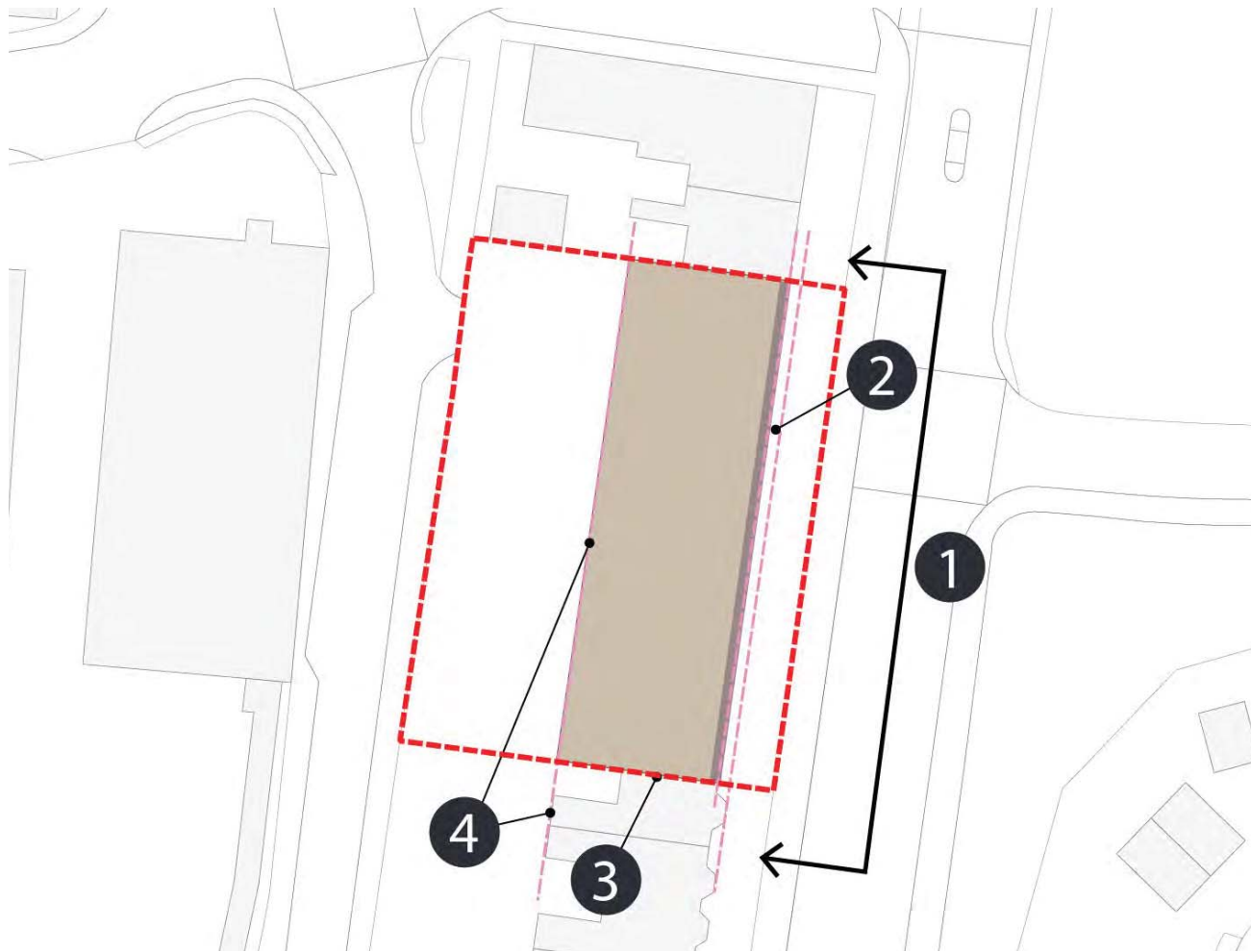
Open Space and Access.



- Key:
- 1. Heights to match adjacent buildings.
  - 2.5 Storeys
  - 10m

Building Heights.

#### 4.1.1 Plot Codes - Site 1 - Proposals

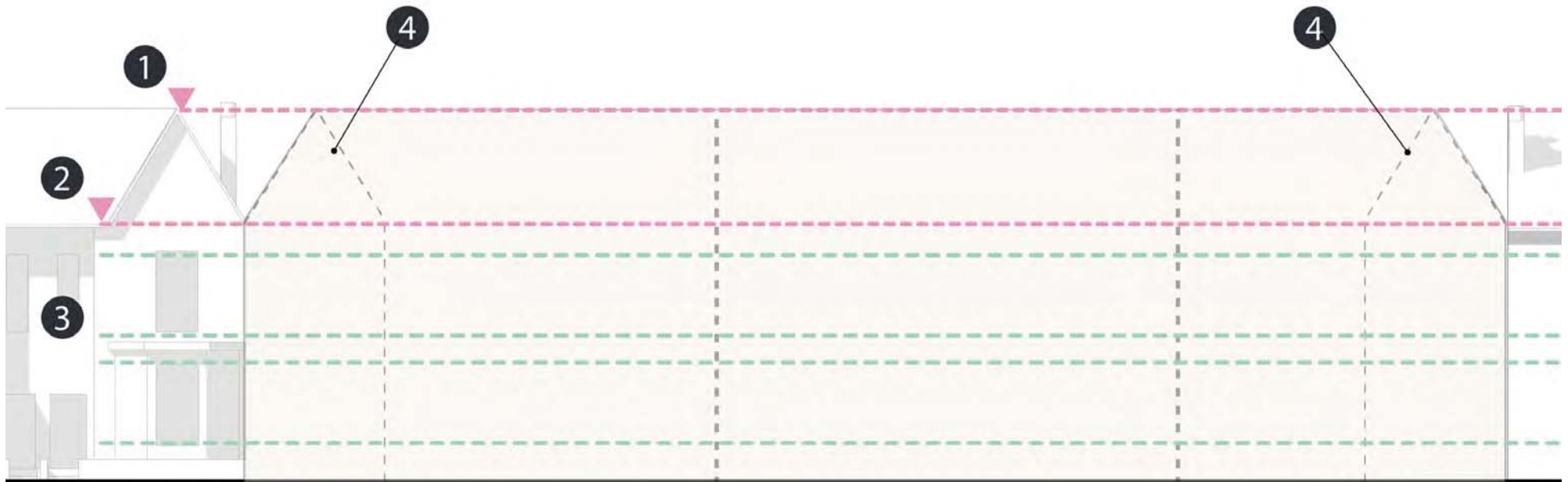


Key:

1. Elevation 1
2. Building line along Chester Road to align with adjacent buildings
3. Proposed development to sit against adjacent party wall or plot boundary and have a maximum width of 15 metres
4. Building line along the back must not extend beyond adjacent rear building lines

Building Line and Active Frontage.

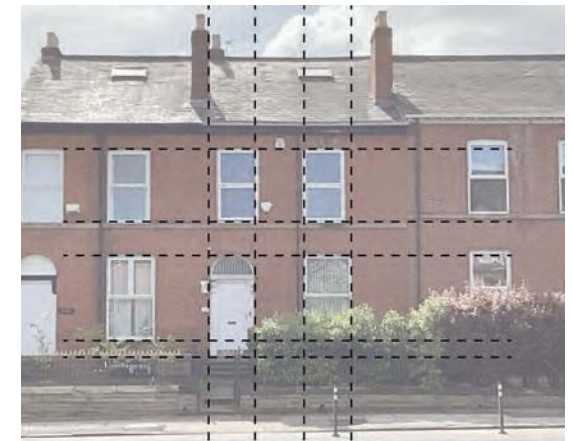
#### 4.1.1 Plot Codes - Site 1 - Proposals



Elevation 1

#### Key:

1. Ridge line of new building to match adjacent- refer to 'existing building heights' and 'building heights' diagrams
2. Eaves line of new building to match adjacent
3. Fenestration to follow setting out of adjacent buildings- refer to Design Codes 'HOUSES 7 – Shape and Proportion of Openings' (page 24), and 'HOUSES 8 – Recessed Doors and Windows' (page 25)
4. Option to add gable end, to reference adjacent Birkdale Terrace.



Example elevation rhythm of Winfield Terrace -refer to HOUSES 6 – Surrounding Context and Rhythm

#### 4.1.2 Plot Codes - Site 2 - Site Analysis



Site 2 is located between Chester Road and Empress Street, with Wright Street dividing the site into two halves. The Veno Building and Trafford Press sit at the south of the site, and Empress Mill is located to the north. These buildings are significant remnants of the area's commercial and industrial past.

#### Key:

1. Sunpath.
2. Hard-standing to rear of Empress Mill.
3. Car park.
4. Dense green space.
5. Green space.
6. Main vehicular access.
7. Main pedestrian access.
8. Empress Brewery landmark building.
9. Essence Factory landmark building, Grade II Listed.
10. Empress Mill.
11. Veno Building.
12. Trafford Press.
13. Gate posts on Wright Street.
14. Brick boundary wall to be retained.



Veno Building, Chester Road.



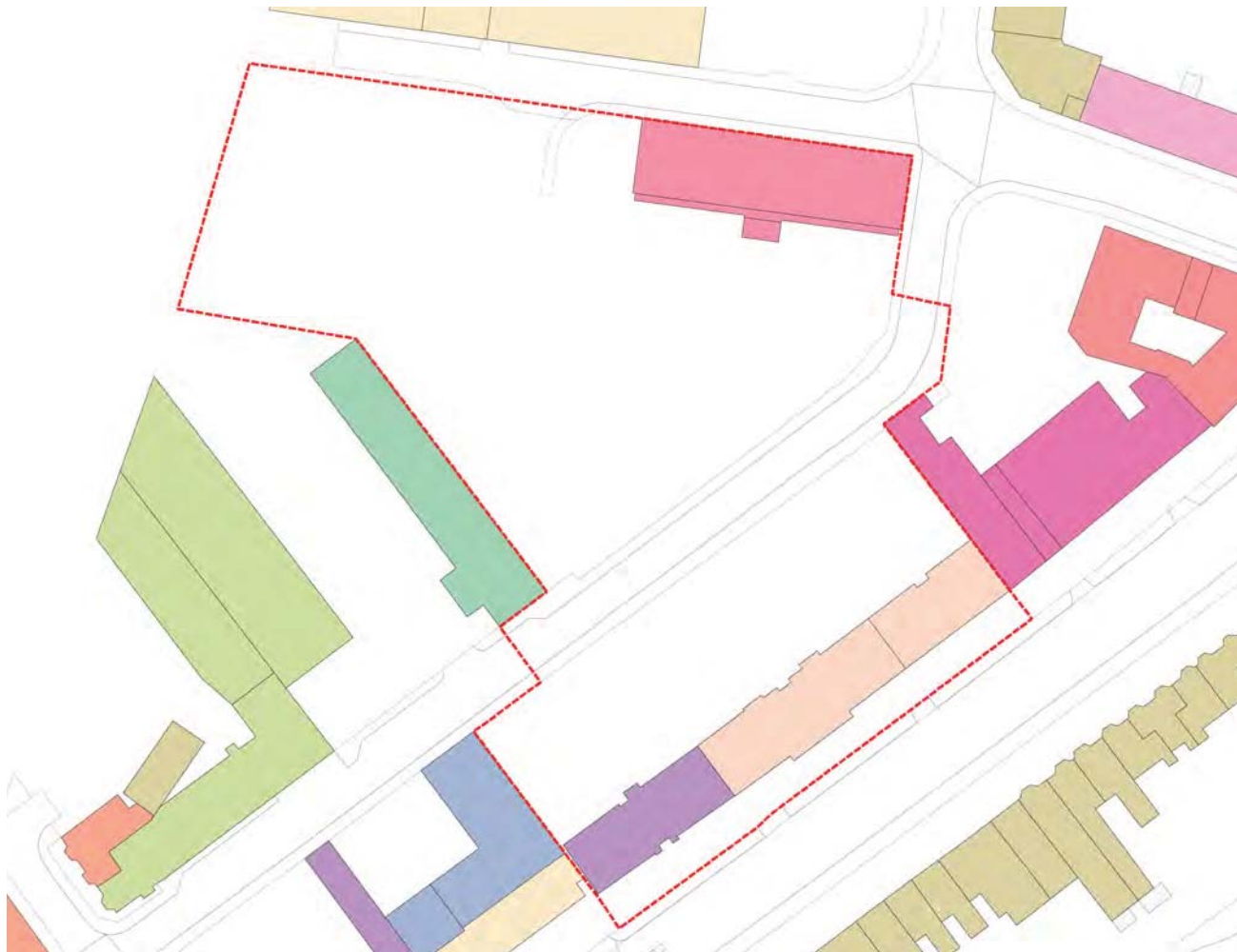
Empress Mill, Empress Street.



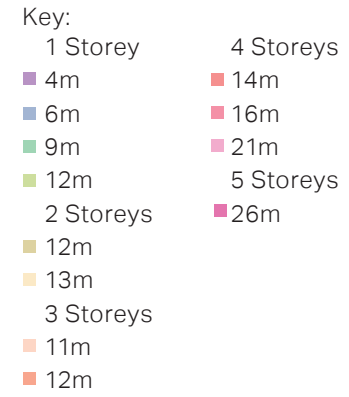
The Essence Factory, Chester Road.



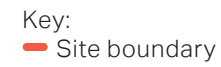
### 4.1.2 Plot Codes - Site 2 - Site Analysis

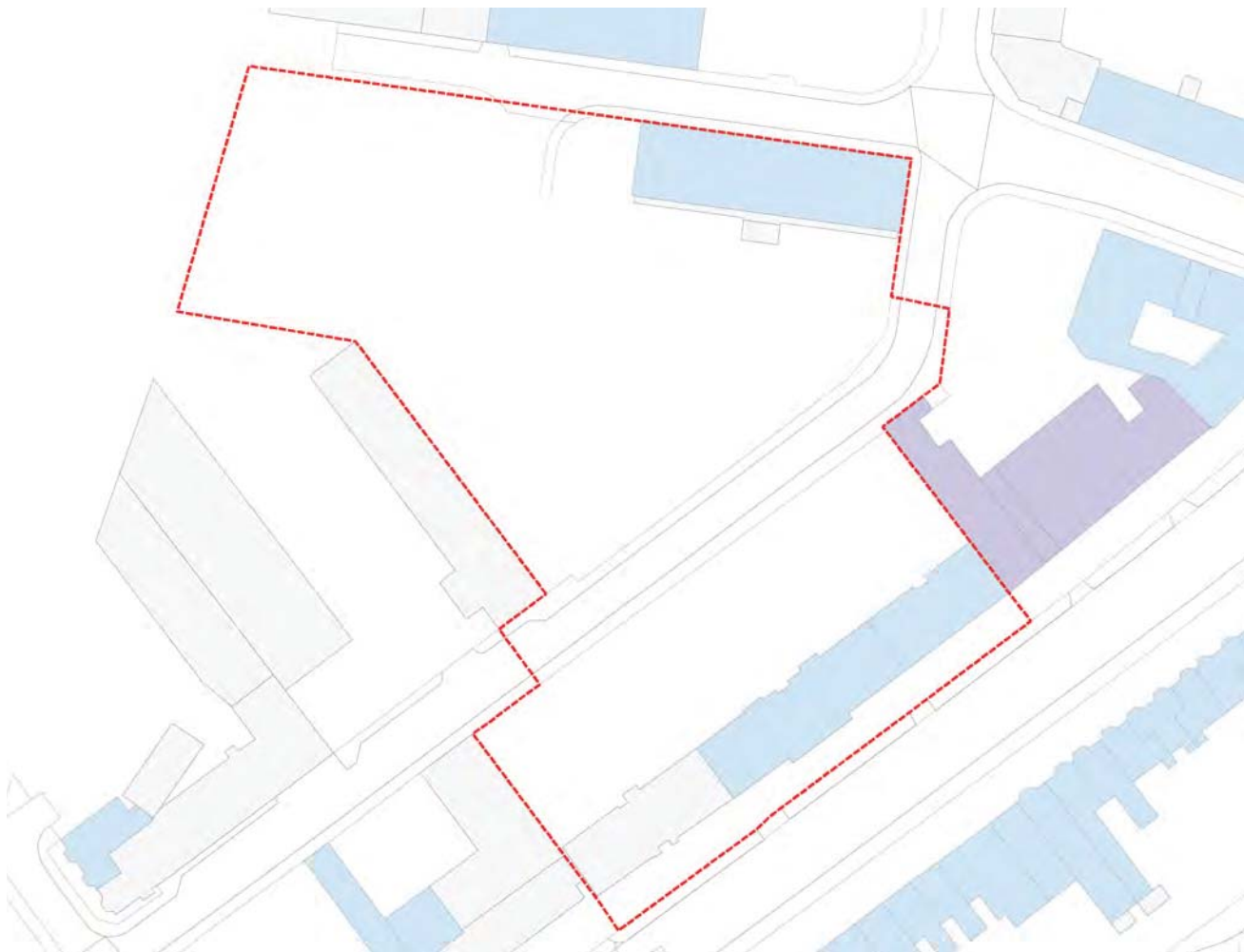


Existing Building Heights.



Aerial View.

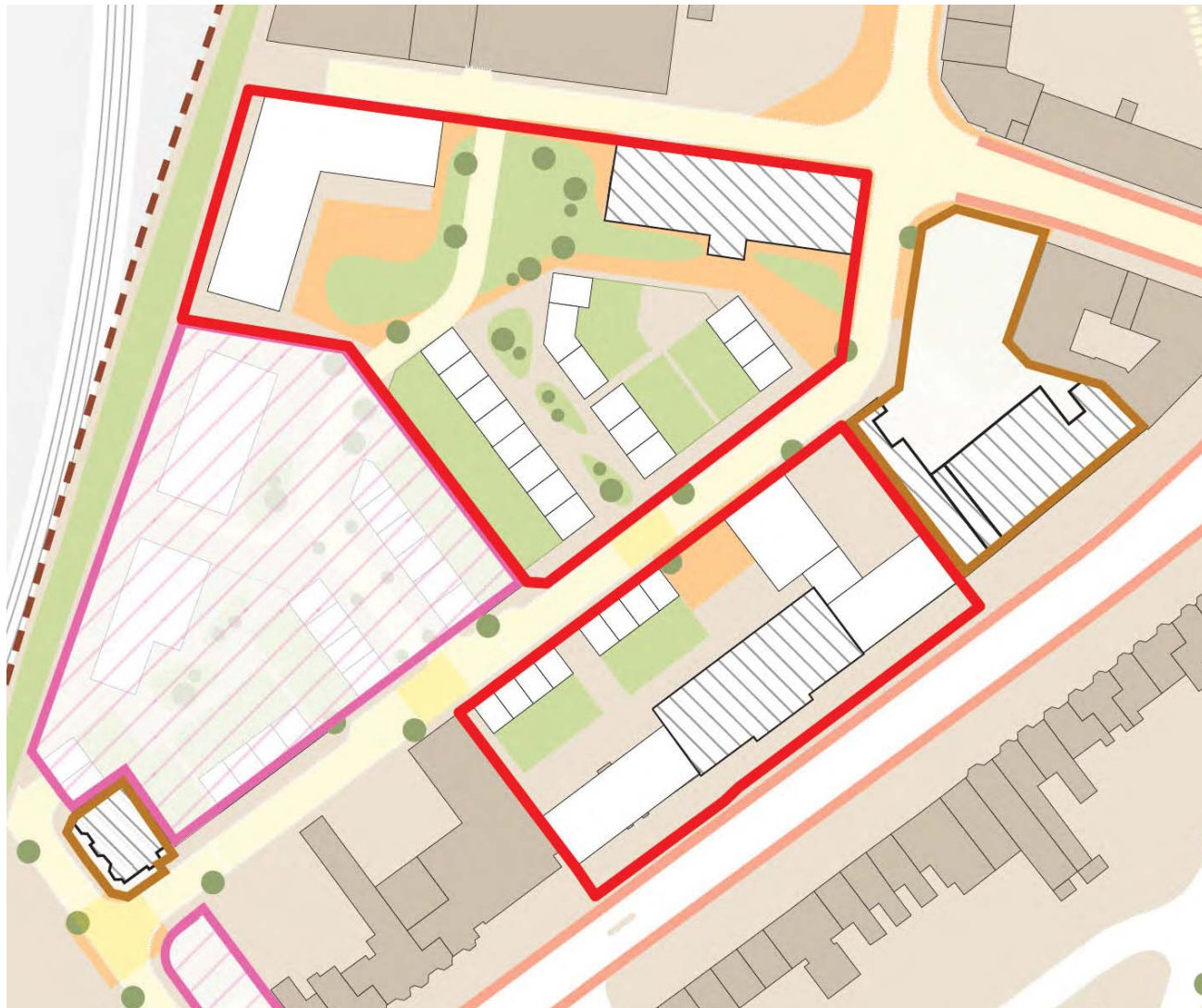













- Key:
- Grade II Listed Building
  - Locally Listed buildings

Existing Heritage.

#### 4.1.2 Plot Codes - Site 2 - Proposals



- Key
-  Site boundary
  -  Indicative layouts for development areas
  -  Existing buildings to be retrofitted
  -  Private, communal, and public green spaces
  -  Key hard open spaces
  -  Existing site plots and buildings
  -  Immediate development opportunities
  -  Sites for retrofit / enhancement
  -  Future development opportunities



Key Plan.

Key:

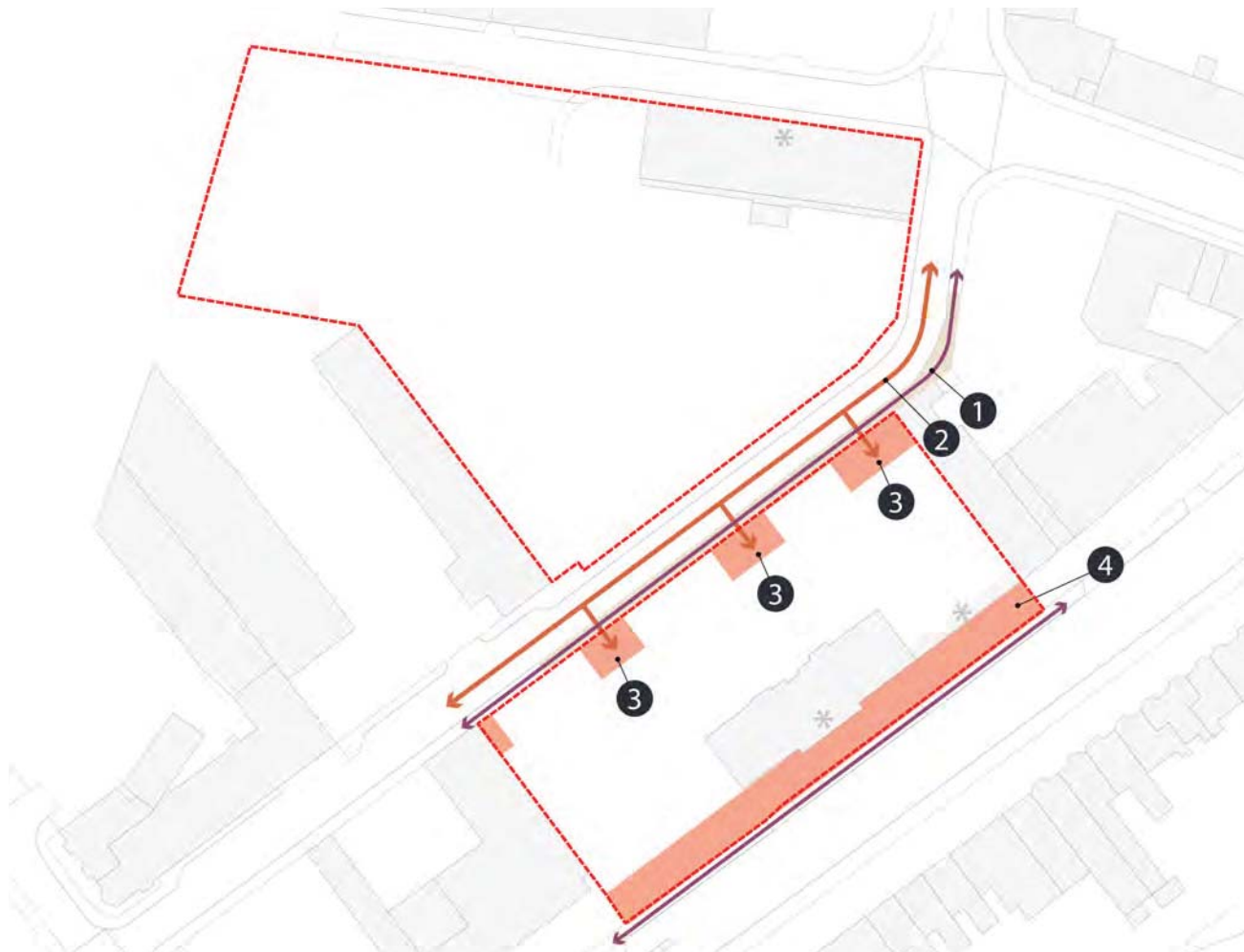
1. Site boundary



Key:

1. Key arrival space adjacent to Empress Mill, suitable for higher quality public realm intervention
2. New neighbourhood public open green space with play area
3. Existing trees retained
4. Pedestrian routes through the site
5. Informal pedestrian routes through the site for permeability
6. Defensible landscape space as interface between public and private
7. Vehicular access through the development site
8. Building lines with active frontages
9. Indicative building lines
10. A communal open space to be delivered within the development plot
11. Gate posts on Wright Street to be retained and integrated in public open space landscape design
12. Where possible, the existing red brick wall should be retained and enhanced

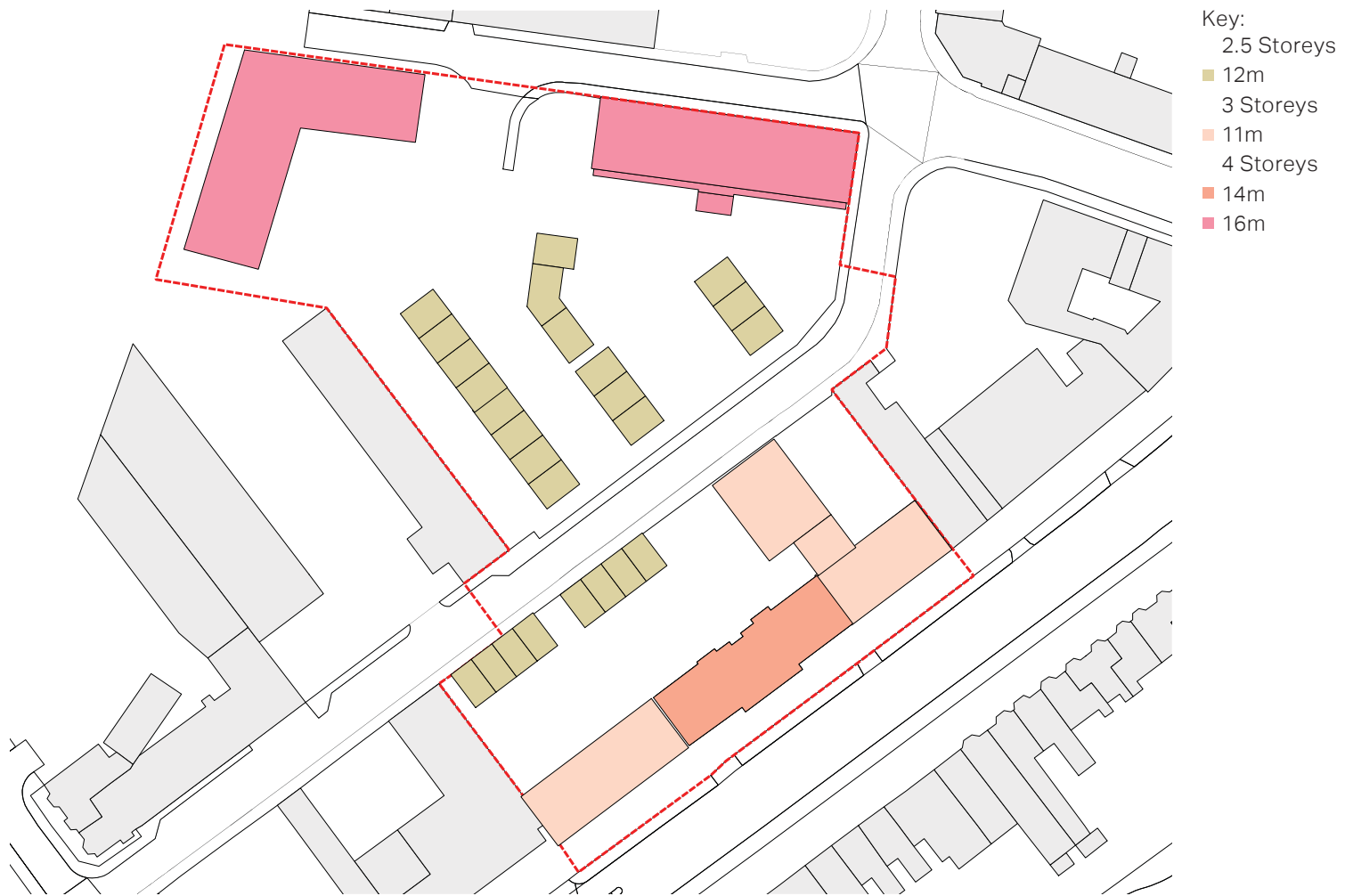
Site 2 North: Open Space and Access.



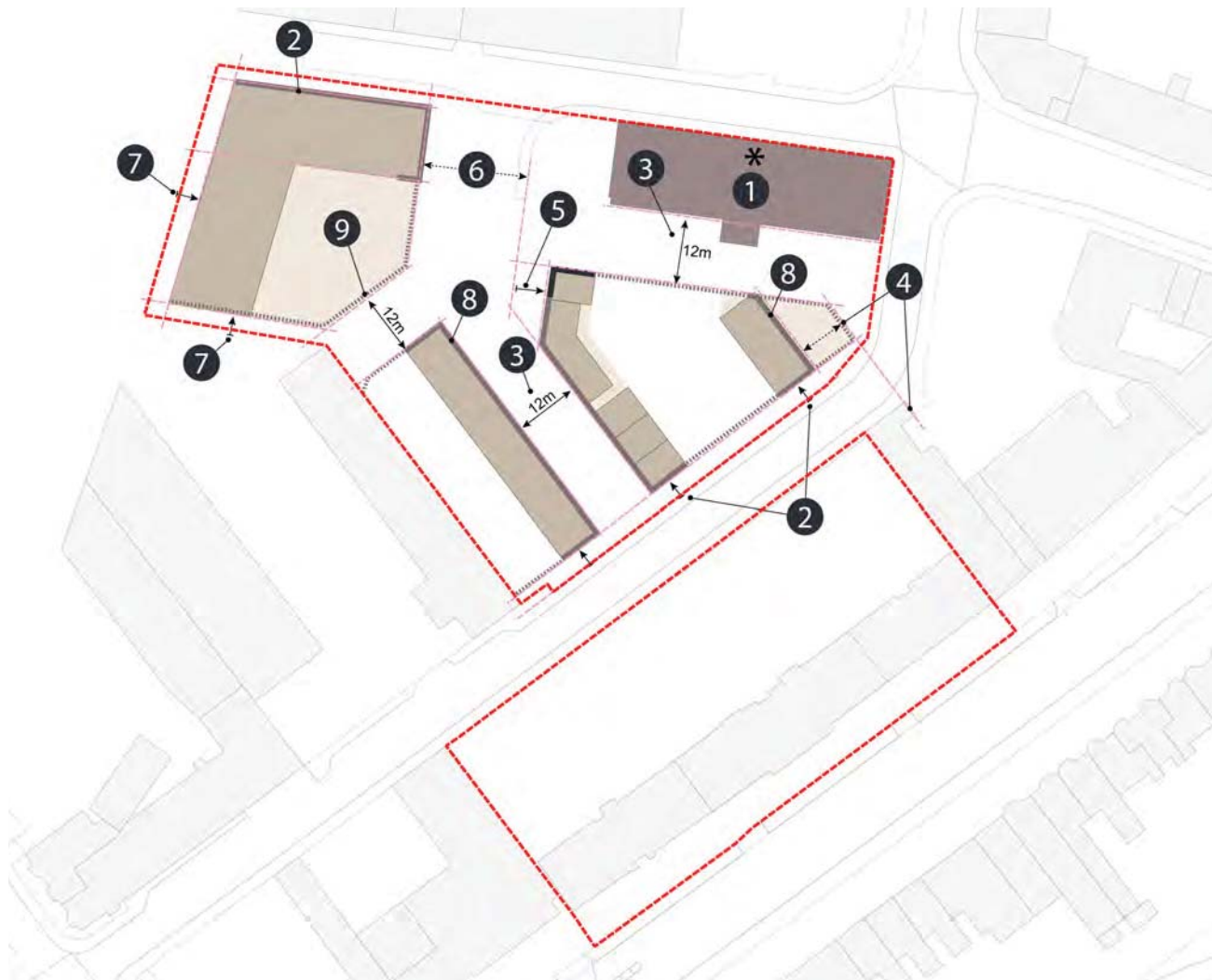
Key:

1. Pedestrian routes through the site
2. Vehicular access through the development site
3. Opportunity for vehicular access onto plot
4. Defensible landscape space as interface between public and private

Site 2 South: Open Space and Access.



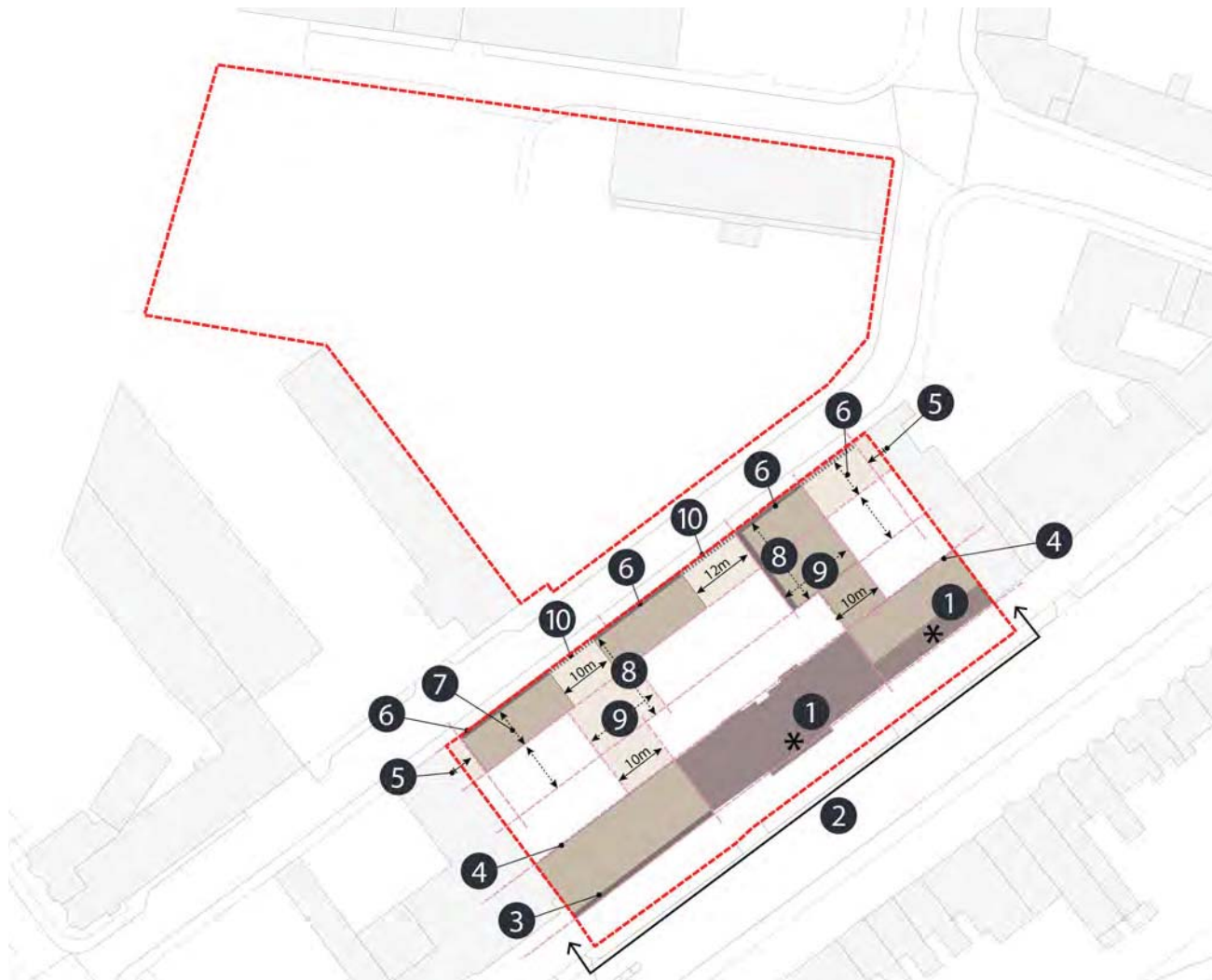
Building Heights.



Key:

1. Existing buildings to be retained
2. Maximum of 1 metre set-back from back of pavement to provide a defensible interface between private and public
3. Between 11 to 12 metres between building lines in tertiary streets to allow for private front garden space as defensible interface between private and public
4. Maximum building line aligned with adjacent mill building corner
5. Maximum 5 metre set-back from back of existing footpath to allow for a larger open green space
6. Maximum of 18 metres between building line and existing back of kerb
7. Minimum 3 metre set-back from plot boundary along tram line and adjacent plot to the south
8. Highlighted building lines must provide active frontages such as doors and windows and/or lively internal uses visible from the outside
9. Dashed building lines are shown indicatively and could change depending on the building layout on the plot but they must provide active frontage

Plot 2 North: Building Line and Active Frontage.

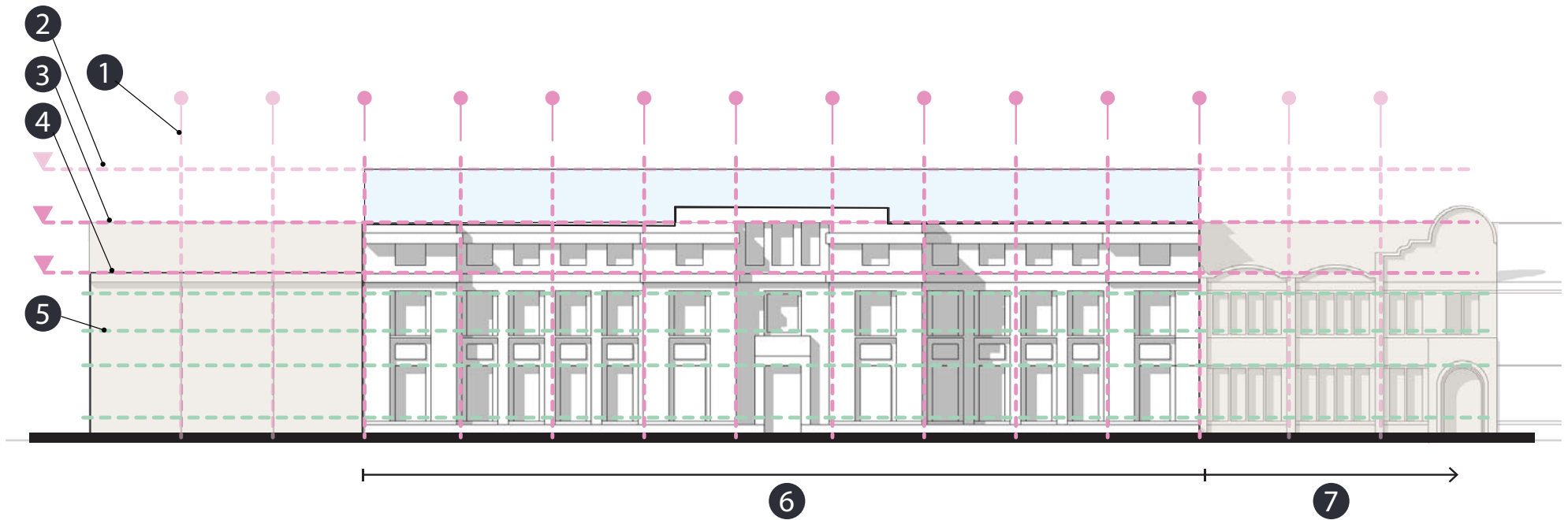


Key:

1. Existing buildings to be retained
2. Elevation 1
3. Building line along Chester Road to align with Veno building, Trafford Press and Empress Mill
4. Rear building line to align with Veno building and Trafford Press
5. A minimum 2 metre set back from neighbouring buildings
6. Continuous building line aligned with adjacent buildings at back of pavement along Wright Street
7. Maximum 8 metre deep building footprint for terraced housing, with 10 metre rear gardens
8. Option for an a maximum 18 metre deep apartment building
9. Maximum apartment building width of 15 metres
10. A minimum of 2 gaps within the building line: 10 metre between terraced houses or 12 metre gab between side of terraced house and apartment building

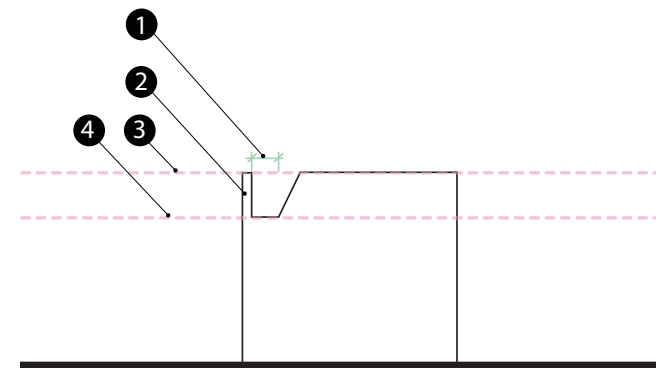
Plot 2 South: Building Line and Active Frontage.





Key:

1. Setting out of set-back to be coherent with bays of adjacent buildings.
2. Height of set back to match storey height of existing building
3. Roof line of adjacent buildings
4. Eaves line of adjacent buildings
5. Setting of fenestration to reference adjacent buildings
6. Venio
7. Trafford Press



Cross-section of Trafford Press

Key:

1. Min 1.2m between parapet and mansard roof
2. Parapet
3. Ridge line
4. Eaves line

### 4.1.3 Plot Codes - Site 3 - Site Analysis



Key:

1. Sunpath.
2. Main pedestrian access.
3. Main vehicular access.
4. Hard-standing.
5. Green space.
6. Car park.



Chester Road, towards the north.



Chester Road, towards the south.



Car park from Chester Road,

Site 3 is situated to the south of the Regeneration Plan Area, in between Chester Road and Stretford Road. Within the site is a large car park and the majority of the site is hard-standing. The Westpoint building sits adjacent to the site and is one of the tallest buildings within the surrounding area.

### 4.1.3 Plot Codes - Site 3 - Site Analysis



Existing Building Heights.

- Key:
- 1 Storey
  - 2 Storeys
  - 3 Storeys
  - 4 Storeys
  - 5 Storeys
  - 6 Storeys
  - 7 Storeys
  - 8 Storeys












Aerial View.

- Key:
- Site boundary

### 4.1.3 Plot Codes - Site 3 - Proposals



- Key
-  Site boundary
  -  Indicative layouts for development areas
  -  Existing buildings to be retrofitted
  -  Private, communal, and public green spaces
  -  Key hard open spaces
  -  Existing site plots and buildings
  -  Immediate development opportunities
  -  Sites for retrofit / enhancement
  -  Future development opportunities

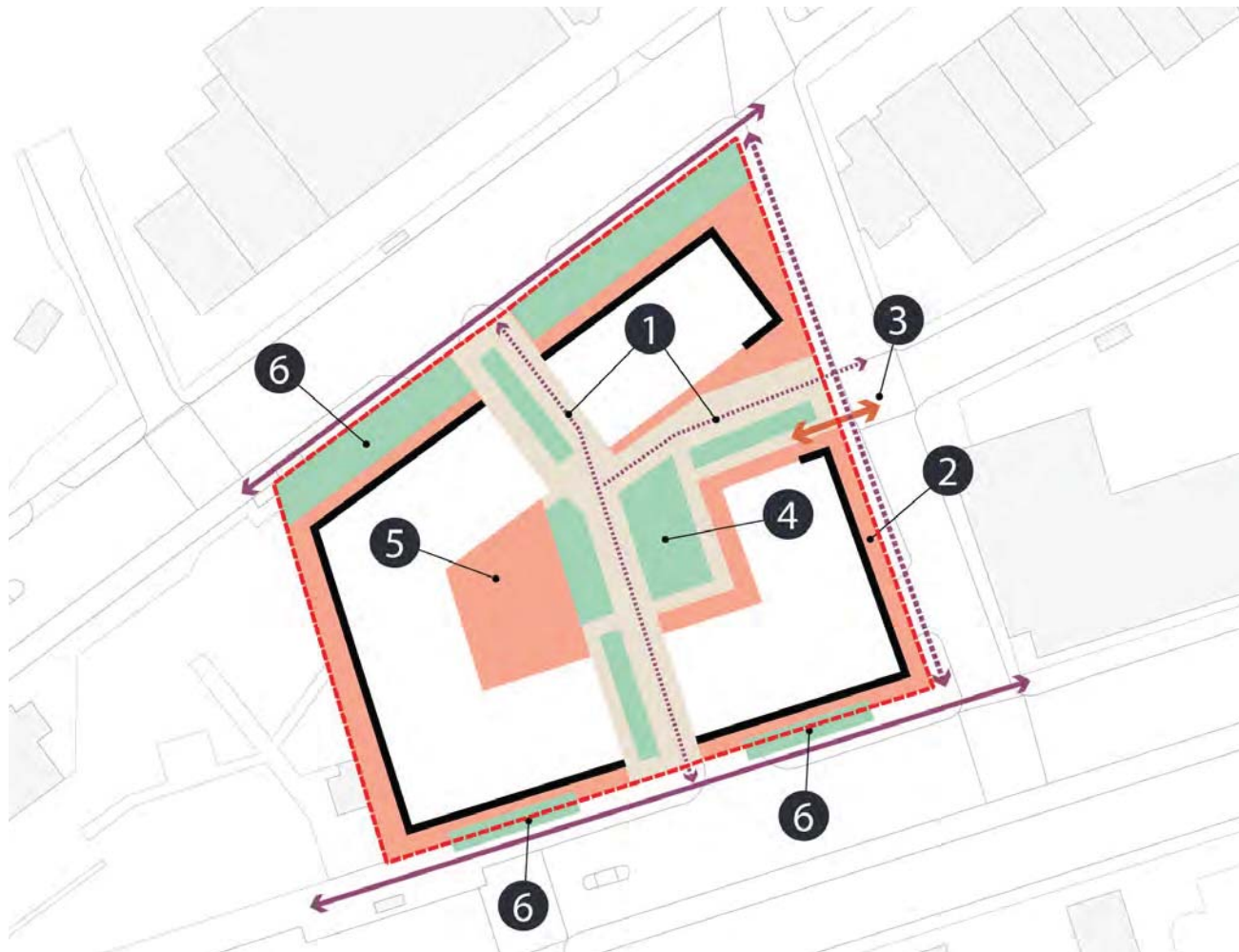


Key Plan.

Key:

1. Site boundary

### 4.1.3 Plot Codes - Site 3 - Proposals



Key:

1. Permeability through the site
2. Defensible landscape space as interface between public and private
3. Vehicular access possible from Nuttall Street to development site
4. New publicly accessible open green space
5. Private or communal space
6. Opportunity for tree planting along Chester Road and Stretford Road

Open Space and Access.

### 4.1.3 Plot Codes - Site 3 - Proposals

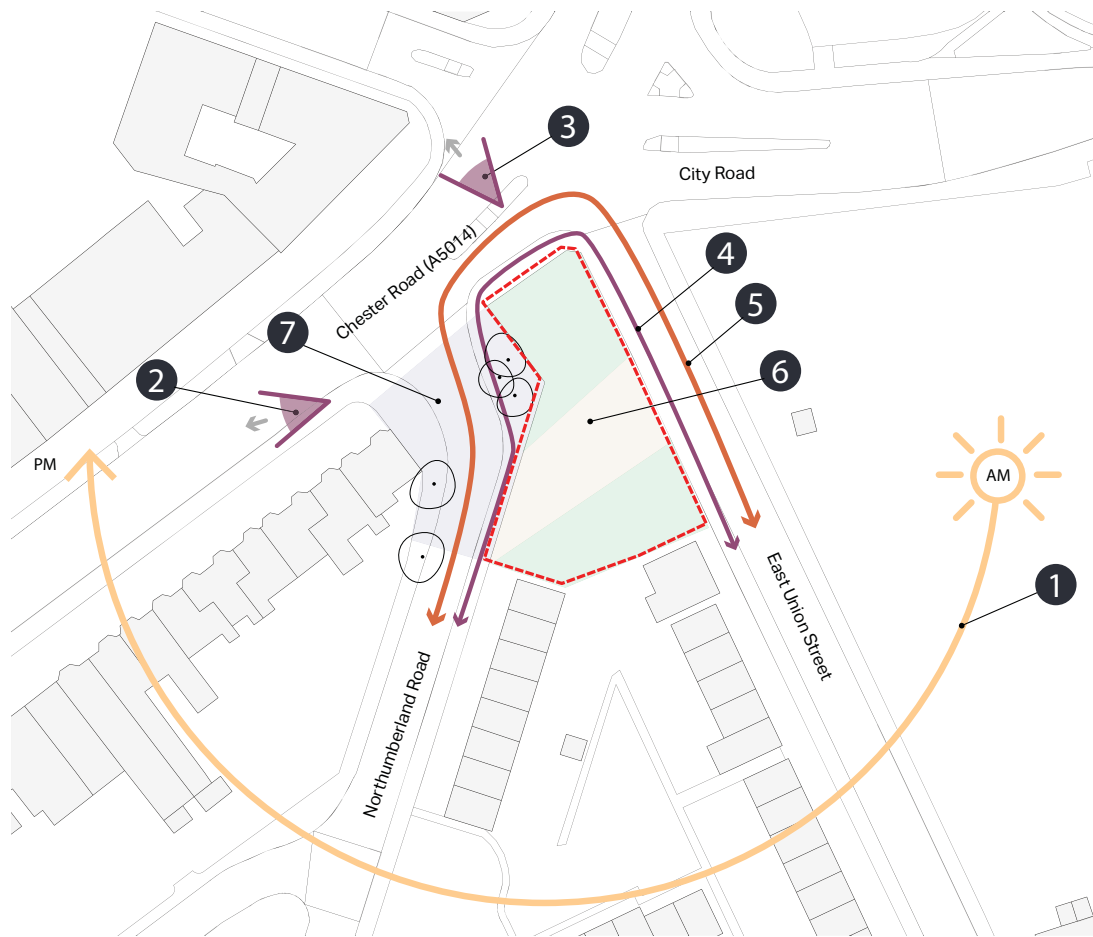


Key:

1. Key building corner, consider views along Chester Road and Stretford Road
2. 7 metre set-back from back of pavement plot boundary along Chester Road
3. 2-3 metre set-back from back of pavement plot boundary along Stretford Road and Nuttal Street
4. 4-6 metre set-back from western plot boundary
5. Maximum of 15 metre deep building blocks to retain a central courtyard
6. Maximum of 3.5 metres from back of pavement plot boundary
7. 10 metre gap between buildings along Chester Road and Stretford Road must be provided
8. All perimeter building lines must provide active frontages such as doors and windows and/or lively internal uses visible from the outside
9. Views to be considered towards prominent building corners

Building Line and Active Frontage.

#### 4.1.4 Plot Codes - Site 4 - Site Analysis



Key:

1. Sunpath.
2. View of Essence Factory, Trafford Press and the Veno Building.
3. View of Essence Factory and the Former Brewery.
4. Main pedestrian access.
5. Main vehicular access.
6. Open yard space.
7. Hard-standing, some green space.
8. Northumberland Road.



Chester Road.



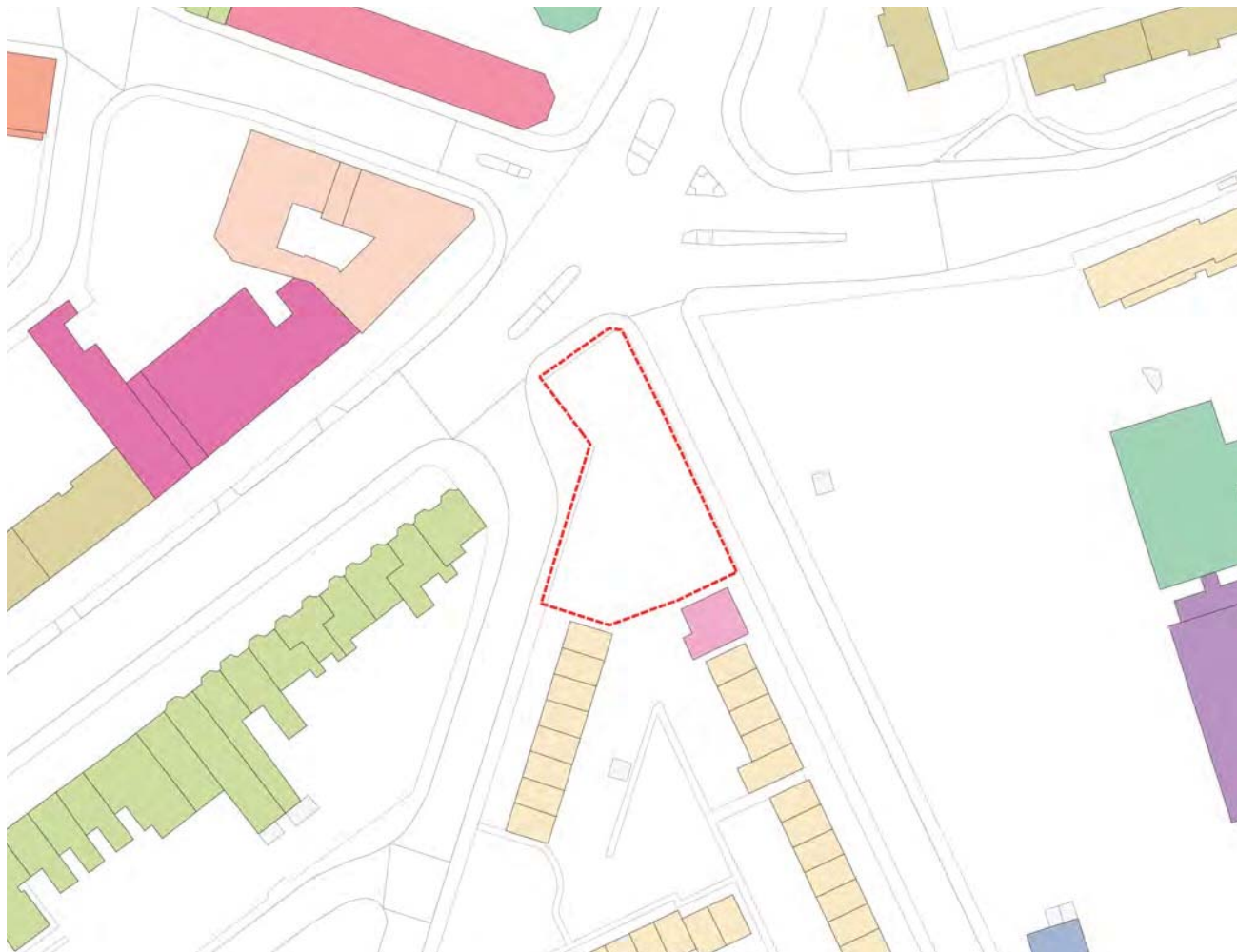
East Union Street, towards the north.



East Union Street, towards the south.

Site 4 is located where Chester Road meets City Road and is bounded by East Union Street. One of the main features of the site is its proximity to the main routes into the Empress Regeneration Plan Area, and significant historic buildings such as the Essence Factory and Former Brewery.

#### 4.1.4 Plot Codes - Site 4 - Site Analysis



Existing Building Heights.

Key:

1 Storey	4 Storeys
3m	12m
6m	14m
2 Storeys	16m
10m	21m
12m	5 Storeys
3 Storeys	14m
11m	26m



Aerial View.










Key:

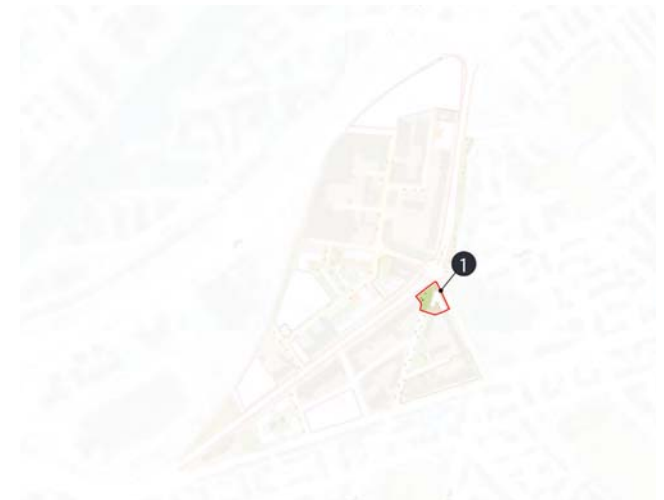
— Site boundary
-----------------



#### 4.1.4 Plot Codes - Site 4 - Proposals



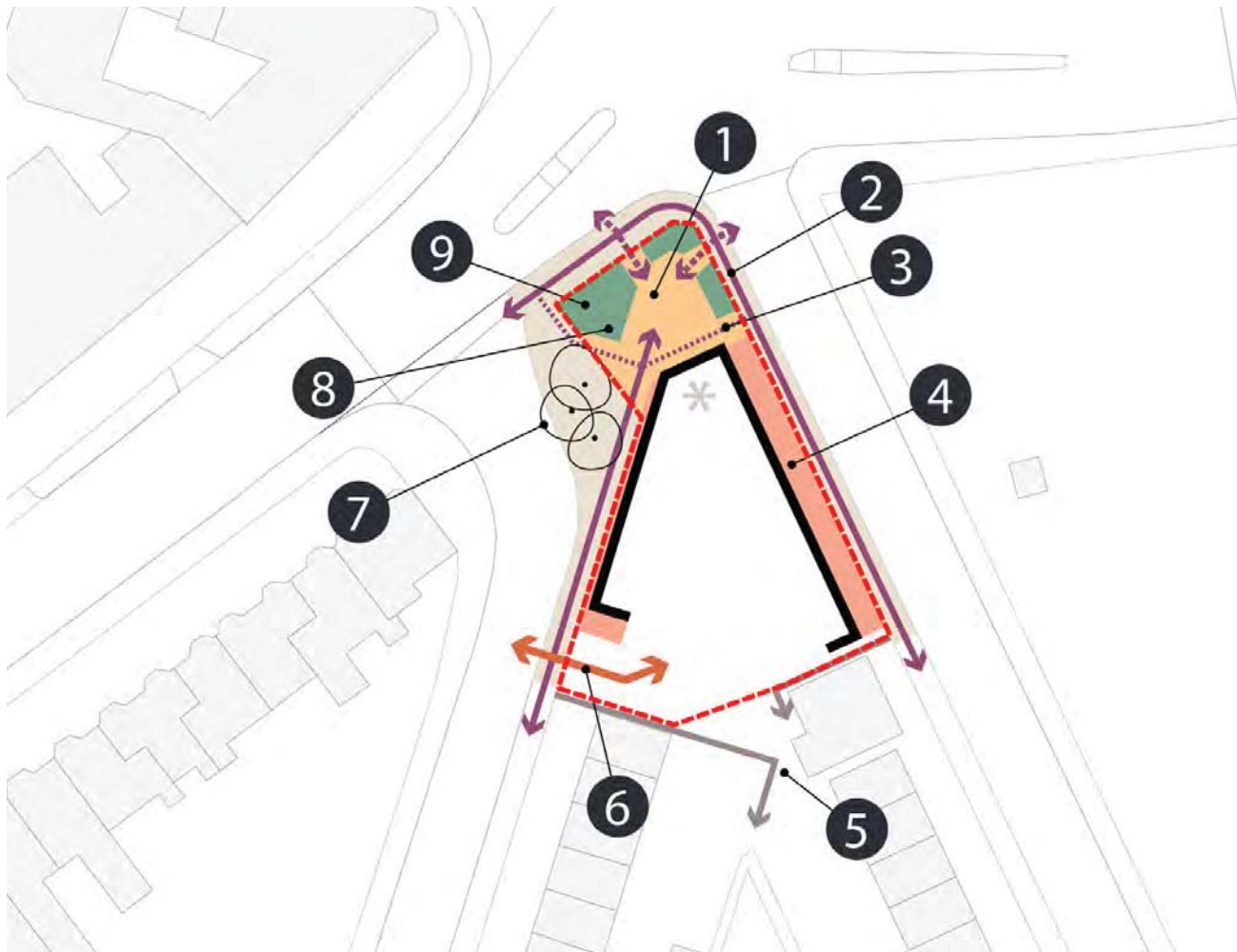
- Key
-  Site boundary
  -  Indicative layouts for development areas
  -  Existing buildings to be retrofitted
  -  Private, communal, and public green spaces
  -  Key hard open spaces
  -  Existing site plots and buildings
  -  Immediate development opportunities
  -  Sites for retrofit / enhancement
  -  Future development opportunities



Key Plan.

Key:

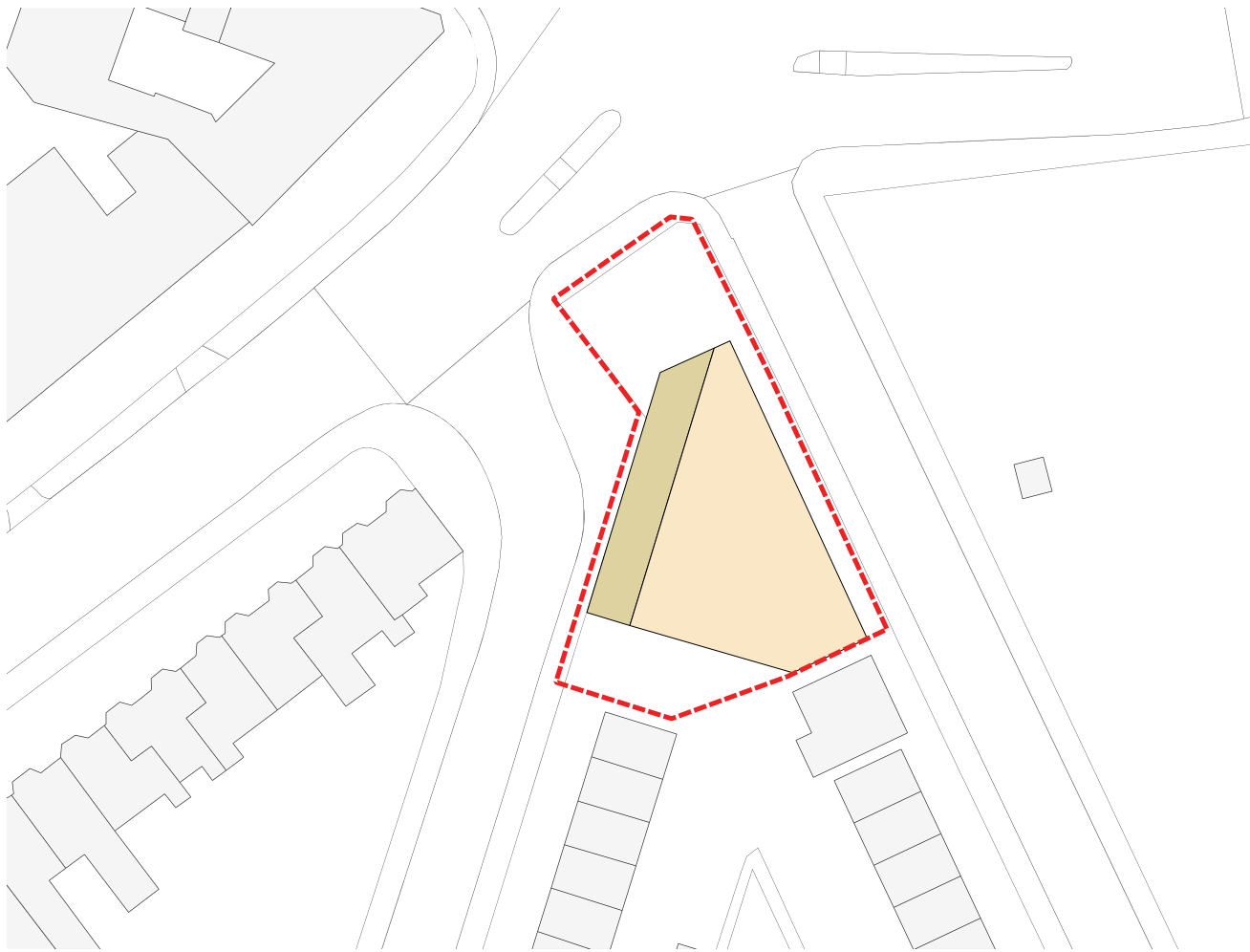
1. Site boundary



Key:

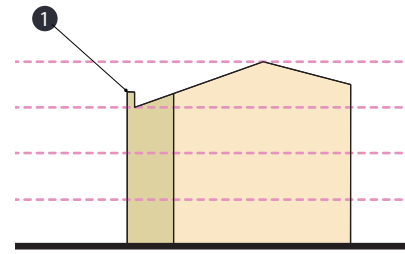
1. Key arrival space suitable for higher quality public realm intervention
2. Important pedestrian routes
3. Permeability through the site
4. Defensible landscape space as interface between public and private
5. Retained pedestrian access to neighbouring plots and buildings
6. Vehicular access
7. Existing trees retained
8. New public open green space
9. Indicative location for planting as a defensible edge around the arrival space

Open Space and Access.

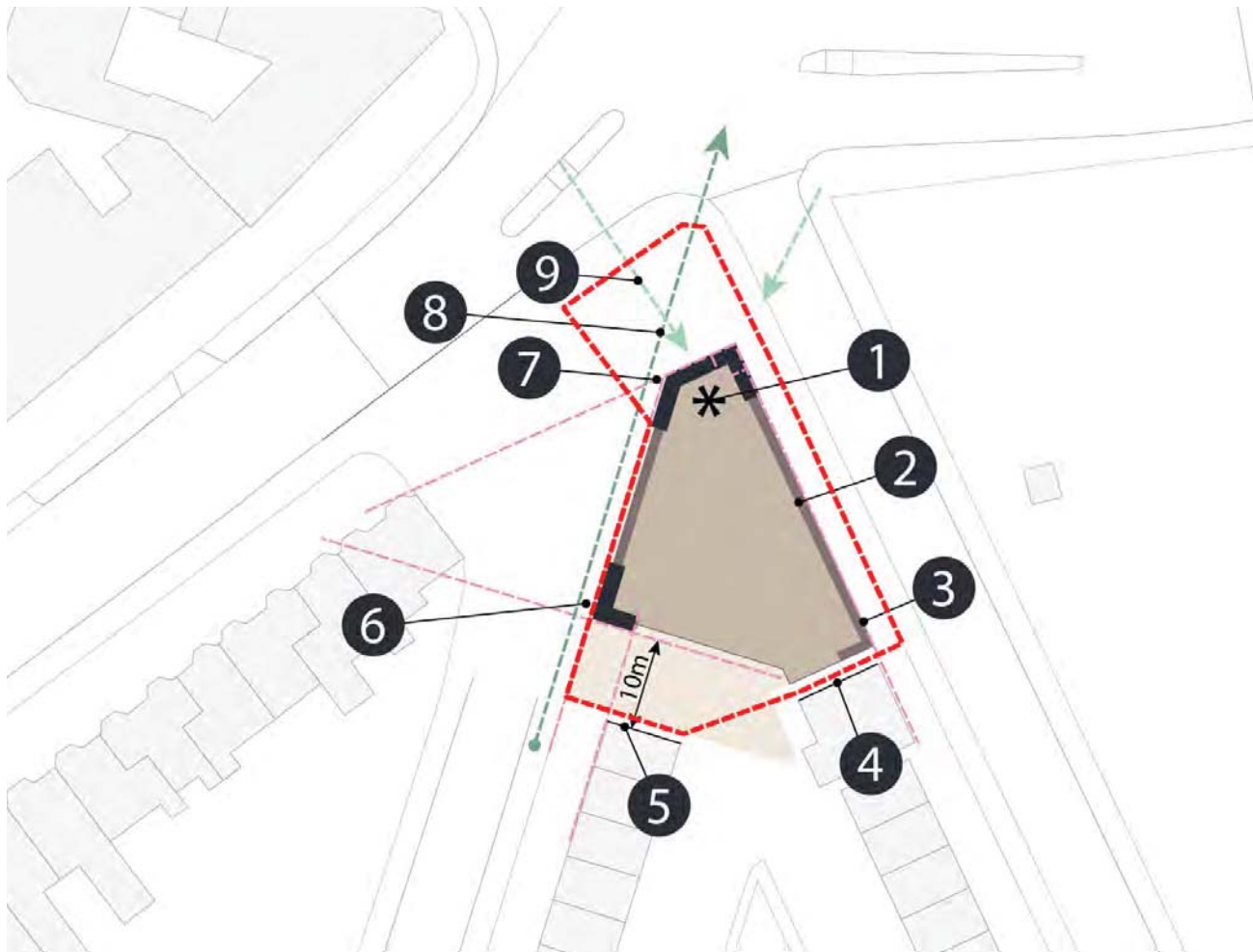


Key:  
 3 Storeys  
 9m  
 4 Storeys  
 12m

Building Heights.



Section.  
 Key:  
 1. Parapet



Key:

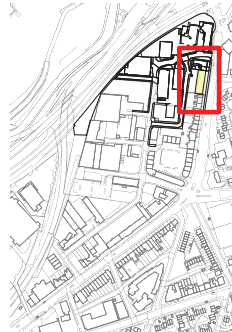
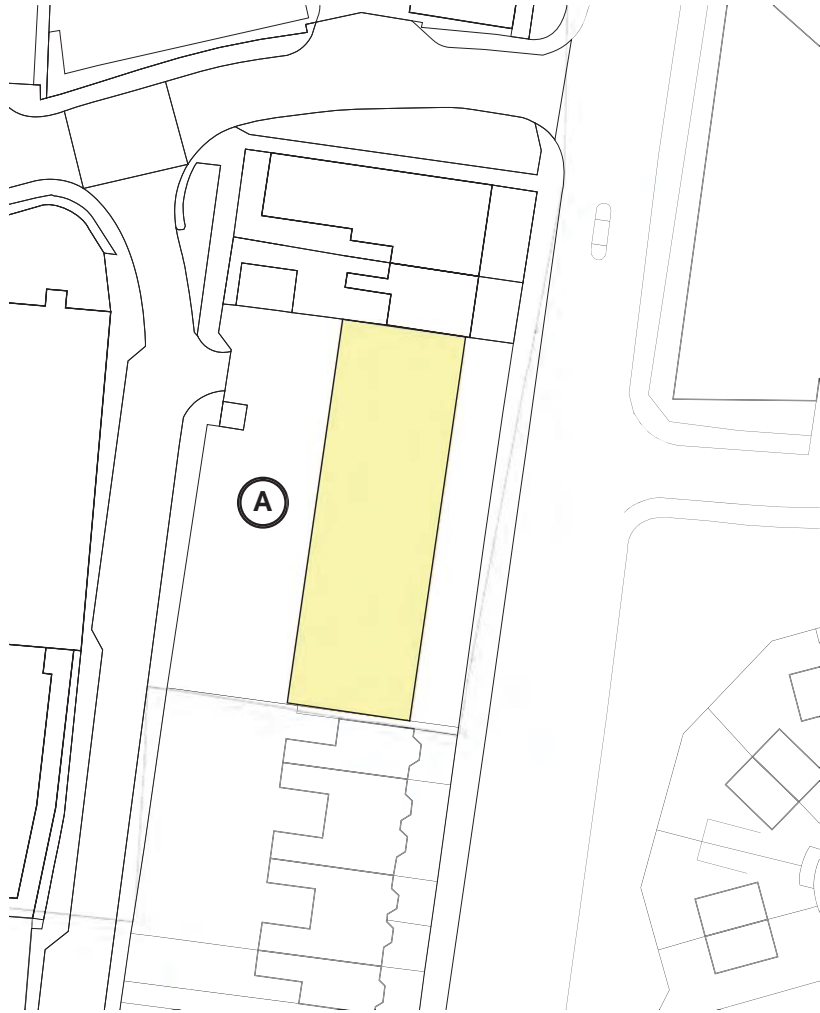
1. Key landmark building corner
2. Building lines along E Union Street and Northumberland Road must provide active frontages such as doors and windows and/or lively internal uses visible from the outside
3. Building line consistent with neighbouring building
4. A minimum of 3 metre separation with adjacent building line to retain access
5. A minimum of 10 metre separation with adjacent building
6. Key corner visible from Northumberland Road and Manchester Street junction
7. Building line, orthogonally to E Union St, does not exceed terraced housing along Chester Road
8. Long distance views towards Chester Road retained
9. Key views to be considered as part of the arrival experience

Building Line and Active Frontage.

Donald Insall Associates



# Site 01

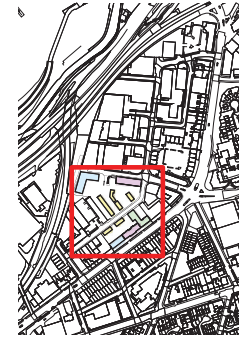
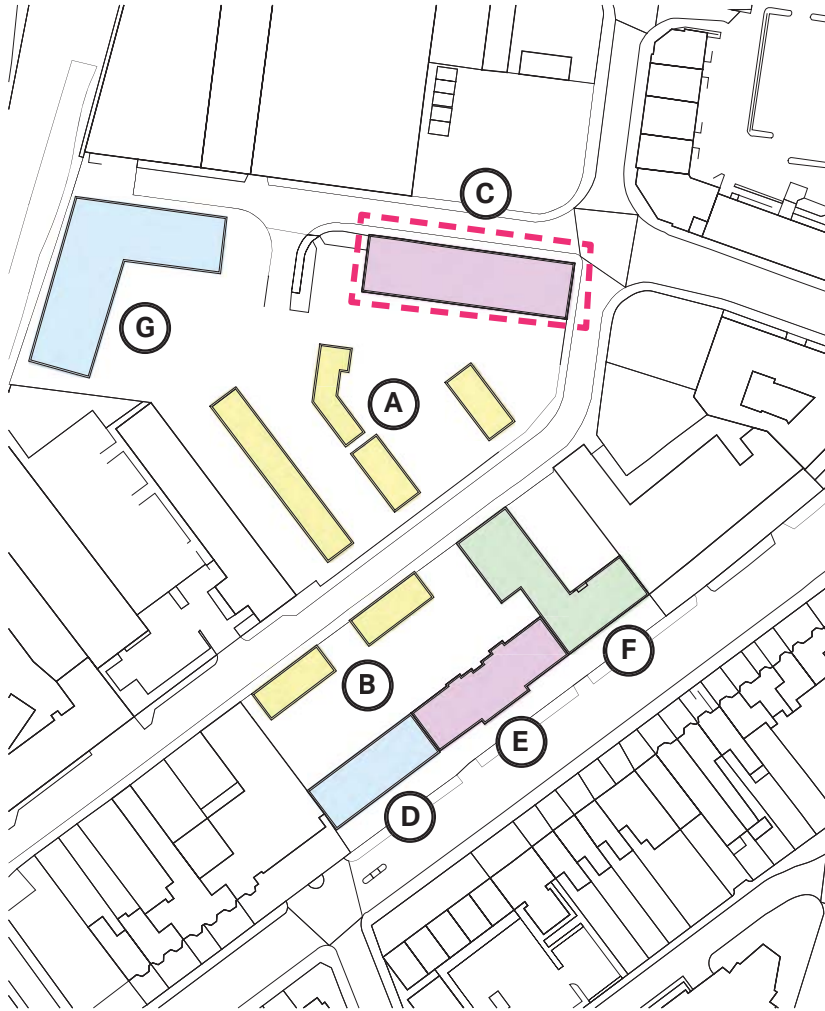


Key Plan (Not to Scale)



■ New Build- Resi Townhouse/ Mansion Block

# Site 02

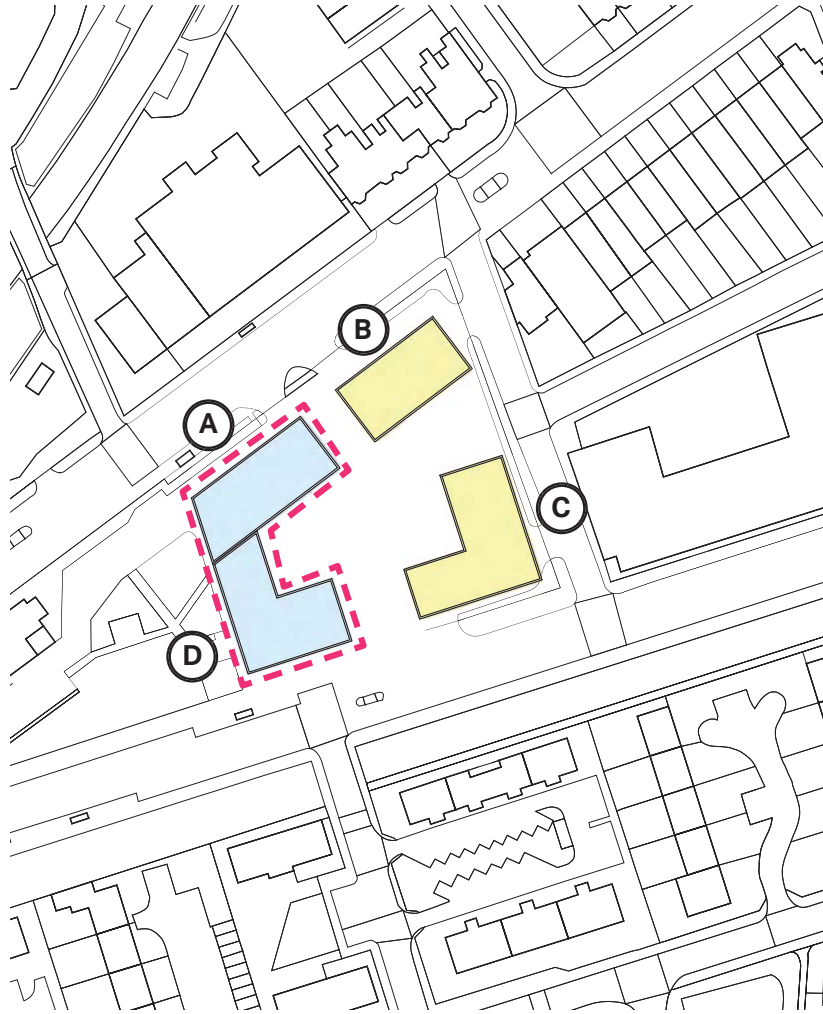


Key Plan (Not to Scale)

- New Build- Resi Townhouse/ Mansion Block
- New Build- Apartments
- Refurb- Resi
- Façade Retention & New Build
- Commercial Ground Floor (potential amenity space for apartments or car parking)




Site 03

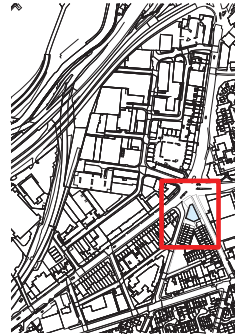
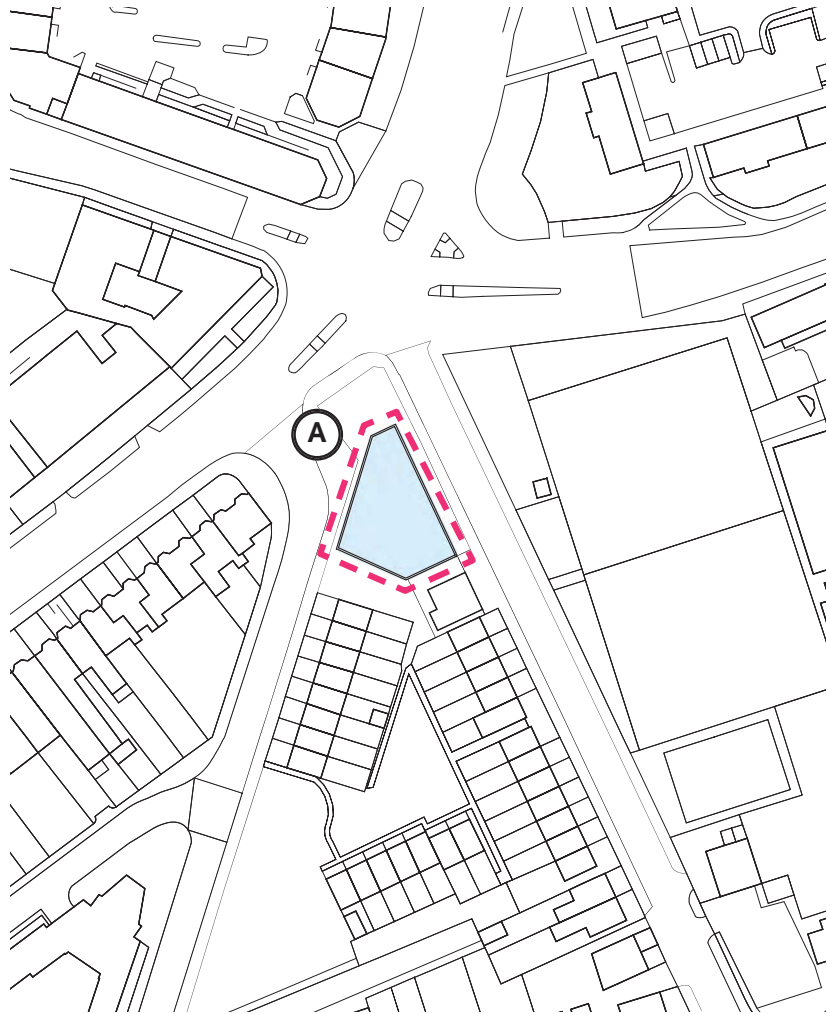


Key Plan (Not to Scale)



-  New Build- Resi Townhouse/ Mansion Block
-  New Build- Apartments
-  Commercial Ground Floor (potential amenity space for apartments or car parking)

# Site 04



Key Plan (Not to Scale)



■ New Build- Apartments

- - - Commercial Ground Floor (potential amenity space for apartments or car parking)

### Site 1

Block	Gross Area	Nett	Type	Storeys	Quantity	Notes
A	N/A		Town House	2.5	9	112.5sqm
					9	

### Site 2

Block	Gross Area	Nett	Type	Storeys	Quantity	Notes
A	N/A		Town House	2.5	9	112.5sqm
B	N/A		Town House	2.5	8	112.5sqm
C	659	527	Commercial/Apartments	4	33	11 x 1 bed (50sqm). 22 x 2 bed (70sqm)
D	381	304	Apartments	3	14	6 x 1 bed (50sqm). 8 x 2bed (70sqm)
E	441	353	Apartments	4	20	7 x 1 bed (50sqm). 13 x 2 bed (70sqm)
F	505	404	Apartments	3	18	8 x 1 bed (50sqm). 10 x 2 bed (70sqm)
G	720	576	Apartments	4	44	22 x 1 bed (50sqm). 24 x 2 bed (70sqm)
					146	

### Site 3

Block	Gross Area	Nett	Type	Storeys	Quantity	Notes
A	470	376	Commercial/Apartments	6	9	7 x 1 bed (50sqm). 20 x 2 bed (70sqm)
B	N/A		Town House	2.5	9	112.5sqm
C	N/A	577	Town House	2.5	12	112.5sqm
D	497	397.6	Apartments	5	25	7 x 1 bed (50sqm). 18 x 2bed (70sqm)
					55	

### Site 4

Block	Gross Area	Nett	Type	Storeys	Quantity	Notes
A	520	416	Commercial/Apartments	4	9	8 x 1 bed (50sqm). 12 x 2 bed (70sqm)
					9	

Donald Insall Associates